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Jacob Graves Digitally signed by Jacob Graves
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GRESHAM SMITH
2095 LAKESIDE CENTRE WAY, SUITE 210
KNOXVILLE, TN 37922
JAKE GRAVES, P.E., P.E. NO. 122211

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

ROADWAY SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY - SIGN1
TITLE SHEET	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
GENERAL NOTES	2C
SPECIAL NOTES	2D
ENVIRONMENTAL NOTES	2E – 2E2
RIGHT-OF-WAY NOTES, UTILITY NOTES, AND UTILITY OWNERS	3
RIGHT-OF-WAY ACQUISITION TABLE	3A
PRESENT LAYOUT	4
PROPOSED LAYOUT	4A
EROSION PREVENTION AND SEDIMENT CONTROL PLANS	5
CHANNEL CROSS SECTIONS	6-12
WALL CROSS SECTIONS	13-17
TRAFFIC CONTROL PLANS	T1
EXISTING UTILITIES	U1-1

YEAR	PROJECT NO.	SHEET NO.
2026	411040-3-004	ROADWAY - SIGN 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

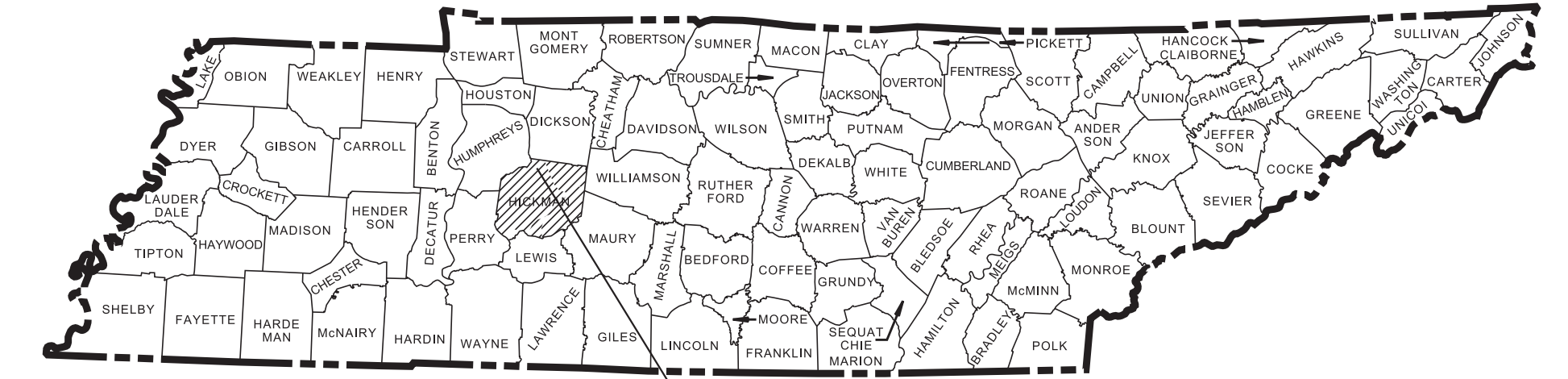
HICKMAN COUNTY

STRUCTURAL REPAIR AND PROTECTION OF
BRIDGE 41-10040-1.53 L&R OVER
GARNER CREEK

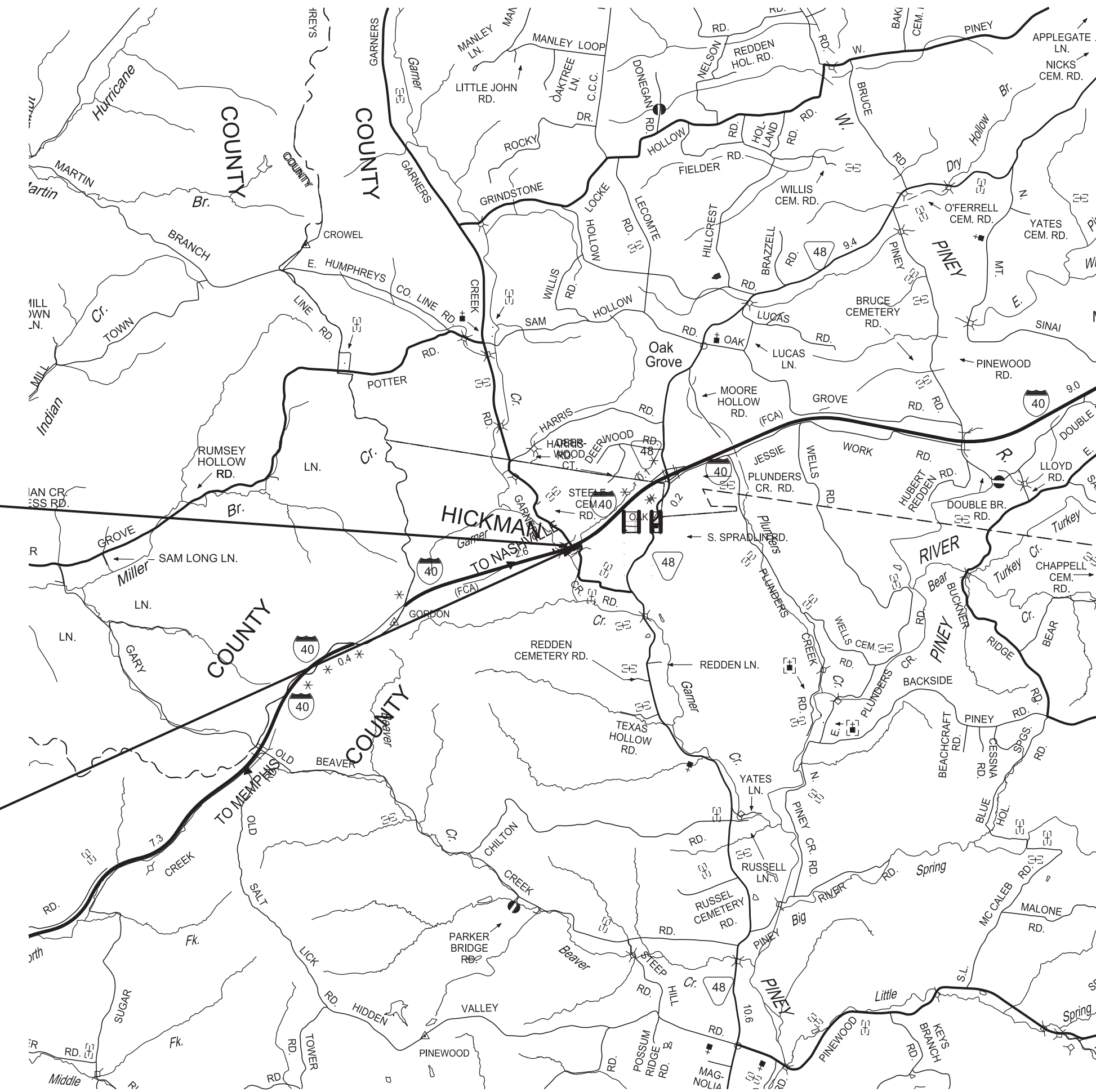
BRIDGE REPAIR

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	<input checked="" type="radio"/>
WORK ZONE SIGNIFICANCE DETERMINATION	SIGNIFICANT	<input checked="" type="radio"/>

TENN.	YEAR	SHEET NO.
	2026	1
FED. AID PROJ. NO.		
STATE PROJ. NO.	411040-M3-004	



PROJECT LOCATION
BRIDGE ID. # 41100400027 41100400028



NO EXCLUSIONS

LAND DISTURBANCE
LESS THAN 1 ACRE

BEGIN PROJECT NO. 411040-M3-004 CONSTRUCTION

STA. 101+63.34

N 602660.8211 E 1523105.7124

END PROJECT NO. 411040-M3-004 CONSTRUCTION

STA. 105+02.20

N 602427.9263 E 1523298.1210

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED APRIL 1, 2026 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT REPAIR OFFICE PROJECT MANAGER: JOSEPH BENDER

DESIGNER : BRADEN WELLS, P.E.

CHECKED BY : JAKE GRAVES, P.E.

P.E. NO. 411040-M3-004

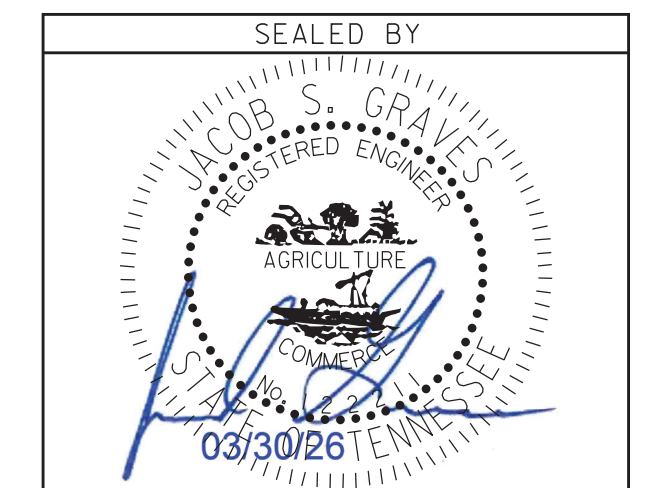
PIN NO. 133334.00

SCALE: 1"= 5280'



SURVEY 10-24-22	TRAFFIC DATA	
	ADT (2023)	N/A

COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00008 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 18B.



APPROVED: *Shane Hester*
SHANE HESTER, CHIEF ENGINEER

DATE:

APPROVED: *Will Reid*
WILL REID, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

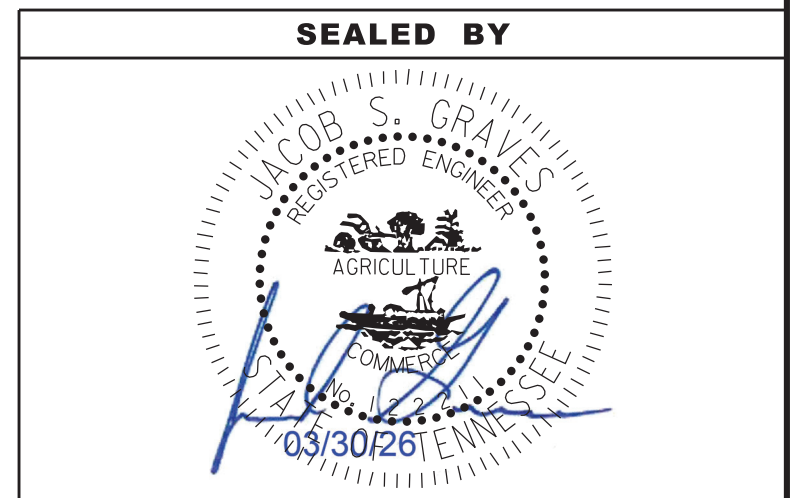
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PS&E ROADWAY INDEX

STANDARD ROADWAY DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	1A

SHEET NAME	SHEET NO.	DWG.	REV.	DESCRIPTION
SIGNATURE SHEET	ROADWAY-SIGN1	10-100.00		STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS
TITLE SHEET	1			
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS	1A	RD-TP-1	09-26-16	STANDARD ROADWAY DRAWINGS TITLE SHEET
PROJECT COMMITMENTS	1B	RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
ESTIMATED ROADWAY QUANTITIES	2	RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
GENERAL NOTES	2C	RD-L-1	01-30-26	STANDARD LEGEND
SPECIAL NOTES	2D	RD-L-7	02-20-20	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
ENVIRONMENTAL NOTES	2E – 2E2			
RIGHT-OF-WAY NOTES, UTILITY NOTES, AND UTILITY OWNERS	3	10-101.00		ROADWAY DESIGN STANDARDS
RIGHT-OF-WAY ACQUISITION TABLE	3A	RD11-TS-1	06-28-19	DESIGN STANDARDS FOR LOW-VOLUME ROADS
PRESENT LAYOUT	4	10-109.00		EROSION PREVENTION AND SEDIMENT CONTROL
PROPOSED LAYOUT	4A	EC-STR-30A		INSTREAM DIVERSION (WITH TRAFFIC)
EROSION PREVENTION AND SEDIMENT CONTROL PLANS	5	EC-STR-2	08-01-12	SEDIMENT FILTER BAG
CHANNEL CROSS SECTIONS	6-12	EC-STR-3C	03-01-23	SILT FENCE WITH WIRE BACKING
WALL CROSS SECTIONS	13-17	EC-STR-25	08-01-12	TEMPORARY CULVERT CROSSING, CONSTRUCTION EXIT, CONSTRUCTION FORD
TRAFFIC CONTROL PLANS	T1			
BRIDGE PLANS	B – 1			
EXISTING UTILITIES	U1-1			
NOTE: THE ALPHABETICAL LETTERS "I", "O" & "Q" ARE NOT USED IN THE NUMBERING OF SHEETS.				



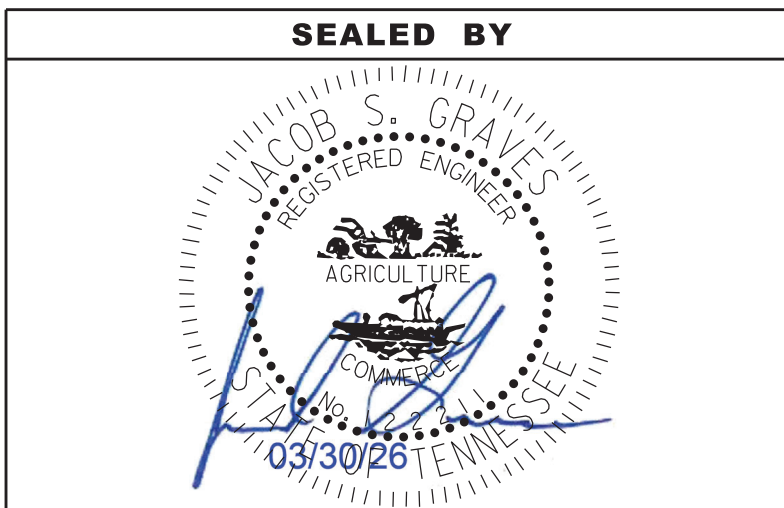
**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**ROADWAY INDEX
AND
STANDARD
ROADWAY
DRAWINGS**

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISION	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL	Asbestos Containing Material (ACM) surveys were completed on the following bridges and asbestos was detected: 1. Bridge No. 41100400027 I-40 EB over Garner Creek LM 1.53 (41-10040-01.53R) has 18 deck drains that are assumed asbestos based on the results from bridge 41100400028. 2. Bridge No. 41100400028 I-40 WB over Garner Creek LM 1.53 (41-10040-01.53L) has 18 deck drains at 20% chrysotile.	-
EDHZ002	ENVIRONMENTAL	The State of Tennessee asbestos accreditation requirements (TDEC Rules Chapter 1200-01-20) mandates that ACM abatement work be performed by an accredited firm (contractor) using accredited abatement workers and supervisors. Abatement of this material shall be accomplished per SP202ACM Special Provision Regarding Removal of Asbestos-Containing Materials. ACM abatement should be completed prior to any demolition activities if possible. Prior to the demolition or rehabilitation of any structure (bridge or building), the contractor is required to submit the National Emission Standards for Hazardous Air Pollutants standard 10-day notice of demolition to the TDEC Division of Air Pollution Control (per TDOT Standard Specifications for Road and Bridge Construction (January 1, 2021) Sections 107.08.D and 202.03).	-
ETR3001	ENVIRONMENTAL	In accordance with the Programmatic Consultation for Addressing Cliff Swallows and Barn Swallows on Transportation Projects dated 9/16/2020, cliff swallow and barn swallow nests, eggs, or birds (young and adults) will not be disturbed between April 15 and July 31. From August 1 to April 14, nests may be removed or destroyed, and measures may be implemented to prevent future nest building at the site (e.g., closing off area using netting).	-
ETR3002	ENVIRONMENTAL	A fish sweep will be conducted immediately prior to in-stream work. The contractor shall contact the Region 3 Environmental Tech Group, at least 14 days prior to in-stream work to coordinate the fish sweep: 615-335-8783, R3.EnvTechOffice@tn.gov, 6601 Centennial Blvd Bldg A 2nd Flr Nashville, TN 37243.	-
ETR3003	ENVIRONMENTAL	In order to reduce impacts to state listed fish during spawning season, in-stream work is prohibited from May 15th to July 31st.	

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STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PROJECT
 COMMITMENTS

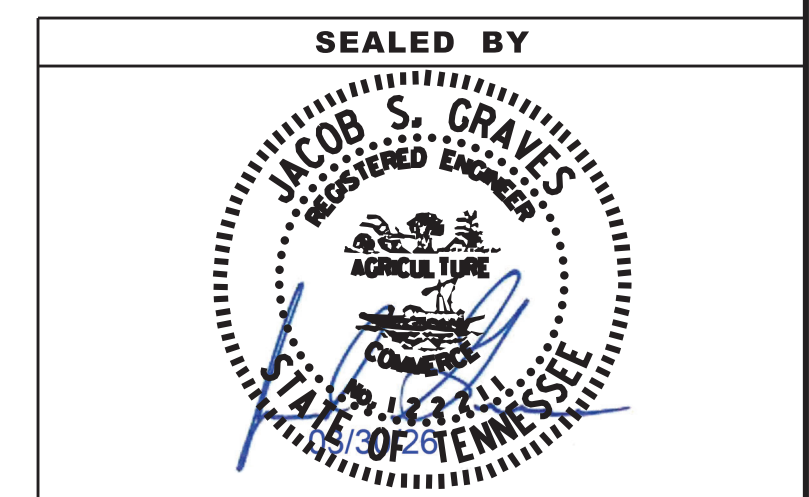
TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	2

ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	411040-M3-004 QUANTITY
	105-01 CONSTRUCTION STAKES, LINES AND GRADES	LS	1
(1)	201-01 CLEARING AND GRUBBING	LS	1
(3)	203-01 ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED)	C.Y.	100
	203-07 FURNISHING & SPREADING TOPSOIL	C.Y.	297
(2)(7)	203-50 CONSTRUCTION OF HAUL ROADS	LS	1
(5)	209-05 SEDIMENT REMOVAL	C.Y.	24
(4)(5)(6)	209-08.02 TEMPORARY SILT FENCE (WITH BACKING)	L.F.	670
(4)(5)	209-09.04 SEDIMENT FILTER BAG (15' X 10')	EACH	2
(4)(5)	209-65.04 TEMPORARY IN STREAM DIVERSION	L.F.	430
(4)(5)	303-10.01 MINERAL AGGREGATE (SIZE 57)	TON	160
	621-03.02 18" TEMPORARY DRAINAGE PIPE	L.F.	35
(4)(5)(7)	709-05.05 MACHINED RIP-RAP (CLASS A-3)	TON	200
	712-01 TRAFFIC CONTROL	LS	1
	712-06 SIGNS (CONSTRUCTION)	S.F.	73
	712-07.03 TEMPORARY BARRICADE TYPE III	L.F.	20
	717-01 MOBILIZATION	LS	1
(5)(7)	740-10.03 GEOTEXTILE (TYPE III)(EROSION CONTROL)	S.Y.	484
	801-01 SEEDING (WITH MULCH)	UNIT	24
	801-03 WATER (SEEDING AND SODDING)	M.G.	2.40

FOOTNOTES:

- (1) ITEM NO. 201-01 SHALL INCLUDE THE REMOVAL OF ABANDONED UTILITY POLES (IF NECESSARY) AND THE REMOVAL AND REPLACEMENT OF BARBEDWIRE FENCING NEAR THE CONSTRUCTION ENTRANCE/ EXIT (IF NECESSARY) TO PLACE THE HAUL ROAD ACCORDING TO APPROX. LOCATON OF HAUL ROAD AS SHOWN ON SHEET 4, 4A, AND 5.
- (2) ITEM NO. 203-50 SHALL INCLUDE 1072 S.Y. GEOTEXTILE (TYPE IV), 383 TONS MACHINED RIP-RAP (CLASS A-1), AND 179 TONS MINERAL AGGREGATE (SIZE 57). THE MINERAL AGGREGRATE INCLUDES AN ADDITIONAL TEN (10) PERCENT FOR MAINTENANCE.
- (3) INCLUDES 30 CY TO BE USED FOR CONSTRUCTION OF TEMPORARY CONSTRUCTION ENTRANCE/ EXIT.
- (4) SEE SUBSECTION 209.07 OF STANDARD SPECIFICATIONS FOR MAINTAINACE REPLACEMENT.
- (5) AS DIRECTED BY THE ENGINEER.
- (6) PROVIDE J HOOKS WHEN BMP IS NOT PARALLEL TO CONTOURS.
- (7) GEOTEXTILE TO BE PLACED UNDER ALL RIP-RAP.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (2) CERTIFICATION FOR ALL BORROW PITS MUST BE OBTAINED IN ACCORDANCE WITH SUBSECTION 107.06 OF THE STANDARD SPECIFICATIONS.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

SEEDING AND SODDING

- (1) ALL EXISTING ROADS WITHIN THE RIGHT-OF-WAY AND NOT IN THE GRADED AREA THAT ARE TO BE ABANDONED SHALL BE SCARIFIED, OBLITERATED, TOPSOILED AND SEEDED. SCARIFYING AND OBLITERATING THE PAVEMENT WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS. TOPSOIL, IN ACCORDANCE WITH SECTION 203 OF THE STANDARD SPECIFICATIONS, WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 203-04. SEEDING, IN ACCORDANCE WITH SECTION 801 OF THE STANDARD SPECIFICATIONS, WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 801-01.
- (2) SOD SHALL BE PLACED AT LOCATIONS SHOWN ON THE PLANS TO PREVENT DAMAGE TO ADJACENT FACILITIES AND PROPERTY DUE TO EROSION ON ALL NEWLY GRADED CUT AND FILL SLOPES AS WORK PROGRESSES.
- (3) ITEM NO. 801-01, SEEDING (WITH MULCH), SHALL BE USED ON SLOPES 3:1 OR STEEPER AND OTHER AREAS AS INDICATED IN THE PLANS THAT ARE INACCESSIBLE FOR MOWING.
- (4) ITEM NO. 801-01, SEEDING (WITH MULCH), SHALL BE USED WHERE EROSION CONTROL BLANKET OR SOD ARE NOT APPLIED.

GUARDRAIL

- (1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

MISCELLANEOUS

- (1) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

GRADED SOLID ROCK

- (1) THE ROCK FILL (GRADED SOLID ROCK) MATERIAL SHALL CONSIST OF SOUND, NON-DEGRADABLE LIMESTONE OR SANDSTONE WITH A MAXIMUM SIZE OF 3'-0". AT LEAST 50% (BY WEIGHT) OF THE ROCK SHALL BE UNIFORMLY DISTRIBUTED BETWEEN 1'-0" AND 3'-0" IN DIAMETER, AND NO GREATER THAN 10% (BY WEIGHT) SHALL BE LESS THAN 2" IN DIAMETER. THE MATERIAL SHALL BE ROUGHLY EQUIDIMENSIONAL; THIN, SLABBY MATERIALS WILL NOT BE ACCEPTED. THE CONTRACTOR SHALL BE REQUIRED TO PROCESS THE MATERIAL WITH AN ACCEPTABLE MECHANICAL MEANS (A SCREENING PROCESS CAPABLE OF PRODUCING THE REQUIRED GRADATION). THE ROCK SHALL BE APPROVED BY A REPRESENTATIVE OF THE DIVISION OF MATERIALS AND TESTS BEFORE USE.
- (2) THIS GRADED SOLID ROCK MATERIAL SHALL BE PLACED IN LAYERS NOT EXCEEDING FIVE FEET IN DEPTH.

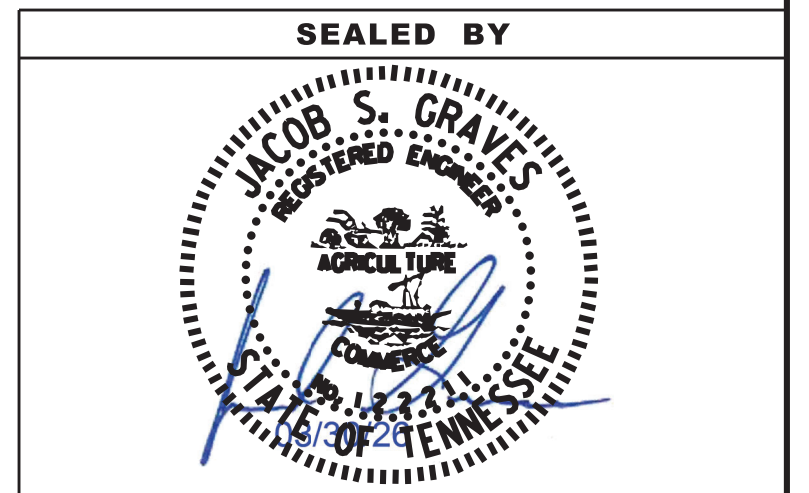
SIGNING

- (1) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	2C



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
NOTES

SPECIAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	2D

DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

- (1) THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).
- (2) ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATION OF BRIDGE(S). ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.
- (3) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

RETAINING WALLS

- (1) THE AREA BETWEEN STATION 102+12.98 TO STATION 105+02.20 SHALL REMAIN CLEAR FOR THE CONSTRUCTION OF THE RETAINING WALL. NO UTILITY LINES MAY BE PLACED THERE WITHOUT APPROVAL FROM STRUCTURES DIVISION.
- (2) THE OPTIONS FOR RETAINING WALL TYPES SHALL BE LIMITED TO THE APPROVED ALTERNATIVES AS SPECIFIED ON THE RETAINING WALL SHEET(S).

SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL
NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	2E

ENVIRONMENTAL NOTES

SUBSECTION 1 – ENVIRONMENTAL GENERAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (1) SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT NATURAL RESOURCES AND WATER QUALITY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG NATURAL RESOURCES IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS, WETLANDS OR OTHER NATURAL FEATURES IN ACCORDANCE WITH TDOT STANDARDS. EPSC MEASURES SHALL BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.
- (2) NEW CHANNEL CONSTRUCTION SHALL BE COMPLETED IN THE DRY AND STABILIZED FOR AT LEAST 72 HOURS PRIOR TO DIVERTING WATER FROM THE EXISTING AND/OR TEMPORARY CHANNEL.
- (3) INSTREAM EPSC DEVICES REQUIRE THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION REVIEW AND MUST BE PROCESSED BY THE PERMITS SECTION TO OBTAIN WATER QUALITY PERMITS.
- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (5) THE WIDTH OF THE FILL ASSOCIATED WITH TEMPORARY CROSSINGS SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR THE ACTUAL CROSSING, NOT TO EXCEED THE WIDTH SPECIFIED IN THE STANDARD DRAWING.
- (6) STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CULVERT CROSSINGS SHALL BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES SHALL BE USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAMBED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK SHALL BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS SHALL BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO PREEXISTING ELEVATIONS. ALL TEMPORARY CROSSINGS SHALL BE CONSTRUCTED IN ACCORDANCE WITH STD. DWG. EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (E.G. BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAMBED IS AN ACCEPTABLE OPTION.
- (7) HEAVY EQUIPMENT WORKING IN WETLANDS WITH PERMITTED TEMPORARY IMPACTS SHALL BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE AND COMPACTION UNLESS SPECIFICALLY ADDRESSED IN THE CONSTRUCTION PLANS. ANY MATS AND OTHER MEASURES USED FOR HEAVY EQUIPMENT SHALL BE REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED. ALL AFFECTED AREAS SHOULD BE RETURNED TO PRE-EXISTING CONDITIONS.
- (8) WETLANDS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING, OR TRANSPORTATION AREAS, UNLESS SPECIFICALLY PROVIDED FOR IN THE CONSTRUCTION PLANS AND PERMITS.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (10) NO ACTIVITY MAY SUBSTANTIALLY DISRUPT THE MOVEMENT OF THOSE SPECIES OF AQUATIC LIFE INDIGENOUS TO THE WATER BODY, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA.
- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND

EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).

- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (13) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO ARCHAEOLOGY, ECOLOGY, HISTORICAL, HAZARDOUS MATERIALS, AIR AND NOISE, TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING ANY MATERIAL AND STAGING AREAS AND THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS TO BE USED. ANY SUCH PERMITS SHALL BE SUPPLIED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO THE USE OF THE PERMITTED AREA(S).
- (14) ANY DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT RESPONSIBLE PARTY. THE ENVIRONMENTAL DIVISION, DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.
- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.
- (16) THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATE. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.
- (17) ALL WATER QUALITY PERMITS SHALL BE POSTED NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE ACCESSIBLE TO THE PUBLIC. THE NAME, COMPANY NAME, EMAIL ADDRESS, TELEPHONE NUMBER AND ADDRESS OF THE PROJECT SITE OWNER, OPERATOR, OR A LOCAL CONTACT PERSON WITH A BRIEF DESCRIPTION OF THE PROJECT SHALL ALSO BE POSTED. IF POSTING THIS INFORMATION NEAR A MAIN ENTRANCE IS INFEASIBLE, THE INFORMATION SHALL BE PLACED IN A PUBLICLY ACCESSIBLE LOCATION NEAR WHERE THE CONSTRUCTION IS ACTIVELY UNDERWAY AND MOVED AS NECESSARY. THIS LOCATION SHALL BE POSTED AT THE CONSTRUCTION SITE. ALL POSTINGS SHALL BE MAINTAINED IN LEGIBLE CONDITION.

SUPPORT ACTIVITIES

- (18) MATERIALS AND STAGING AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY ENVIRONMENTAL PERMITS, OBTAINED SOLELY BY THE CONTRACTOR. THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATES. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.

STREAMS, WETLANDS & BUFFER ZONES

- (19) ONCE WATER IS DIVERTED INTO A NEWLY CONSTRUCTED AND STABILIZED RELOCATED STREAM / CHANNEL, THE ECOLOGY SECTION SHALL BE NOTIFIED. THE STREAM NAME, STREAM NUMBER, AND DATE THE WATER WAS DIVERTED INTO THE NEWLY CONSTRUCTED STREAM / CHANNEL SHALL BE SUPPLIED WITH THE NOTIFICATION.

ENVIRONMENTAL

- (20) EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL.

SUBSECTION 2 – ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

- (5) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SUBSECTION 3 – EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

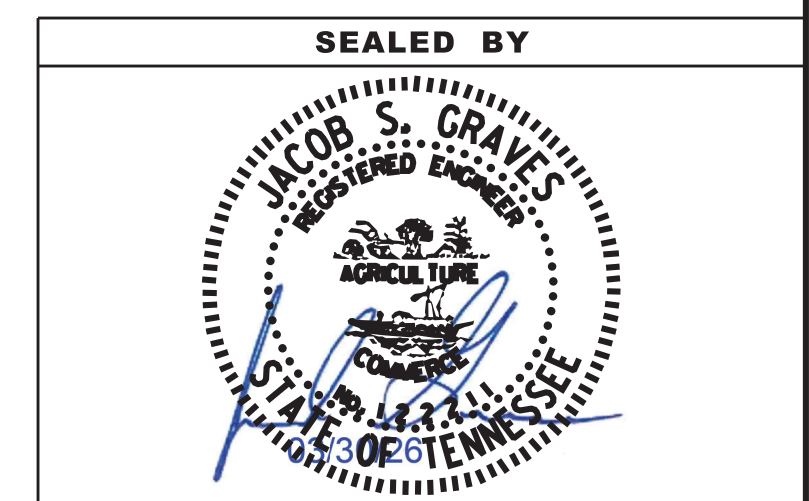
EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.
- (2) AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.
- (3) UNLESS OTHERWISE NOTED IN THE PLANS, THE CONTRACTOR SHALL NOT CLEAR/DISTURB ANY AREA BEYOND 15 FEET FROM SLOPE LINES.
- (4) PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 14 CALENDAR DAYS PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS APPLIED.
- (5) CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION, INCLUDING STREAM AND WETLAND BUFFERS (UNLESS PERMITTED), SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.

SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (7) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY BUT MUST BE REINSTALLED AT THE END OF THE WORKDAY OR BEFORE/DURING A PRECIPITATION EVENT.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE



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ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

- (9) OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION EXIT (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.
- (10) THE DEWATERING OF WORK AREAS, TRENCHES, FOUNDATIONS, EXCAVATIONS, ETC. THAT HAVE COLLECTED STORMWATER, WATER FROM VEHICLE WASH AREAS, OR GROUNDWATER SHALL BE EITHER HELD IN SETTLING BASINS OR TREATED BY FILTRATION AND/OR CHEMICAL TREATMENT PRIOR TO ITS DISCHARGE. ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S GUIDELINES AND FULLY DESCRIBED IN THE EPSC PLANS. WATER DISCHARGED SHALL NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITHIN THE RECEIVING NATURAL RESOURCE. WATER MUST BE HELD IN SETTLING BASINS UNTIL AT LEAST AS CLEAR AS THE RECEIVING WATERS. SETTLING BASINS SHALL NOT BE LOCATED CLOSER THAN 20 FEET FROM THE TOP BANK OF A STREAM. SETTLING BASINS AND SEDIMENT TRAPS SHALL BE PROPERLY DESIGNED ACCORDING TO THE SIZE OF THE DRAINAGE AREAS OR VOLUME OF WATER TO BE TREATED. TREATED WATER MUST BE DISCHARGED THROUGH A PIPE OR WELL-VEGETATED OR LINED CHANNEL, SO THAT THE DISCHARGE DOES NOT CAUSE EROSION OR SEDIMENT TRANSPORT. DISCHARGES FROM BASINS AND IMPOUNDMENTS SHALL UTILIZE OUTLET STRUCTURES THAT ONLY WITHDRAW WATER FROM NEAR THE SURFACE OF THE BASIN OR IMPOUNDMENT. DISCHARGES MUST NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITH THE RECEIVING STREAM.

INSPECTION, MAINTENANCE & REPAIR

- (11) THE TDOT CONSTRUCTION SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S RESPONSIBLE PARTY ARE RESPONSIBLE FOR INSPECTIONS. MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR. THE TDOT CONSTRUCTION SUPERVISOR OR THEIR DESIGNEE SHALL COMPLETE THE EPSC INSPECTION REPORTS AND DISTRIBUTE COPIES PER THE CONTRACT.
- (12) TDOT CONSULTANTS AND CONTRACTOR STAFF RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. TDOT STAFF AND SUPERVISORS RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDOT "FUNDAMENTALS OF EROSION AND SEDIMENT CONTROL" CLASS AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION.
- (13) EPSC CONTROLS SHALL BE INSPECTED ACCORDING TO PERMIT REQUIREMENTS TO VERIFY MEASURES HAVE BEEN INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS, SPECIFICATIONS, AND GOOD ENGINEERING PRACTICES. EPSC INSPECTIONS SHALL BE DOCUMENTED ON THE TDOT EPSC INSPECTION REPORT.
- (14) DISCHARGE POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING EROSION AND CONTROLLING SEDIMENT INCLUDING SIGNIFICANT IMPACTS TO SURROUNDING NATURAL RESOURCES AND ADJACENT PROPERTY OWNERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWN GRADIENT LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFFSITE ROADWAY SEDIMENT TRACKING.
- (15) UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE 24 HOUR TIMEFRAME, WRITTEN DOCUMENTATION SHALL BE PROVIDED IN THE FIELD DIARY AND EPSC INSPECTION REPORT. AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.

- (16) INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES SHALL BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE STEPS TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE EPSC MEASURES AT THE CONTRACTOR'S OWN EXPENSE.
- (17) THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.
- (18) SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE ONTO ADJACENT PROPERTIES AND INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.

EROSION PREVENTION

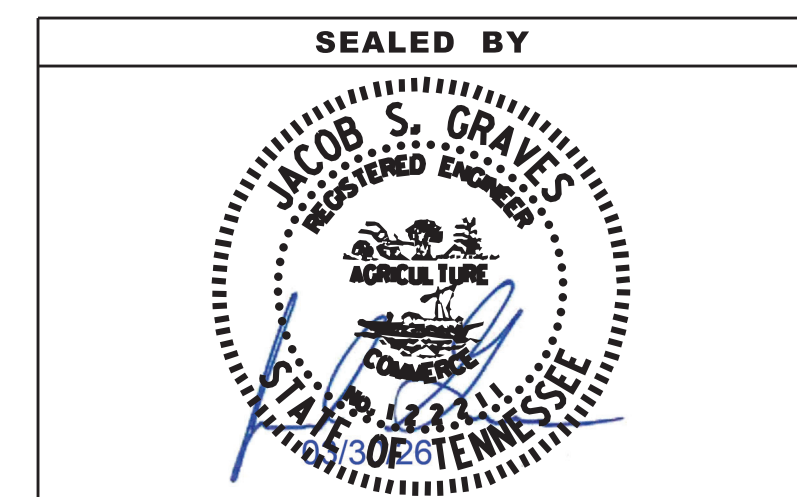
- (19) CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.
- (20) THE ACCEPTED EPSC PLAN SHALL REQUIRE THAT EPSC MEASURES BE IN PLACE BEFORE CLEARING, GRUBBING, EXCAVATION, GRADING, CULVERT OR BRIDGE CONSTRUCTION, CUTTING, FILLING, OR ANY OTHER EARTHWORK OCCURS, EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (21) NO WORK SHALL BE STARTED UNTIL THE CONTRACTOR'S PLAN FOR THE STAGING OF OPERATIONS, INCLUDING THE PLAN FOR STAGING OF TEMPORARY AND PERMANENT EPSC MEASURES, HAS BEEN ACCEPTED BY THE TDOT RESPONSIBLE PARTY. THE CONTRACTOR'S EPSC PLAN SHALL INCORPORATE AND SUPPLEMENT, AS ACCEPTABLE, THE BASIC EPSC DEVICES ON THE EPSC PLAN.
- (22) TEMPORARY STABILIZATION SHALL BE INITIATED WITHIN 14 CALENDAR DAYS WHEN CONSTRUCTION ACTIVITIES ON A PORTION OF THE SITE ARE TEMPORARILY CEASED AND EARTH DISTURBING ACTIVITIES WILL NOT RESUME UNTIL AFTER 14 CALENDAR DAYS. PERMANENT STABILIZATION MEASURES IN DISTURBED AREAS SHALL BE INITIATED WITHIN 14 CALENDAR DAYS AFTER FINAL GRADING OF ANY PHASE OF CONSTRUCTION.
- (23) STEEP SLOPES SHALL BE TEMPORARILY STABILIZED NOT LATER THAN 7 DAYS AFTER CONSTRUCTION ACTIVITY ON THE SLOPE HAS TEMPORARILY OR PERMANENTLY CEASED. STEEP SLOPES ARE DEFINED AS A NATURAL OR CREATED SLOPE OF 35% GRADE OR GREATER REGARDLESS OF HEIGHT.
- (24) PERMANENT STABILIZATION WILL REPLACE TEMPORARY MEASURES AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.
- (25) TEMPORARY OR PERMANENT STABILIZATION MUST BE FREE OF FINES (SILT AND CLAY SIZED PARTICLES). UNPACKED GRAVEL CONTAINING FINES OR CRUSHER-RUN WILL NOT BE CONSIDERED SUFFICIENT STABILIZATION.
- (26) DELAYING THE PLANTING OF COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED.

PERMITS, PLANS & RECORDS

- (27) THE EPSC PLAN IS TO SERVE AS AN INITIAL GUIDE FOR SITE PERSONNEL AS THE CONSTRUCTION PROCESS DEVELOPS. IT MUST BE AMENDED, MODIFIED, AND UPDATED WHENEVER A CHANGE IN THE DESIGN OR CONSTRUCTION OF THE PROJECT OCCURS. THE STAGES DEPICTED IN THE EPSC PLANS MAY NOT COINCIDE WITH THE ACTUAL PHASES OF CONSTRUCTION ESTABLISHED BY THE CONTRACTOR DURING CONSTRUCTION, THUS MODIFICATIONS WILL BE REQUIRED TO ENSURE THE EPSC PLAN IS MAINTAINED TO DEPICT CURRENT SITE CONDITIONS. IT SHOULD BE MAINTAINED SUCH THAT IT WILL ALWAYS REFLECT THE MEASURES THAT ARE INSTALLED DURING THE VARIOUS PHASES OF CONSTRUCTION. IT IS IMPRACTICAL TO DETERMINE ALL THE INTERMEDIATE PHASES OF CONSTRUCTION THAT WILL OCCUR; THUS THESE DOCUMENTS WILL HAVE TO BE UPDATED THROUGHOUT THE LIFE OF THE CONSTRUCTION PROJECT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (28) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (29) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (30) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (31) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (32) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (33) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (34) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (35) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (36) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (37) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (38) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (39) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.



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SUPPORT ACTIVITIES

- (40) IF OFFSITE BORROW AND WASTE AREAS BECOME NECESSARY DURING THE LIFE OF THE PROJECT, THIS SUPPORT ACTIVITY SHALL BE ADDRESSED PER THE TDOT WASTE AND BORROW MANUAL.
- (41) MATERIALS AND STAGING AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN.
- (42) IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO SUPPLY EPSC PLANS FOR THE MATERIAL AND STAGING AREAS TO THE ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE FOR REVIEW.

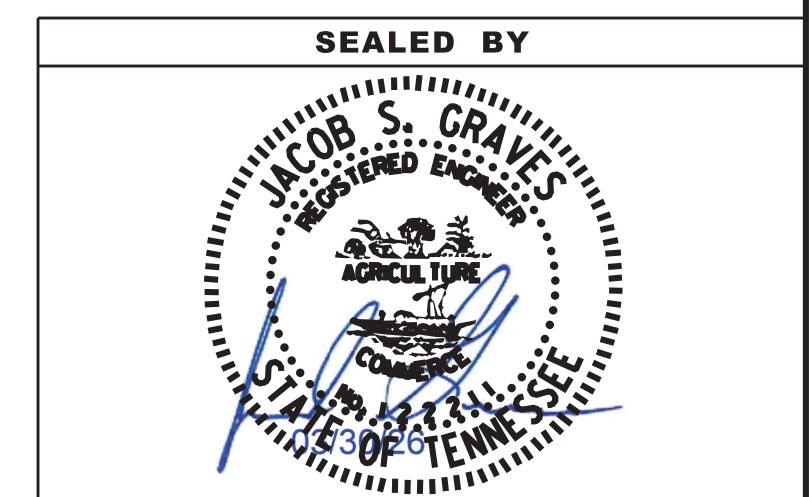
DIRECTLY BUT SHALL BE INCLUDED IN THE COST OF OTHER ITEMS. THIS NOTE EXCLUDES ANY ITEMS SPECIFIED IN THE PLANS FOR THE TEMPORARY DIVERSION CHANNELS (EC-STR-31) AND TEMPORARY DIVERSION CULVERTS (EC STR-32) FOR SINGLE BARREL CULVERT CONSTRUCTION.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (43) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (44) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (45) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (46) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (47) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (48) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (49) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (50) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (51) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (52) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

STREAMS, WETLANDS & BUFFER ZONES

- (53) ANY WORK WITHIN THE STREAM CHANNEL AREA (E.G., PIER FOOTING, RIP-RAP PLACEMENT, CULVERT/BRIDGE CONSTRUCTION, ETC.) SHALL BE SEPARATED FROM FLOWING WATER OR EXPECTED FLOW PATH AND PERFORMED DURING LOW FLOW CONDITIONS. ALL ITEMS USED WITHIN THE STREAM CHANNEL AREA FOR DIVERSION OF FLOW (OR EXPECTED FLOW), UNLESS SPECIFIED IN THE PLANS, SHALL NOT BE PAID FOR



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL
NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	3

RIGHT-OF-WAY

- (1) IT IS INTENDED THAT ALL BUILDINGS AND/OR PORTIONS OF BUILDINGS THAT ARE WITHIN THE PROPOSED RIGHT-OF-WAY AND/OR EASEMENT LINES FOR THE PROJECT BE REMOVED THERE FROM IN THE PROCESS OF RIGHT-OF-WAY ACQUISITION. IF ANY SUCH BUILDINGS OR IMPROVEMENTS ARE NOT REMOVED IN THE COURSE OF RIGHT-OF-WAY ACQUISITION, THE CIVIL ENGINEERING MANAGER 2, ROADWAY DESIGN DIVISION IS TO BE NOTIFIED IN SUFFICIENT TIME TO PERMIT HAVING SUCH REMOVALS DESIGNATED AS A PART OF THE CONSTRUCTION CONTRACT.
- (2) ALL RAMPS MUST CONFORM TO THE DEPARTMENT'S "POLICY ON FINANCING CONSTRUCTION OF PUBLIC ROAD INTERSECTIONS AND DRIVEWAYS ON HIGHWAY RESURFACING, RECONSTRUCTION AND CONSTRUCTION PROJECTS ON NEW LOCATIONS", THE MANUAL ON RULES AND REGULATIONS FOR CONSTRUCTING DRIVEWAYS ON STATE HIGHWAY RIGHT-OF-WAY, STANDARD DRAWING RP-R-1, AND OTHER ACCEPTED DESIGN AND SAFETY STANDARDS.
- (3) EXISTING PAVED DRIVEWAY PER TRACT REMAINDER WILL BE REPLACED IN KIND TO A TOUCHDOWN POINT.
- (4) WHERE THE EXISTING DRIVEWAY IS UNPAVED AND THE PROPOSED DRIVEWAY EXCEEDS 7 PERCENT IN GRADE, EACH DRIVEWAY WILL BE PAVED TO A TOUCHDOWN POINT OR UNTIL THE GRADE IS LESS THAN 7 PERCENT.
- (5) WHERE THE EXISTING DRIVEWAY IS UNPAVED AND THE PROPOSED DRIVEWAY IS LESS THAN 7 PERCENT IN GRADE, EACH DRIVEWAY WILL BE PAVED A SHOULDER WIDTH FROM THE EDGE OF PAVEMENT AND THE REMAINDER OF THAT DRIVEWAY REPLACED IN KIND TO A TOUCHDOWN POINT.
- (6) ANY NECESSARY PAVING OF DRIVEWAYS WILL BE DONE DURING PAVING OPERATIONS ON THE MAIN ROADWAY.
- (7) TRACT REMAINDERS NOT HAVING AN EXISTING DRIVEWAY WILL BE PROVIDED ONE 50-FOOT OPENING IN THE ACCESS CONTROL FENCE AND A DRIVEWAY WILL BE CONSTRUCTED UNLESS ACCESS IS PROVIDED FROM AN INTERSECTING ROAD OR BASED ON PHYSICAL CONDITIONS AND/OR CONFLICTS WITH OTHER DESIGN CONSIDERATIONS WHICH PREVENT AN ACCESS OPENING. PAVING OF THESE NEW DRIVEWAYS WILL BE IN ACCORDANCE TO THE 7 PERCENT CRITERIA PREVIOUSLY MENTIONED FOR EXISTING DRIVEWAYS.
- (8) NEW DRIVEWAYS PROVIDED IN THE PLANS WILL BE PAVED BASED ON THE 7 PERCENT CRITERIA. THOSE 7 PERCENT OR STEEPER IN GRADE WILL BE PAVED AND THOSE FLATTER THAN 7 PERCENT WILL BE COVERED WITH BASE STONE.
- (9) ON PROJECTS WITHOUT CURB AND GUTTER THAT ARE ON STATE ROUTES, IT WILL BE THE RESPONSIBILITY OF THE OWNER TO SECURE A PERMIT AND TO CONSTRUCT ADDITIONAL DRIVEWAYS AND FIELD ENTRANCES OTHER THAN THOSE PROVIDED IN THE PLANS.
- (10) ON PROJECTS WITH CURB AND GUTTER THAT ARE ON STATE ROUTES, IT WILL BE THE RESPONSIBILITY OF THE OWNER TO SECURE A PERMIT. AFTER THE PERMIT HAS BEEN GRANTED, THE DEPARTMENT WILL CONSTRUCT THE DRIVEWAY OR FIELD ENTRANCE THROUGH THE CURB AND SIDEWALK, PROVIDED THE CURB AND SIDEWALK HAVE NOT BEEN CONSTRUCTED. IT WILL BE THE RESPONSIBILITY OF THE PROPERTY OWNER TO CONSTRUCT THE DRIVEWAY OR FIELD ENTRANCE FROM BACK OF SIDEWALK TO TOUCHDOWN POINT FOR ANY ADDITIONAL DRIVEWAYS OR FIELD ENTRANCES OTHER THAN THOSE PROVIDED IN THE PLANS.
- (11) ON NON-STATE ROUTES, ADDITIONAL DRIVEWAYS AND FIELD ENTRANCES OTHER THAN THOSE PROVIDED IN THE PLANS SHALL REQUIRE A PERMIT ONLY IF THE LOCAL AGENCY SPECIFIES THE NEED FOR THAT PERMIT.
- (12) EASEMENT REQUIRED FOR THE RAILROAD CROSSING IS TO BE OBTAINED BY THE UTILITIES ENGINEER BY PROVISIONS CONTAINED IN THE CROSSING AGREEMENT NEGOTIATED WITH THE RAILROAD.

UTILITY

- (1) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM VISIBLE APPURTENANCES AT THE SITE, PUBLIC RECORDS, AND/OR MAPS PREPARED BY OTHERS. THEREFORE, RELIANCE UPON THE TYPE, SIZE, AND LOCATION OF UTILITIES SHOWN SHOULD BE DONE SO WITH THIS CIRCUMSTANCE CONSIDERED. DETAILED VERIFICATION OF EXISTENCE, LOCATION, AND DEPTH SHOULD ALSO BE MADE PRIOR TO ANY DECISION RELATIVE THERETO IS MADE. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. IN TENNESSEE, IT IS A REQUIREMENT, PER "THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT", THAT ANYONE WHO ENGAGES IN

EXCAVATION MUST NOTIFY ALL KNOWN UNDERGROUND UTILITY OWNERS, NO LESS THAN THREE (3) OR NOT MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE AND ALSO TO AVOID ANY POSSIBLE HAZARD OR CONFLICT. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

CABLE:

Comcast / Xfinity
660 Mainstream Drive
Nashville, TN 37228
CONTACT: Michael Lee
OFFICE PHONE: N/A
CELL PHONE: 615-504-0528
Email: Bas-nashvilleconstructionbetterments@comcast.com

ELECTRIC:

Meriwether Lewis Electric
1625 Highway 100
Centerville, TN 37033
CONTACT: Steve Skelton
OFFICE PHONE: 931 623 1636
CELL PHONE: N/A
Email: Steve.skelton@mllec.com

ELECTRIC:

TVA
1101 Market St. MR-4G
Chattanooga, TN 37402
CONTACT: Stephen Williams
OFFICE PHONE: N/A
CELL PHONE: 662 255 6272
Email: sewilliams@tva.gov

GAS:

Centerville City Hall Gas
128 North Central Avenue
Centerville, TN 37033
CONTACT: Keith Harvill
OFFICE PHONE: 931 729 4246 ext.127
CELL PHONE: 931 994 9100
Email: kharvill@centervilletn.org

TELEPHONE:

AT&T
116 South Cannon Ave.
Murfreesboro, TN 37129
CONTACT: Kenneth Kornegay
OFFICE PHONE: 615 848 2082
CELL PHONE: 615 631 7221
Email: Kk4096@att.com

WATER:

Bon Aqua-Lyles Utility District
7964 Spotlight Rd.
Lyles, TN 37098
CONTACT: David Connor
OFFICE PHONE: 931 670 3957
CELL PHONE: 931 209 5029
Email: Waterman1031@yahoo.com

ELECTRIC:

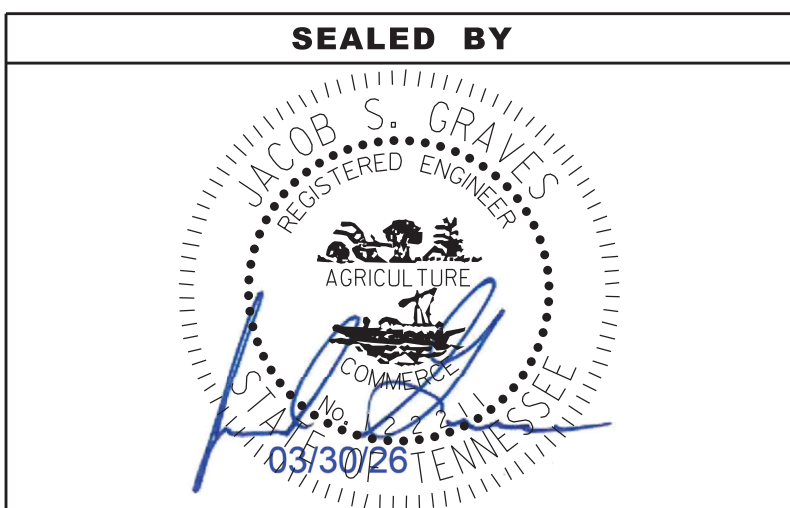
Meriwether Lewis
1625 Highway 100
CONTACT: Brent Warf
OFFICE PHONE: 931 729 7244
CELL PHONE: 931 623 0060
Email: Brent.warf@mllec.com

GAS:

BP Pipelines
30 South Wacker Dr.
Chicago, IL 60606
CONTACT: Keith Boyle
OFFICE PHONE: 312 809 4708
CELL PHONE: 312 358 0711
Email: Keith.boyle@bp.com

TELEPHONE:

Tenn. Electric Corp.
4587 West Main St.
Erin, TN 37061
CONTACT: Ray Bradley
OFFICE PHONE: 931 289 4221
CELL PHONE: 931 348 9036
Email: ebradley@tec.com



COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00008 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 18B.

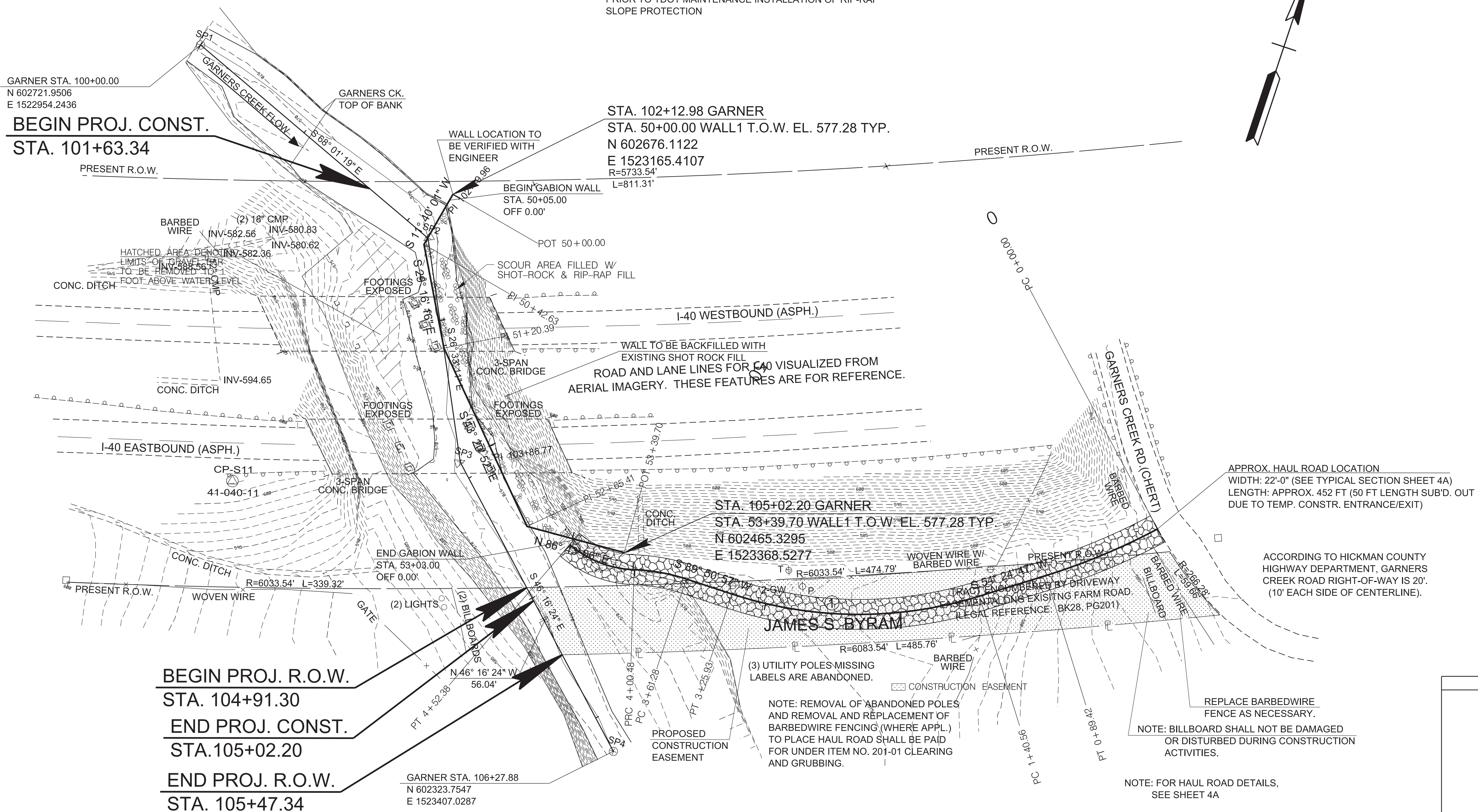
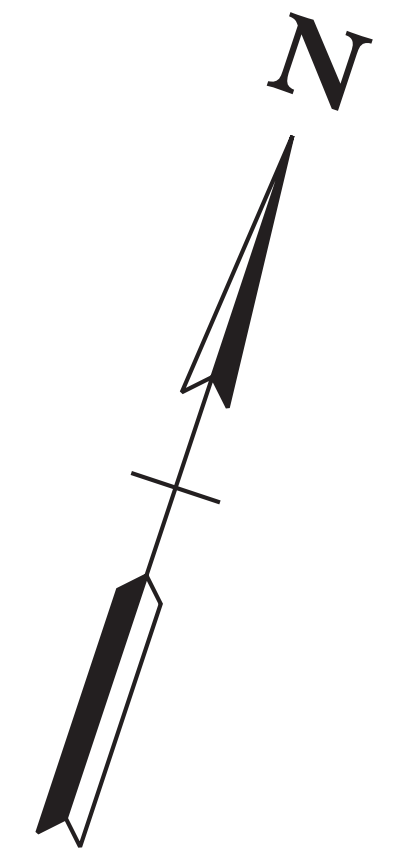
**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**RIGHT-OF-WAY
NOTES,
UTILITY NOTES
AND
UTILITY OWNERS**

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	4

Point	North	East	Elevation	Station	Offset	Feature	Description
S10	602370.3130	1522876.7460	604.6760	100+59.73	355.0868	XCP	41-040-10
S11	602424.3720	1523077.7630	602.2740	103+76.55	168.1963	XCP	41-040-11

*** NOTE: EXISTING CONTOURS REPRESENT CONDITIONS PRIOR TO TDOT MAINTENANCE INSTALLATION OF RIP-RAP SLOPE PROTECTION



GARNER STA. 100+00.00
N 602721.9506
E 1522954.2436
BEGIN PROJ. CONST.
STA. 101+63.34

STA. 102+12.98 GARNER
STA. 50+00.00 WALL 1 T.O.W. EL. 577.28 TYP.
N 602676.1122
E 1523165.4107
R=5733.54'
L=811.31'

STA. 105+02.20 GARNER
STA. 53+39.70 WALL 1 T.O.W. EL. 577.28 TYP
N 602465.3295
E 1523368.5277

BEGIN PROJ. R.O.W.
STA. 104+91.30
END PROJ. CONST.
STA. 105+02.20
END PROJ. R.O.W.
STA. 105+47.34

GARNER STA. 106+27.88
N 602323.7547
E 1523407.0287

APPROX. HAUL ROAD LOCATION
WIDTH: 22'-0" (SEE TYPICAL SECTION SHEET 4A)
LENGTH: APPROX. 452 FT (50 FT LENGTH SUB'D. OUT
DUE TO TEMP. CONSTR. ENTRANCE/EXIT)

ACCORDING TO HICKMAN COUNTY
HIGHWAY DEPARTMENT, GARNERS
CREEK ROAD RIGHT-OF-WAY IS 20'.
(10' EACH SIDE OF CENTERLINE).

(3) UTILITY POLES MISSING
LABELS ARE ABANDONED.
NOTE: REMOVAL OF ABANDONED POLES
AND REMOVAL AND REPLACEMENT OF
BARBEDWIRE FENCING (WHERE APPL.)
TO PLACE HAUL ROAD SHALL BE PAID
FOR UNDER ITEM NO. 201-01 CLEARING
AND GRUBBING.

REPLACE BARBEDWIRE
FENCE AS NECESSARY.
NOTE: BILLBOARD SHALL NOT BE DAMAGED
OR DISTURBED DURING CONSTRUCTION
ACTIVITIES.

NOTE: FOR HAUL ROAD DETAILS,
SEE SHEET 4A

SEALED BY

COORDINATES ARE NAD 83(2011), ARE
DATUM ADJUSTED BY THE FACTOR
OF 1.00008 AND TIED TO THE TGN.
ALL ELEVATIONS ARE REFERENCED
TO THE NAVD 1988 WITH GEOID 18B.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

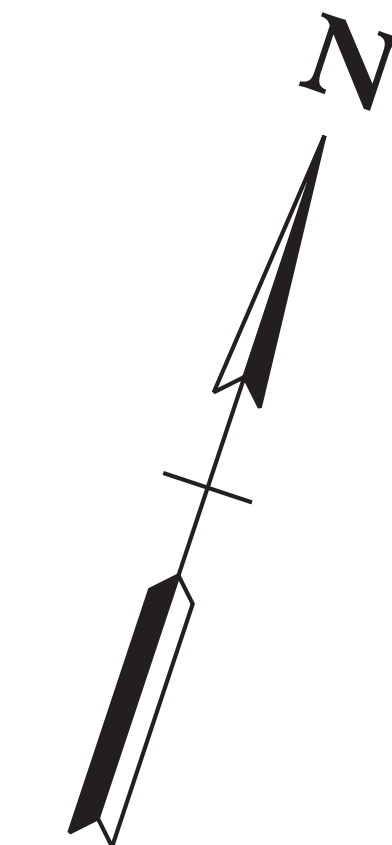
**PRESENT
LAYOUT**
STA. 101+63 TO STA. 105+02
SCALE: 1"= 40'

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Point	North	East	Elevation	Station	Offset	Feature	Description
S10	602370.3130	1522876.7460	604.6760	100+59.73	355.0868	XCP	41-040-10
S11	602424.3720	1523077.7630	602.2740	103+76.55	168.1963	XCP	41-040-11

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	4A

*** NOTE: EXISTING CONTOURS REPRESENT CONDITIONS PRIOR TO TDOT MAINTENANCE INSTALLATION OF RIP-RAP SLOPE PROTECTION



GARNER STA. 100+00.00
N 602721.9506
E 1522954.2436

BEGIN PROJ. CONST.
STA. 101+63.34

PRESENT R.O.W.

GARNER STA. 102+19.96
N 602639.6304
E 1523158.2193

HATCHED AREA DENOTES LIMITS OF GRAVEL BAR TO BE REMOVED TO 1 FOOT ABOVE WATER LEVEL

GARNERS CK.
TOP OF BANK

WALL LOCATION TO BE VERIFIED WITH ENGINEER

STA. 102+12.98 GARNER
STA. 50+00.00 WALL1 T.O.W. EL. 577.28 TYP.
N 602676.1122
E 1523165.4107
R=5733.54'
L=811.31'

PRESENT R.O.W.

BEGIN GABION WALL
STA. 50+05.00
OFF 0.00'

POT 50+00.00

I-40 WESTBOUND (ASPH.)

WALL TO BE BACKFILLED WITH EXISTING SHOT ROCK FILL
ROAD AND LANE LINES FOR I-40 VISUALIZED FROM AERIAL IMAGERY. THESE FEATURES ARE FOR REFERENCE.

I-40 EASTBOUND (ASPH.)

3-SPAN CONC. BRIDGE

CP-S10
41-040-10

CP-S11
41-040-11

GARNER STA. 103+86.77
N 602490.4204
E 1523232.7856

END GABION WALL
STA. 53+03.00
OFF 0.00'

STA. 105+02.20 GARNER
STA. 53+39.70 WALL1 T.O.W. EL. 577.28 TYP.
N 602465.3295
E 1523368.5277

APPROX. HAUL ROAD LOCATION
WIDTH: 22'-0" (SEE TYPICAL SECTION SHEET 4A)
LENGTH: APPROX. 452 FT (50 FT LENGTH SUB'D. OUT DUE TO TEMP. CONSTR. ENTRANCE/EXIT)

ACCORDING TO HICKMAN COUNTY HIGHWAY DEPARTMENT, GARNERS CREEK ROAD RIGHT-OF-WAY IS 20'. (10' EACH SIDE OF CENTERLINE).

PRESENT R.O.W.

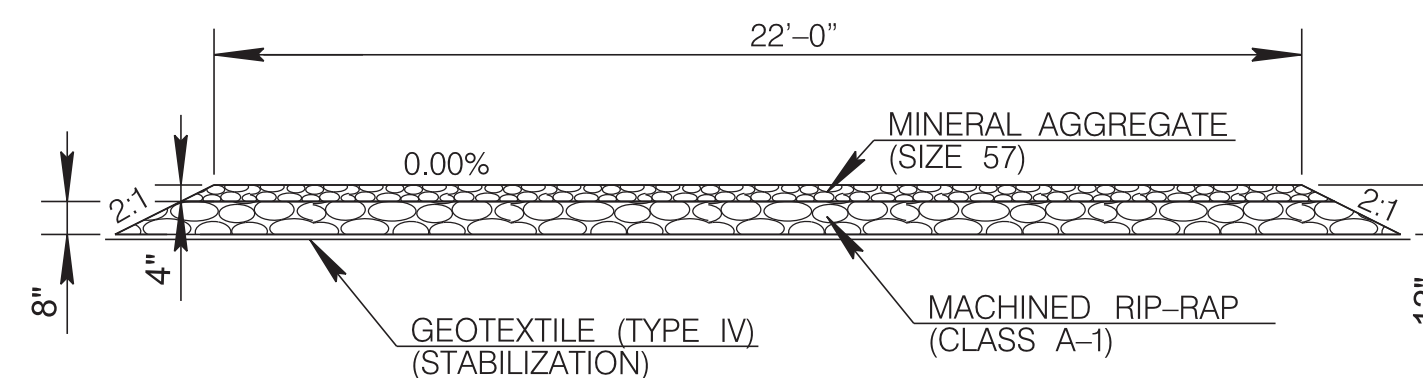
BEGIN PROJ. R.O.W.
STA. 104+91.30
END PROJ. CONST.
STA. 105+02.20
END PROJ. R.O.W.
STA. 105+47.34

GARNER STA. 106+27.88
N 602323.7547
E 1523407.0287

PROPOSED CONSTRUCTION EASEMENT

NOTE: REMOVAL OF ABANDONED POLES AND REMOVAL AND REPLACEMENT OF BARBEDWIRE FENCING (WHERE APPL.) TO PLACE HAUL ROAD SHALL BE PAID FOR UNDER ITEM NO. 201-01 CLEARING AND GRUBBING.

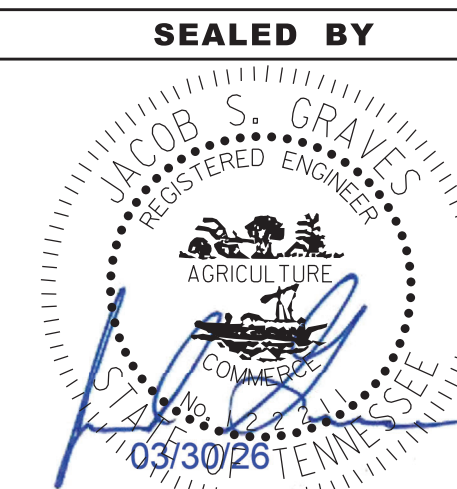
CONSTRUCTION EASEMENT



HAUL ROAD DETAIL

N.T.S.

NOTE: ALL ROCKS FROM HAUL ROAD INSTALLATION MUST BE CONTAINED BY GEOTEXTILE MATTING.
NOTE: ALL SLOPES AND GRASSED AREAS MUST BE SEEDED AND MULCHED IMMEDIATELY FOLLOWING THE REMOVAL OF THE HAUL ROAD.



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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

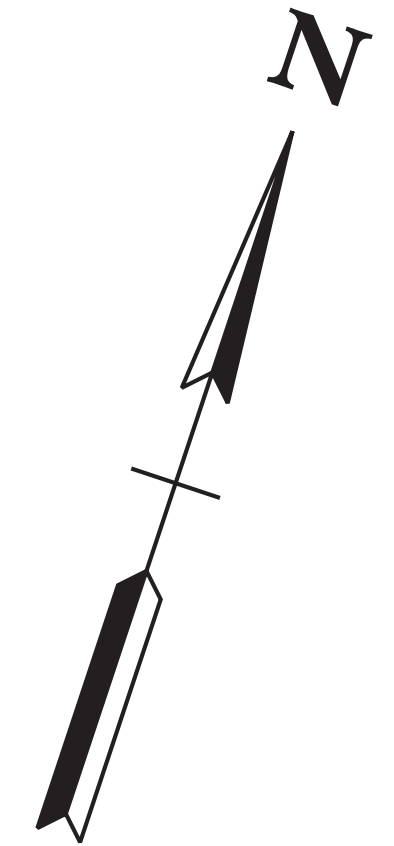
PROPOSED LAYOUT

STA. 101+63 TO STA. 105+02
SCALE: 1"= 40'

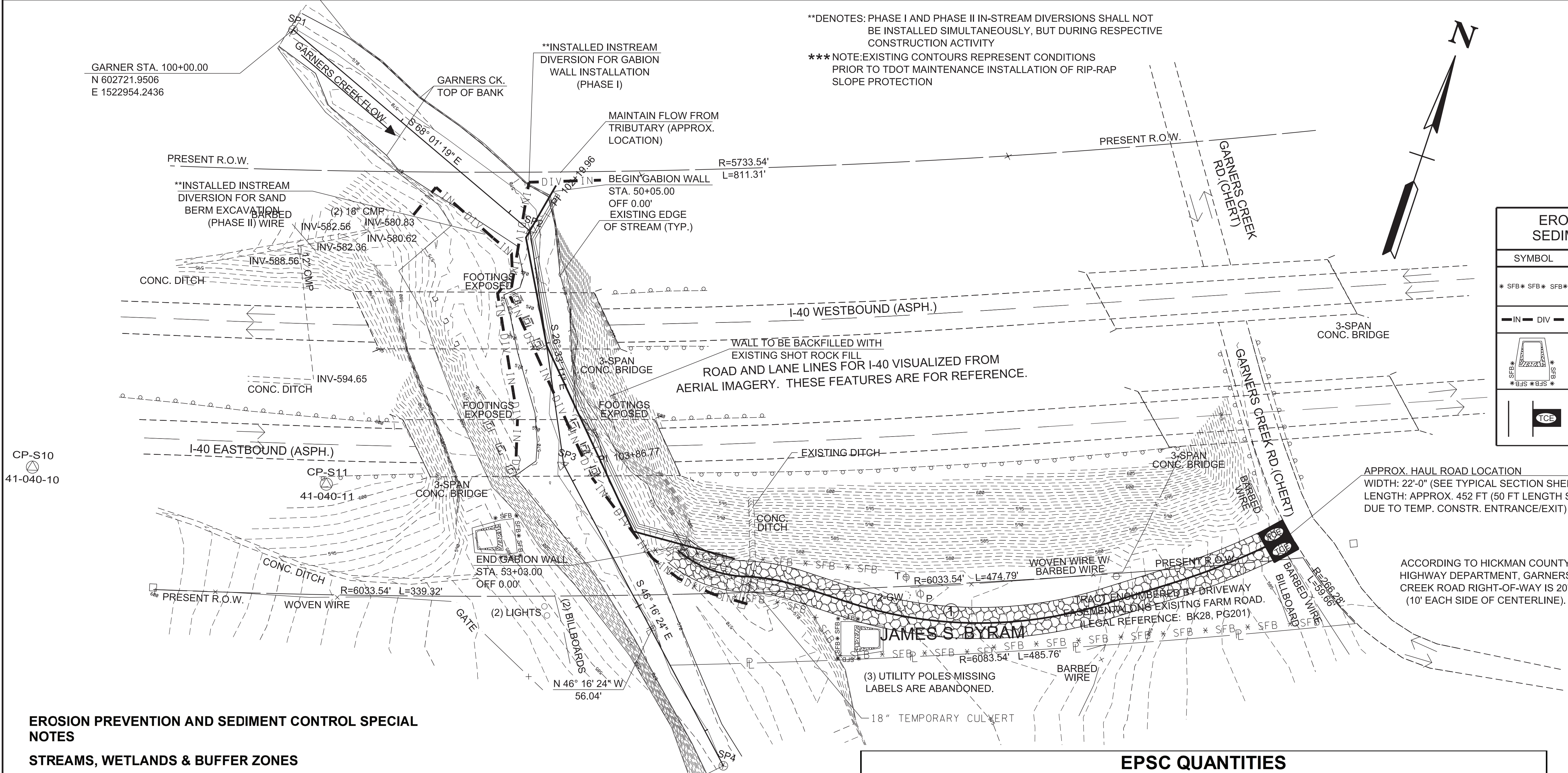
TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	5

**DENOTES: PHASE I AND PHASE II IN-STREAM DIVERSIONS SHALL NOT BE INSTALLED SIMULTANEOUSLY, BUT DURING RESPECTIVE CONSTRUCTION ACTIVITY

***NOTE: EXISTING CONTOURS REPRESENT CONDITIONS PRIOR TO TDOT MAINTENANCE INSTALLATION OF RIP-RAP SLOPE PROTECTION



EROSION PREVENTION AND SEDIMENT CONTROL LEGEND		
SYMBOL	ITEM	STD. DWG.
* SFB * SFB * SFB *	SILT FENCE WITH WIRE BACKING	EC-STR-3C
— IN — DIV —	INSTREAM DIVERSION	EC-STR-30 EC-STR-30A
	SEDIMENT FILTER BAG	EC-STR-2
	TEMPORARY CONSTRUCTION EXIT	EC-STR-25



EROSION PREVENTION AND SEDIMENT CONTROL SPECIAL NOTES

STREAMS, WETLANDS & BUFFER ZONES

- FOR PROJECTS THAT DISCHARGE INTO KNOWN EXCEPTIONAL TENNESSEE WATERS OR WATERS IMPAIRED BY SILTATION, A 60 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM WITH THIS DESIGNATION SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 60 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 30 FEET AT ANY MEASURED LOCATION.
- A 30 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 30 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 15 FEET AT ANY MEASURED LOCATION. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES.
- BUFFER ZONES ARE NOT SEDIMENT CONTROL MEASURES AND MUST NOT BE RELIED UPON AS PRIMARY SEDIMENT CONTROL MEASURES. THE RIPARIAN BUFFER ZONE SHALL BE ESTABLISHED BETWEEN THE TOP OF THE STREAM BANK AND THE DISTURBED CONSTRUCTION AREA. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES. BEST MANAGEMENT PRACTICES (BMPs) PROVIDING EQUIVALENT PROTECTION AS THE NATURAL RIPARIAN ZONE MAY BE USED, WHERE ISSUED, ARAP/401 REQUIREMENTS WILL PREVAIL IF IN CONFLICT WITH THESE BUFFER ZONE REQUIREMENTS.

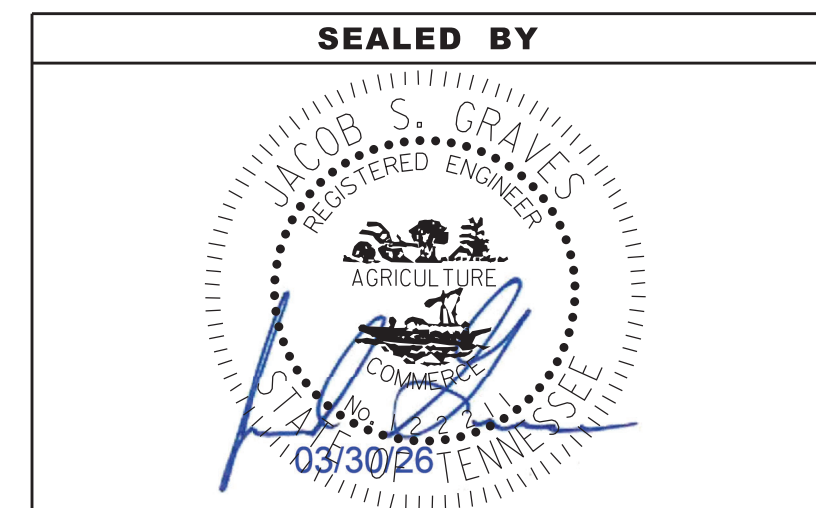
GARNER STA. 106+27.88
N 602323.7547
E 1523407.0287

EPSC QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY
(4)	203-01 ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED)	C.Y.	30
(1)	203-07 FURNISHING & SPREADING TOPSOIL	C.Y.	297
	209-05 SEDIMENT REMOVAL	C.Y.	24
(2)	209-08.02 TEMPORARY SILT FENCE (WITH BACKING)	L.F.	870
(5)	209-09.04 SEDIMENT FILTER BAG (15' X 10')	EACH	2
(3)	209-65.04 TEMPORARY IN STREAM DIVERSION	L.F.	430
(5)	303-10.01 MINERAL AGGREGATE (SIZE 57)	TON	160
(4)	709-05.05 MACHINED RIP-RAP (CLASS A-3)	TON	200
(4)(5)	740-10.03 GEOTEXTILE (TYPE III) (EROSION CONTROL)	S.Y.	312
	801-01 SEEDING (WITH MULCH)	UNIT	24
(5)	801-03 WATER (SEEDING AND SODDING)	M.G.	2.40

- FOOTNOTES:**
- ITEM NUMBER INCLUDES COST OF MATERIAL AND LABOR TO SPREAD TOPSOIL APPROX. 4" DEEP IN THE EASEMENT AREA TO BRING AREA BACK TO EXISTING CONDITIONS PRIOR TO CONSTRUCTION.
 - TO BE USED AS DIRECTED BY THE ENGINEER
 - TO BE PLACED AS DIRECTED BY ENGINEER AND ACCORDING TO TDOT STANDARD DWG. NO. EC-STR-30 & EC-STR-30A
 - TO BE USED FOR TEMPORARY CONSTRUCTION ENTRANCES
 - TO BE USED FOR SEDIMENT FILTER BAG PAD

APPROX. HAUL ROAD LOCATION
WIDTH: 22'-0" (SEE TYPICAL SECTION SHEET 4A)
LENGTH: APPROX. 452 FT (50 FT LENGTH SUB'D. OUT DUE TO TEMP. CONSTR. ENTRANCE/EXIT)

ACCORDING TO HICKMAN COUNTY HIGHWAY DEPARTMENT, GARNERS CREEK ROAD RIGHT-OF-WAY IS 20'. (10' EACH SIDE OF CENTERLINE).



COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00008 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 18B.

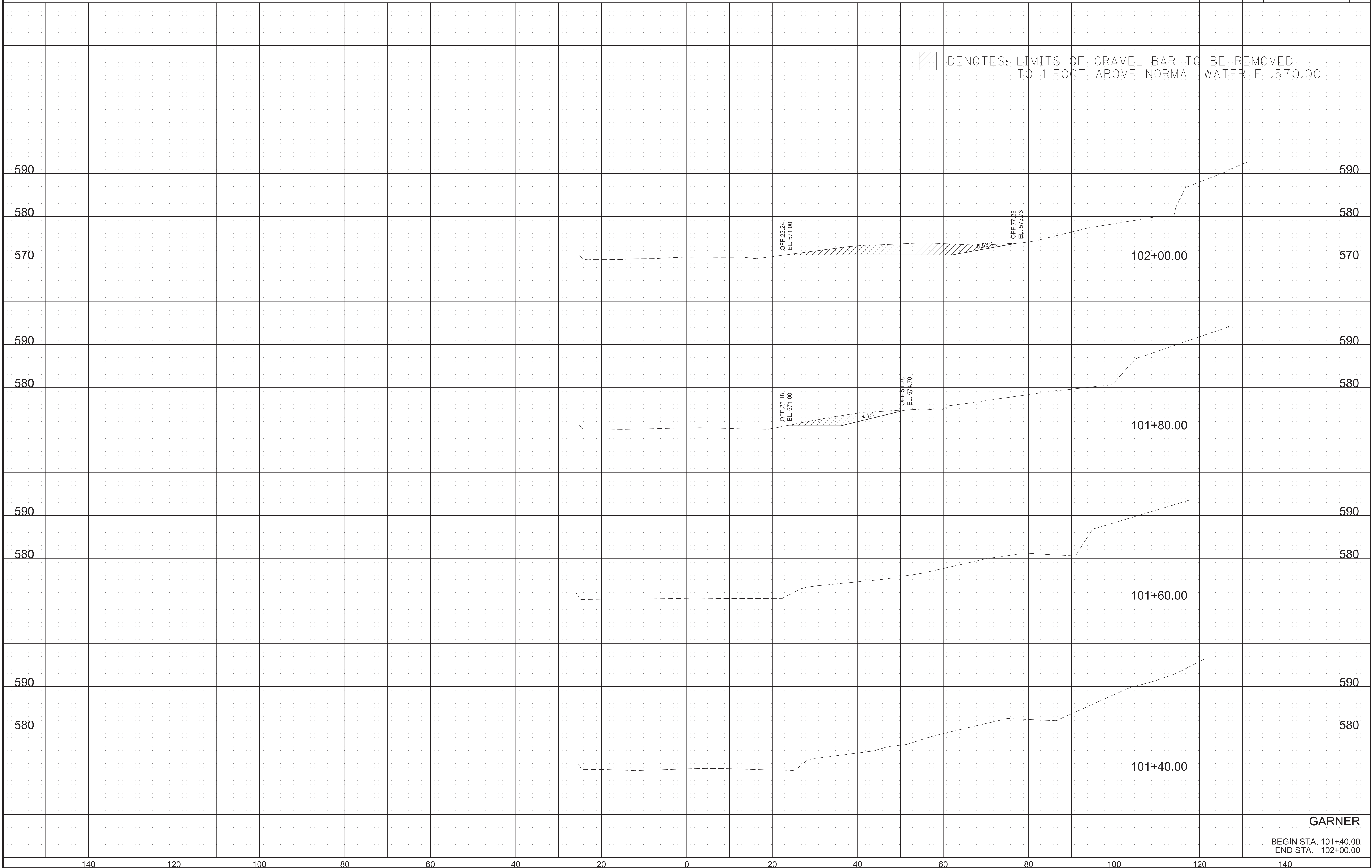
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EROSION PREVENTION & SEDIMENT CONTROL (EPSC)

GARNERS CK. RD.
SCALE: 1"= 40'

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 DENOTES: LIMITS OF GRAVEL BAR TO BE REMOVED TO 1 FOOT ABOVE NORMAL WATER EL.570.00



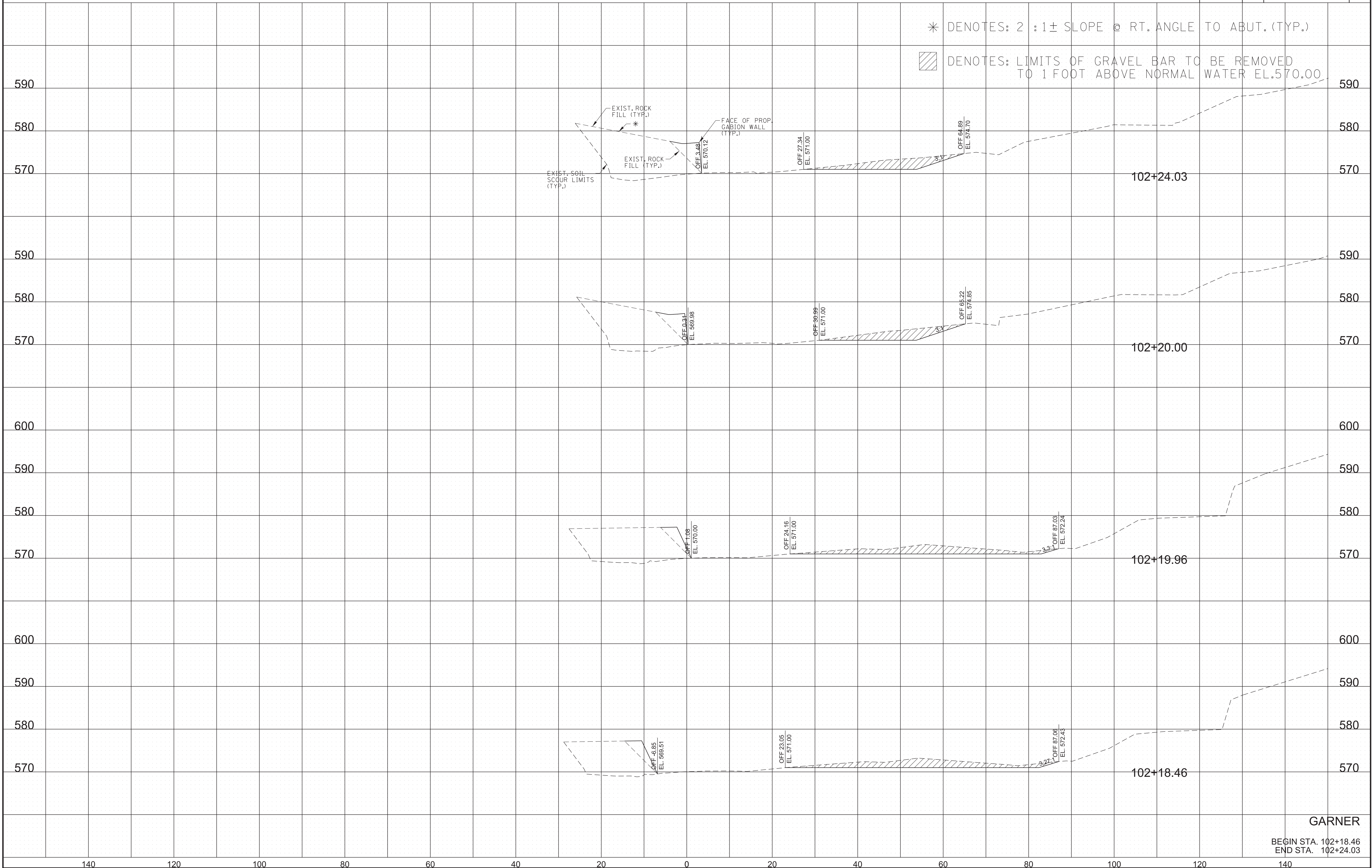
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GARNER
 BEGIN STA. 101+40.00
 END STA. 102+00.00

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	7

* DENOTES: 2 : 1± SLOPE @ RT. ANGLE TO ABUT. (TYP.)

 DENOTES: LIMITS OF GRAVEL BAR TO BE REMOVED TO 1 FOOT ABOVE NORMAL WATER EL.570.00



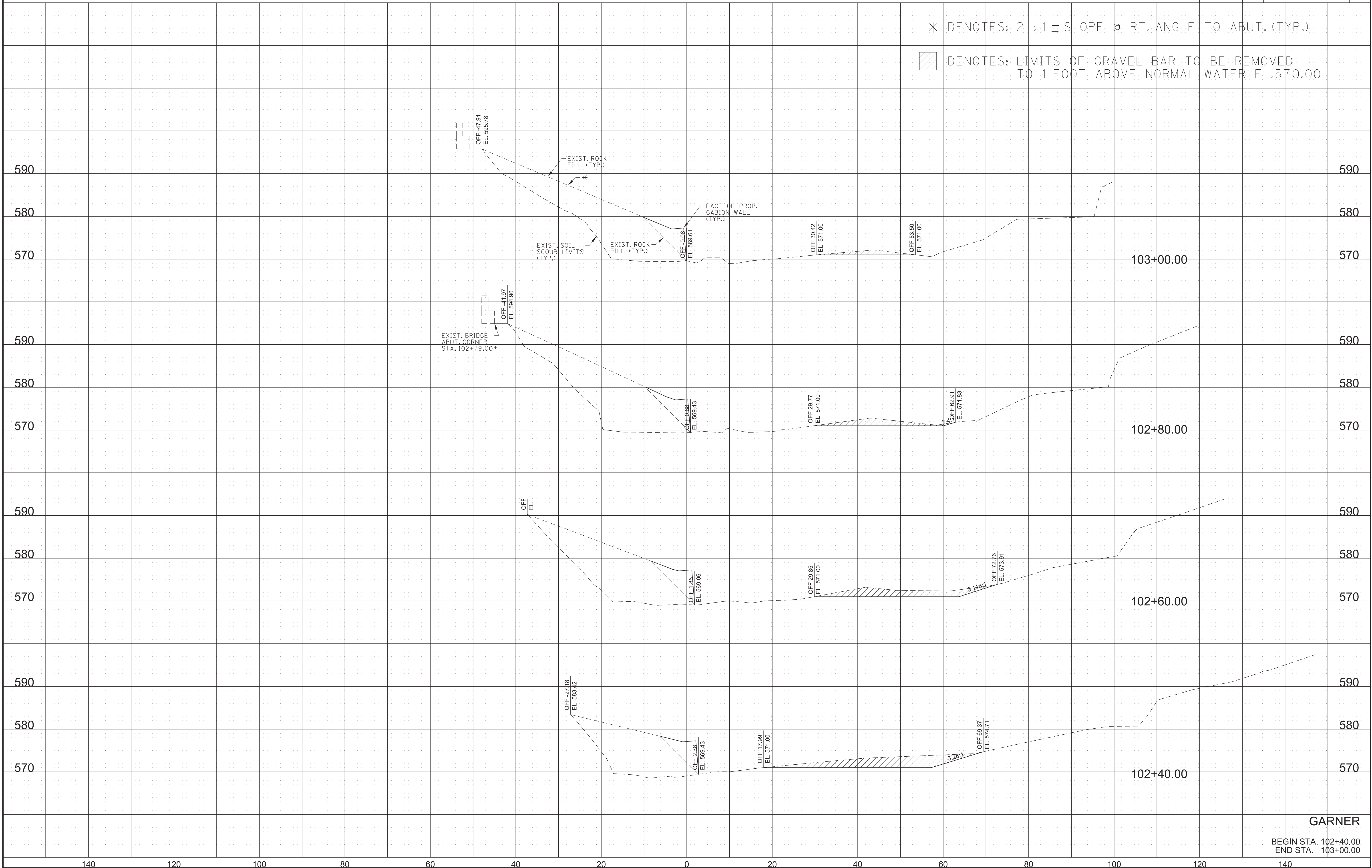
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GARNER

BEGIN STA. 102+18.46
END STA. 102+24.03

* DENOTES: 2 : 1 ± SLOPE @ RT. ANGLE TO ABUT. (TYP.)

▨ DENOTES: LIMITS OF GRAVEL BAR TO BE REMOVED TO 1 FOOT ABOVE NORMAL WATER EL.570.00

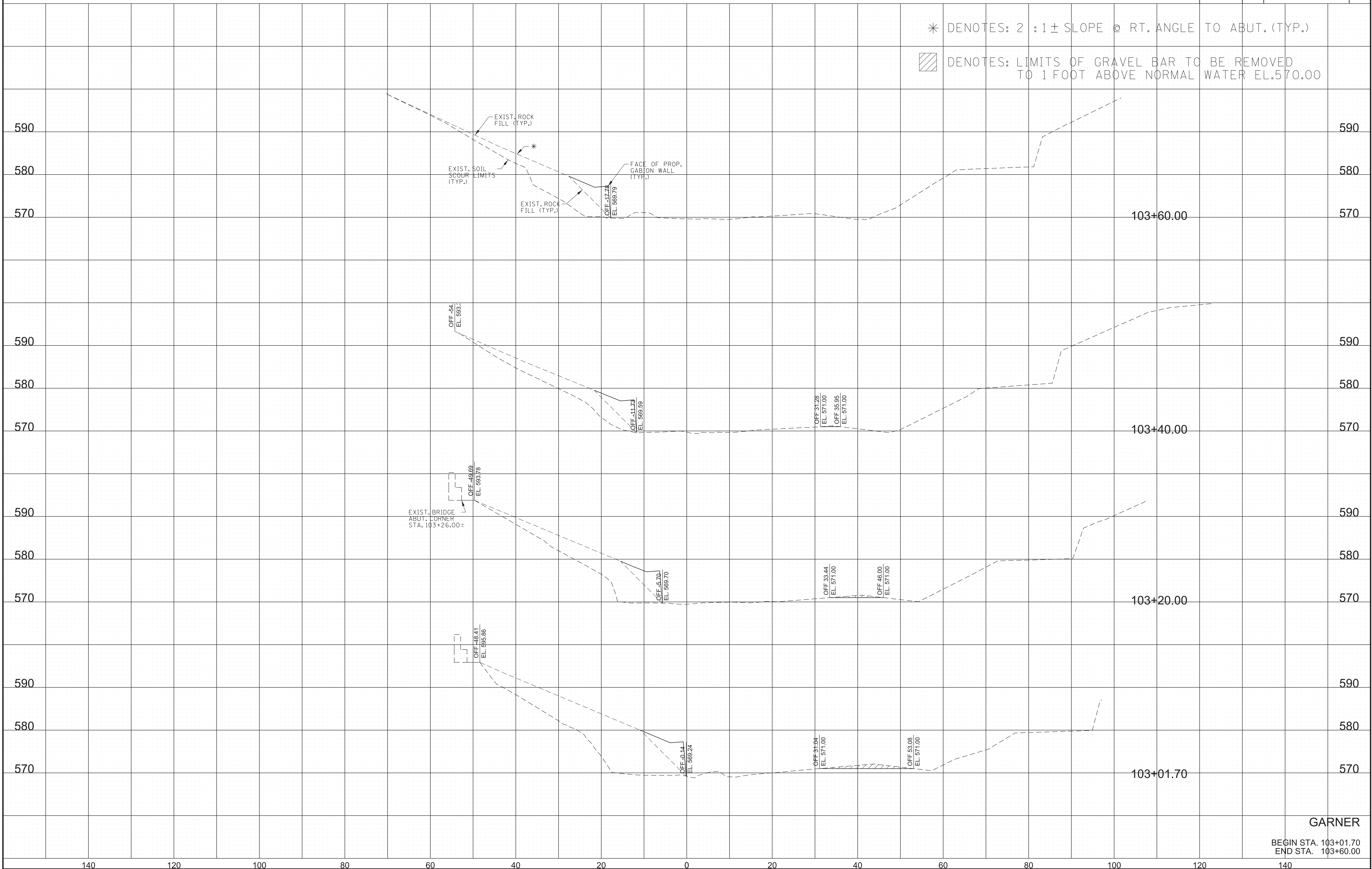


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GARNER
 BEGIN STA. 102+40.00
 END STA. 103+00.00

* DENOTES: 2 : 1 ± SLOPE @ RT. ANGLE TO ABUT. (TYP.)

▨ DENOTES: LIMITS OF GRAVEL BAR TO BE REMOVED TO 1 FOOT ABOVE NORMAL WATER EL.570.00

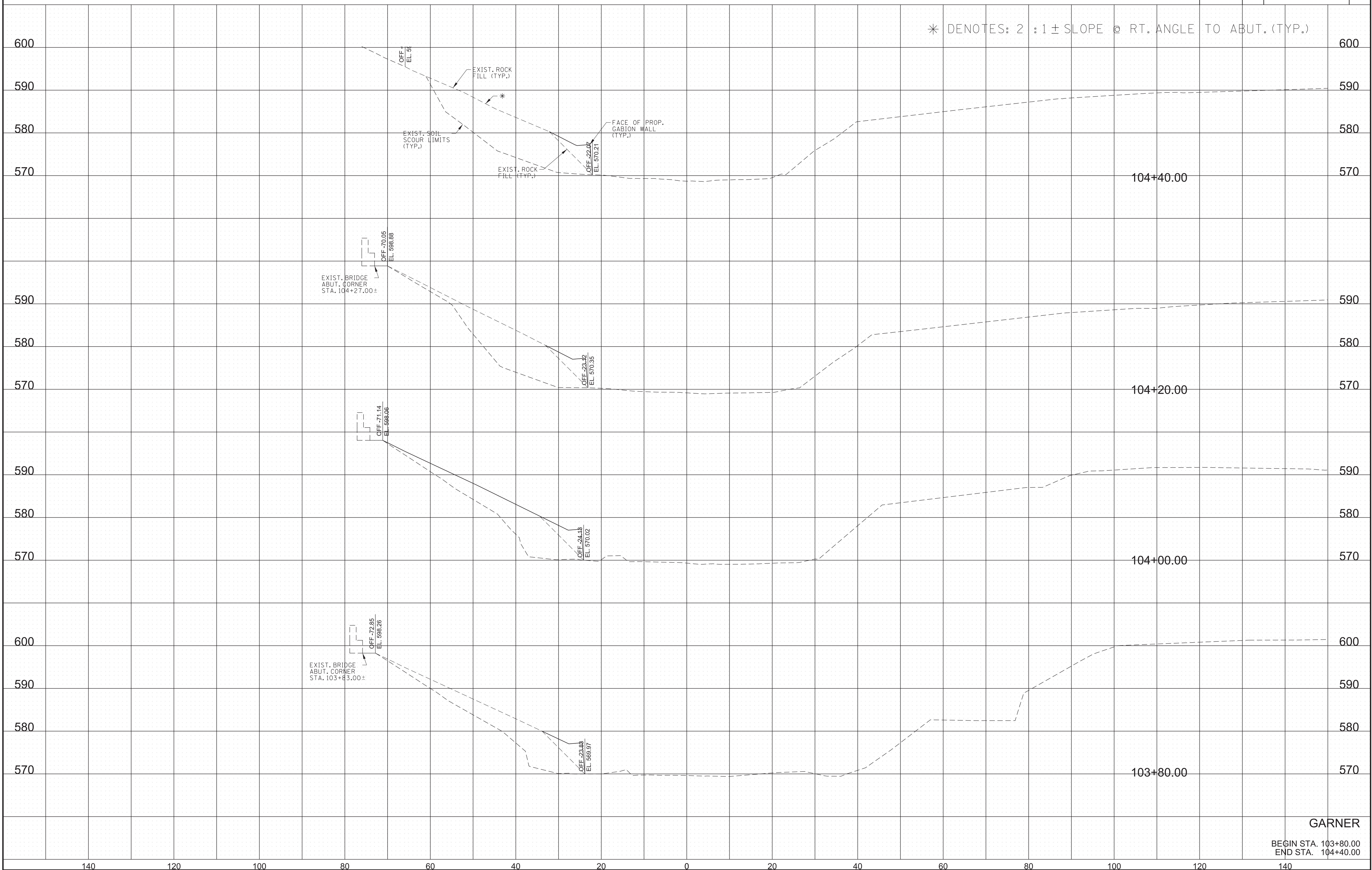


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GARNER
 BEGIN STA. 103+01.70
 END STA. 103+60.00

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	10

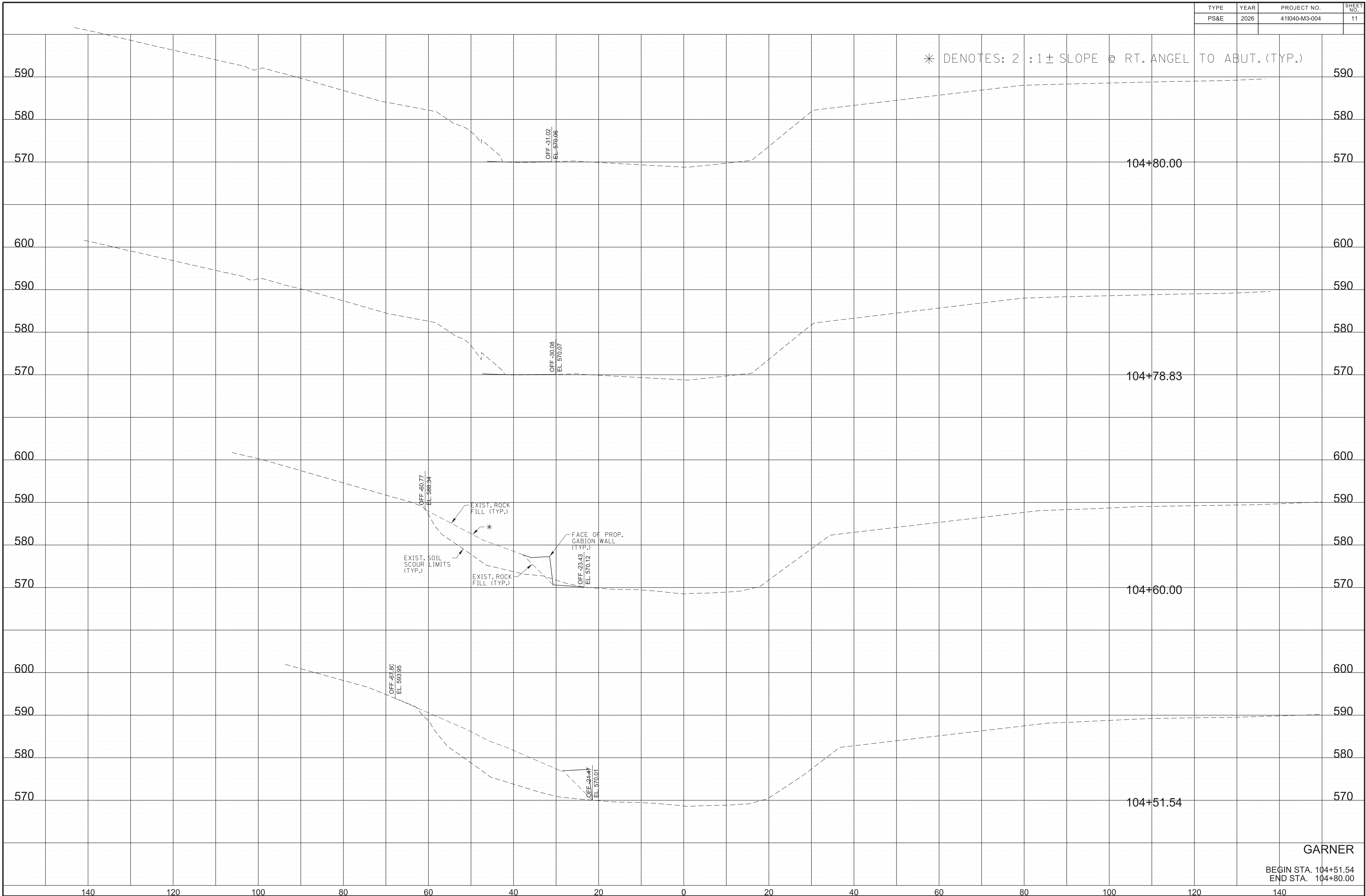
* DENOTES: 2 : 1 ± SLOPE @ RT. ANGLE TO ABUT. (TYP.)



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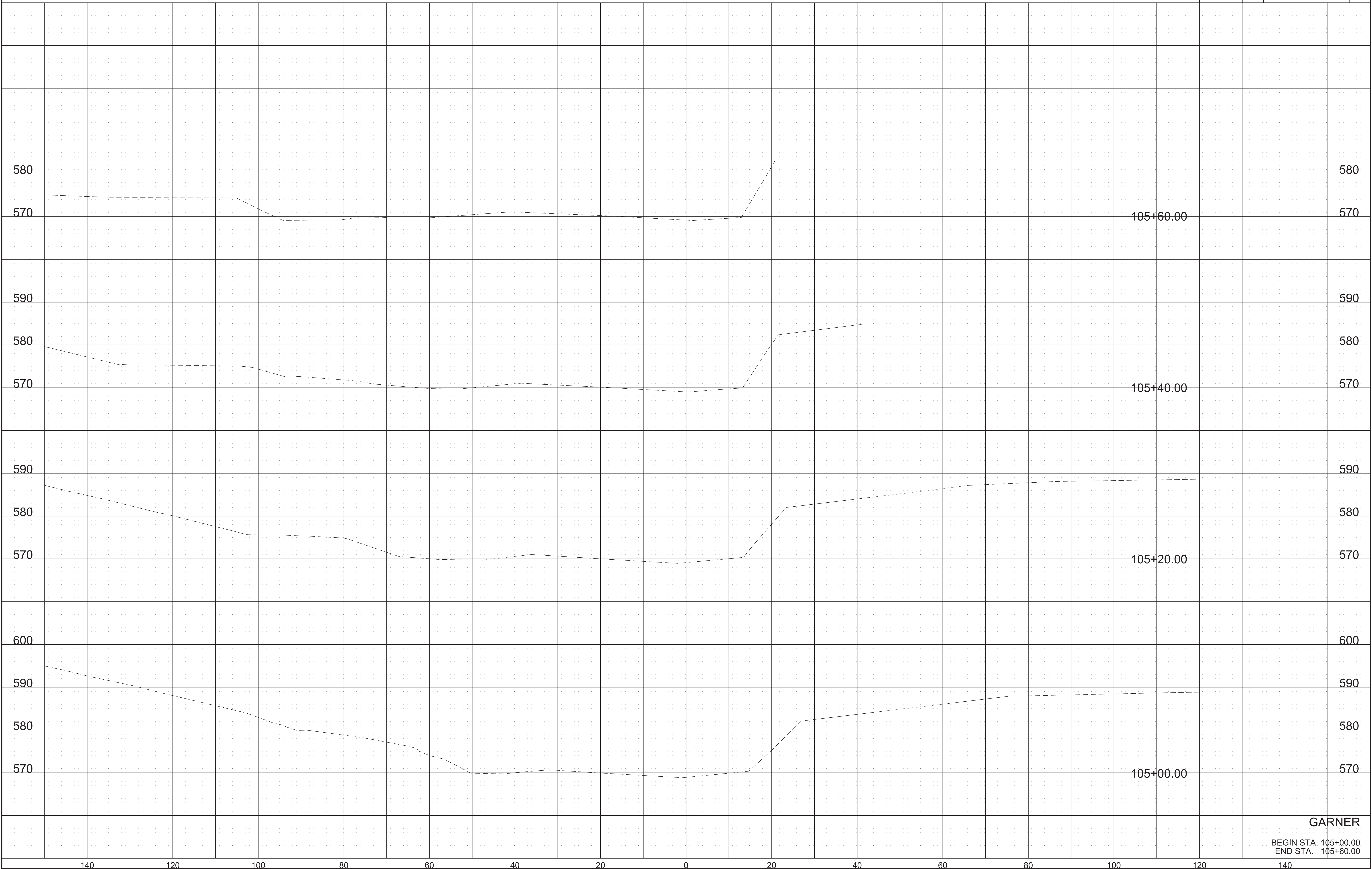
GARNER
 BEGIN STA. 103+80.00
 END STA. 104+40.00

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	11



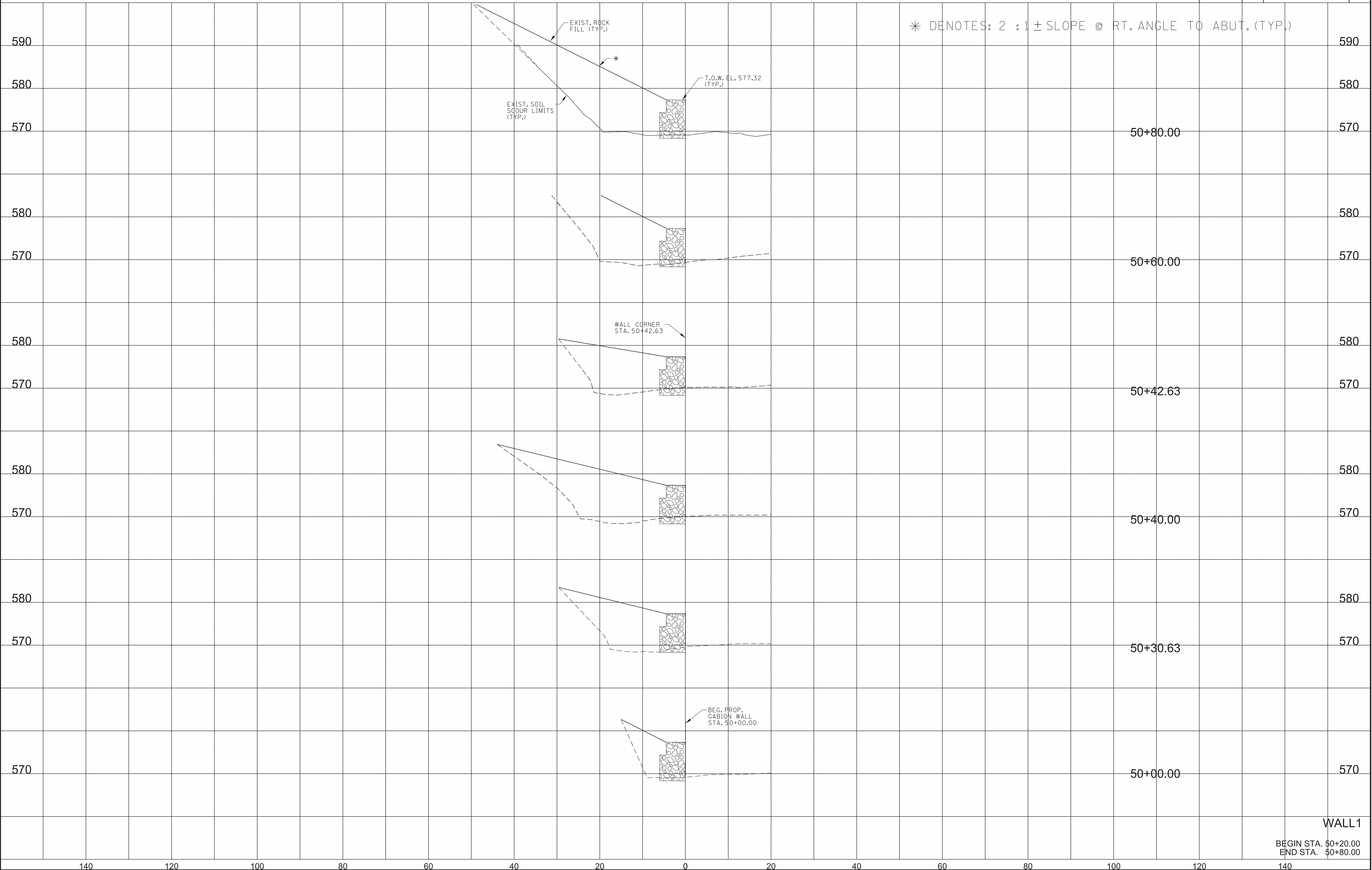
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TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	12



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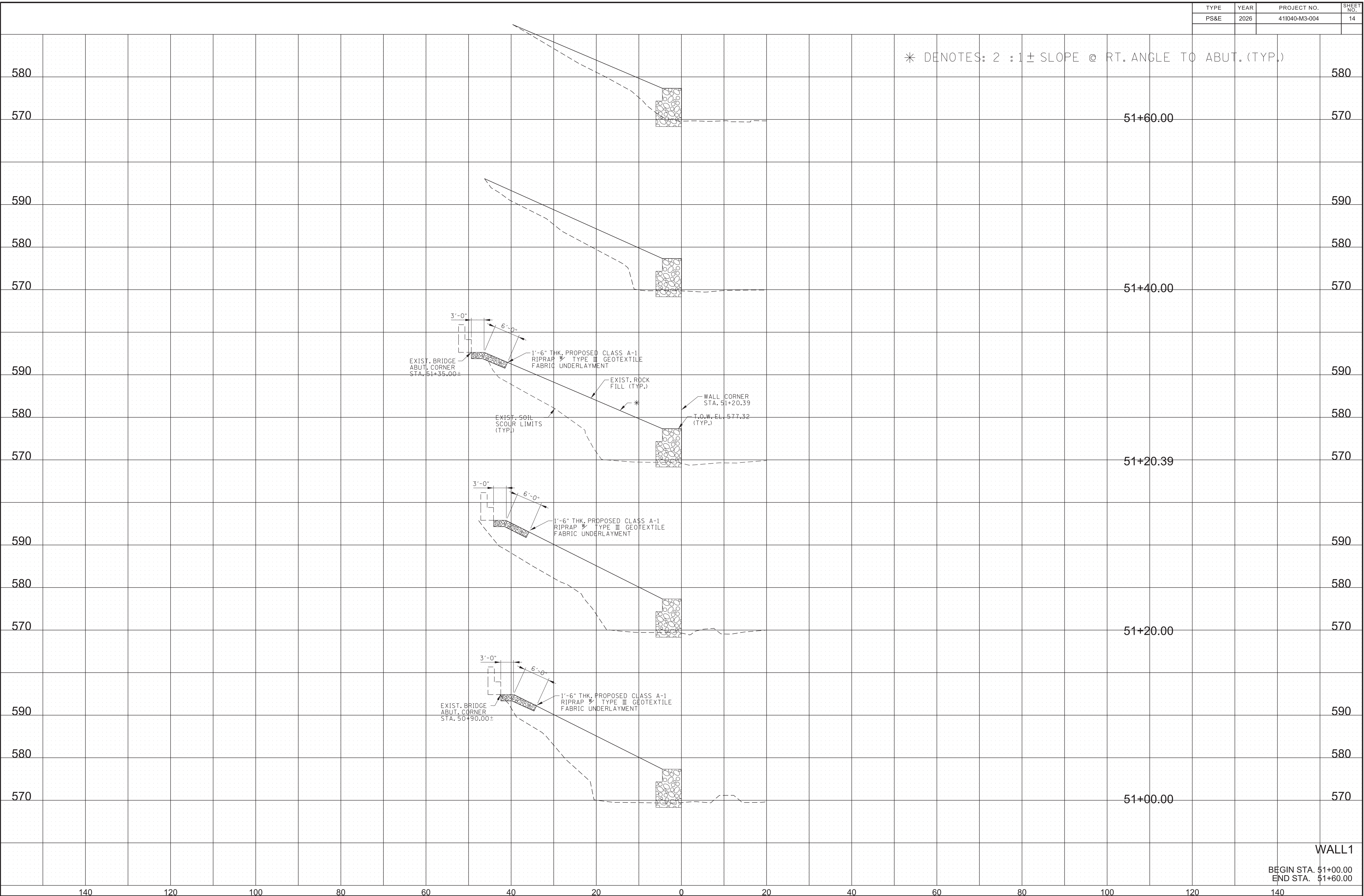
GARNER
 BEGIN STA. 105+00.00
 END STA. 105+60.00



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WALL1
 BEGIN STA. 50+20.00
 END STA. 50+80.00

* DENOTES: 2 : 1 ± SLOPE @ RT. ANGLE TO ABUT. (TYP.)

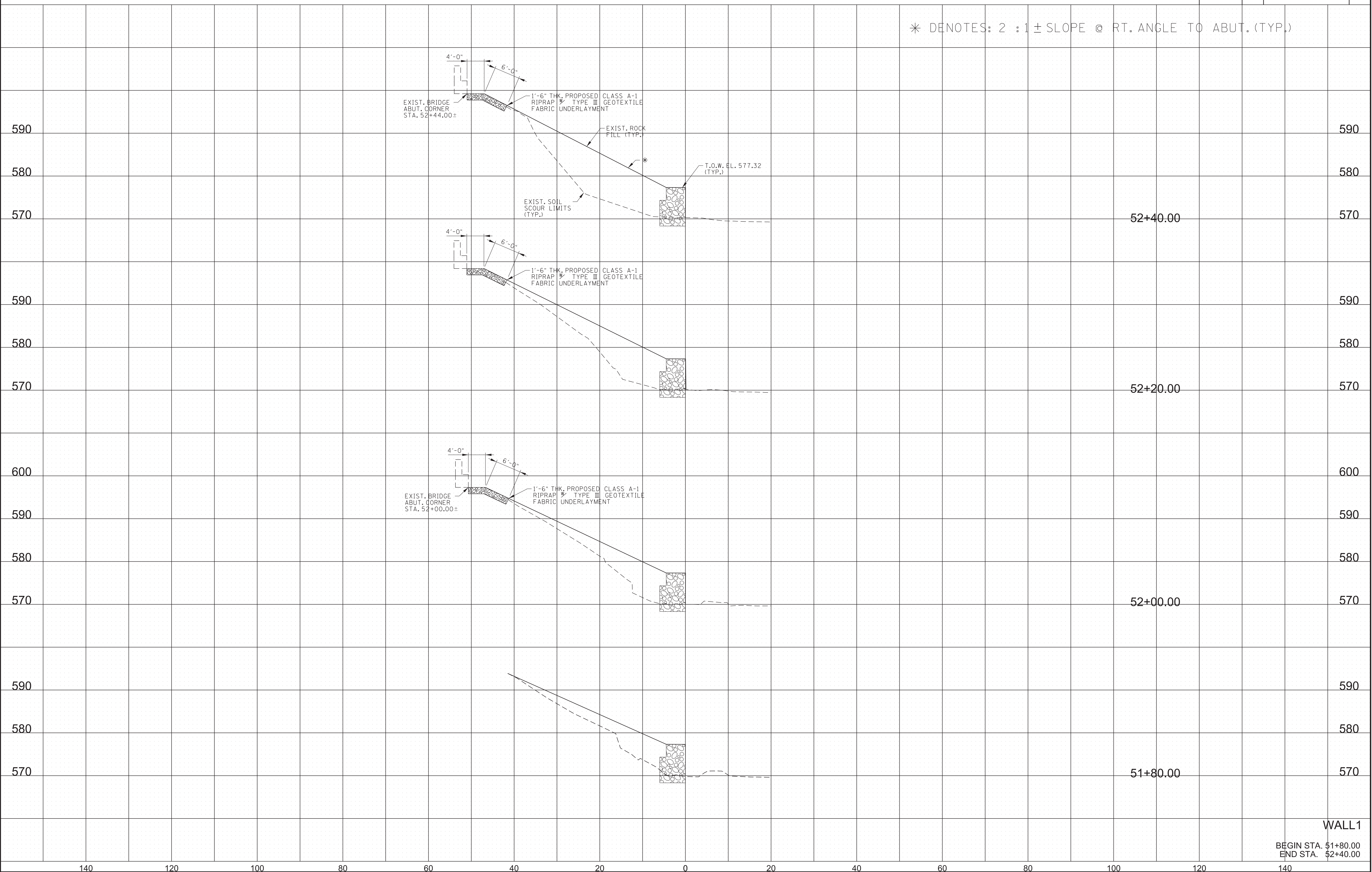


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WALL1

BEGIN STA. 51+00.00
END STA. 51+60.00

* DENOTES: 2 : 1 ± SLOPE @ RT. ANGLE TO ABUT. (TYP.)

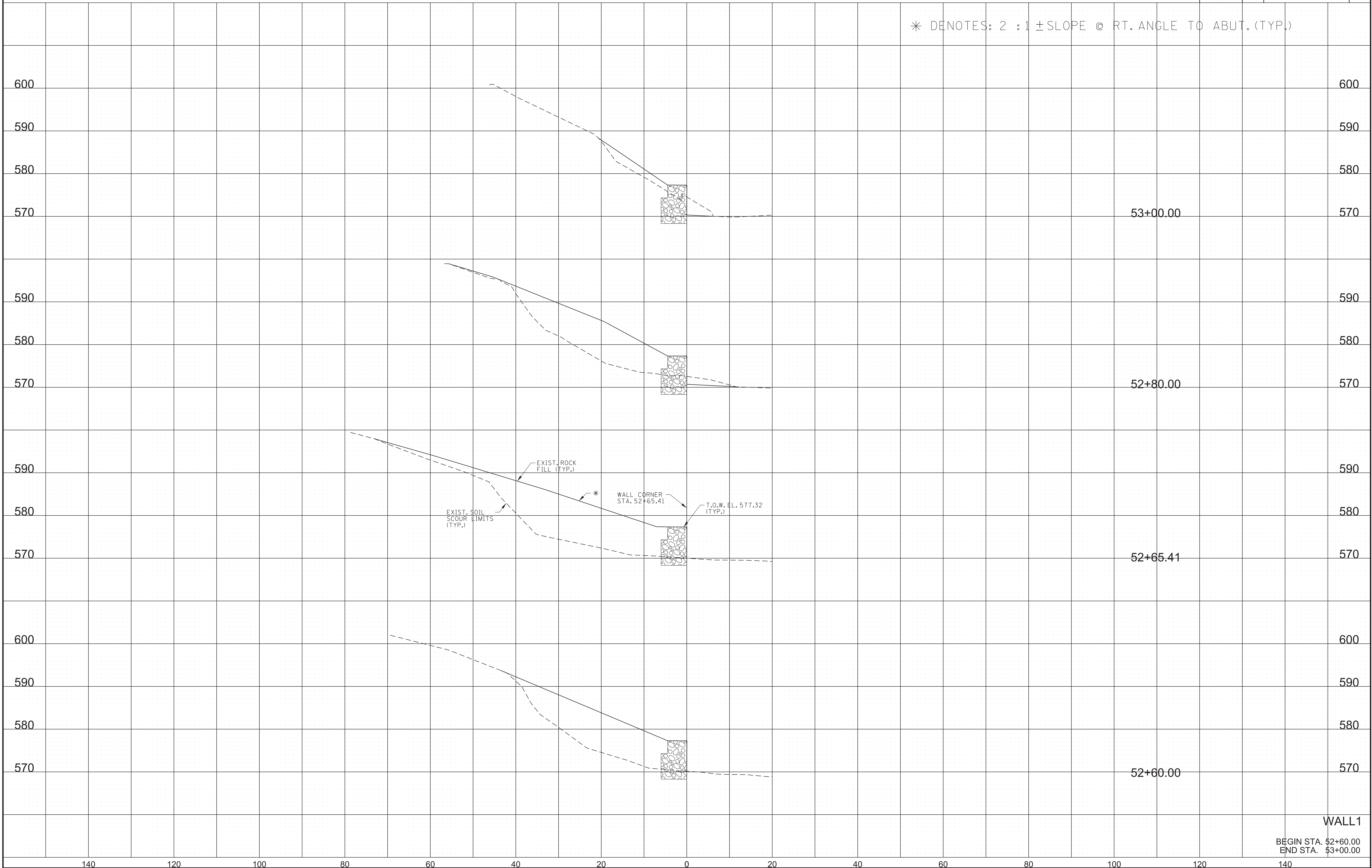


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WALL1

BEGIN STA. 51+80.00
 END STA. 52+40.00

* DENOTES: 2 : 1 ± SLOPE @ RT. ANGLE TO ABUT. (TYP.)

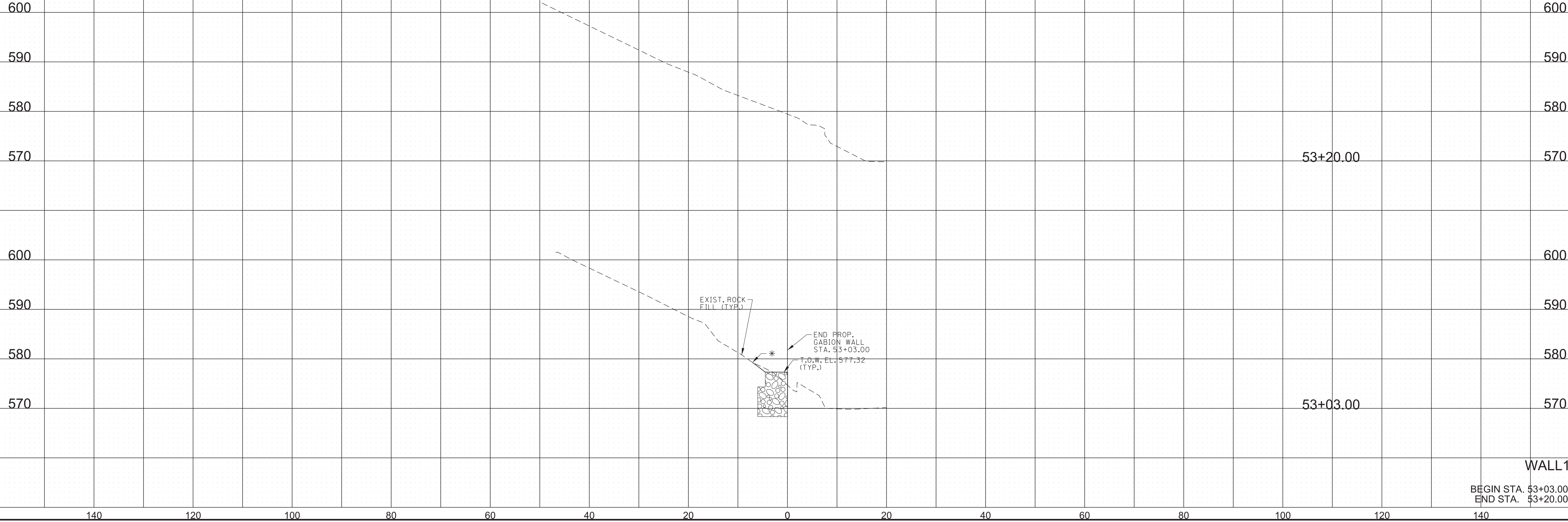


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WALL1
 BEGIN STA. 52+60.00
 END STA. 53+00.00

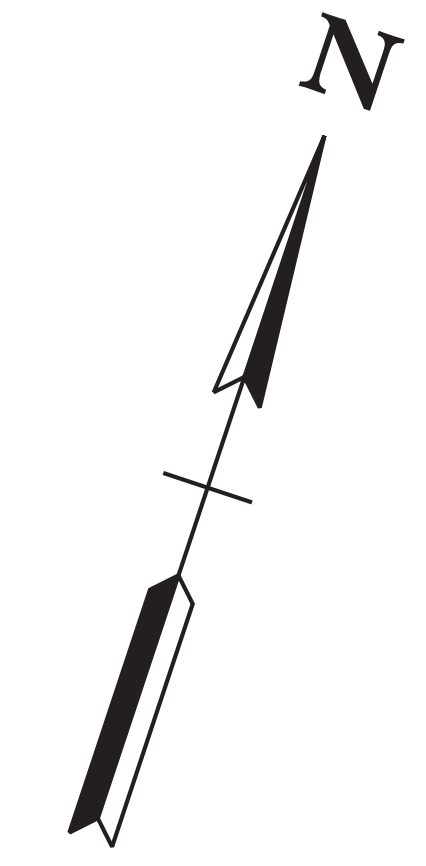
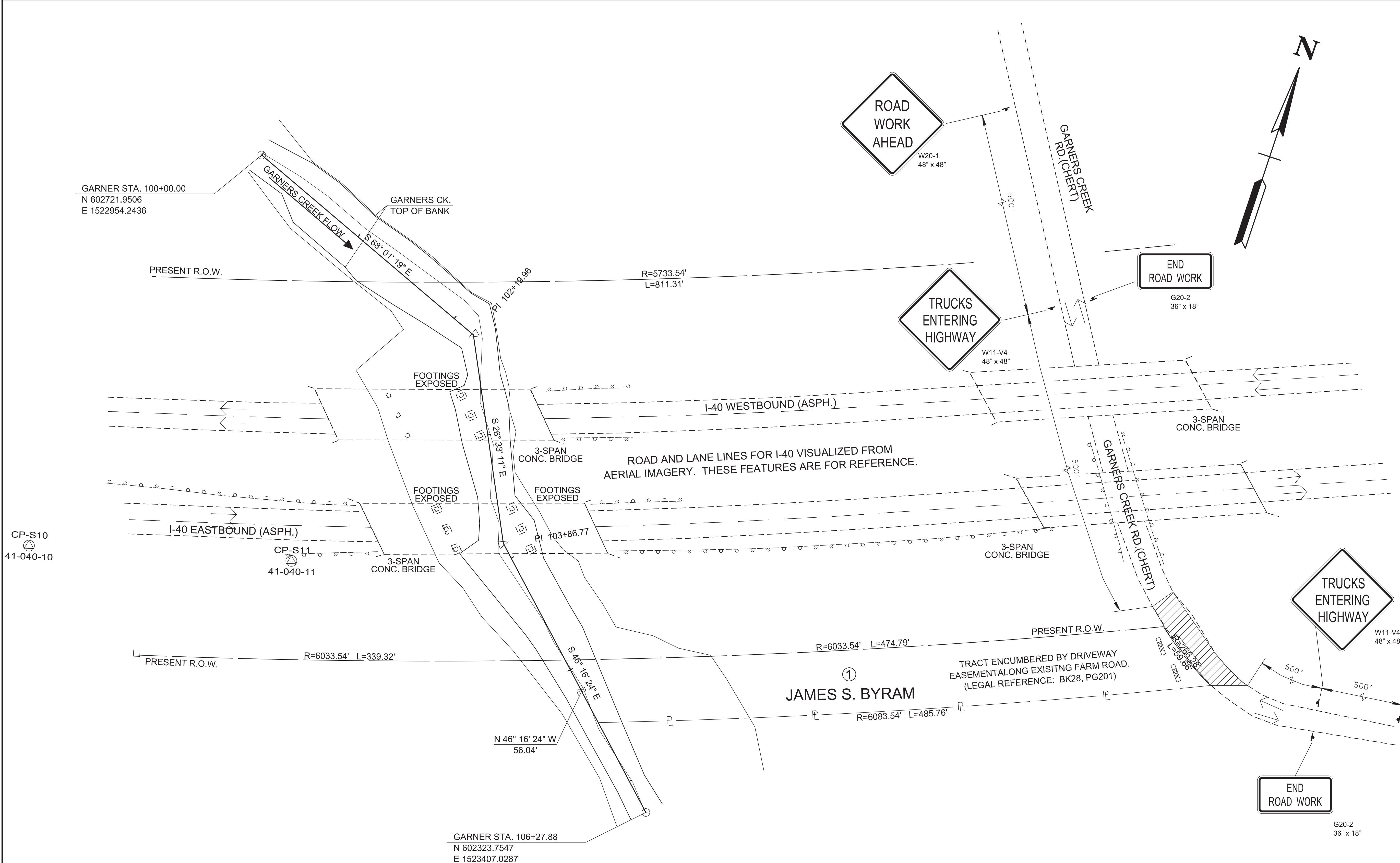
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PS&E	2026	411040-M3-004	17

* DENOTES: 2 : 1 ± SLOPE @ RT. ANGLE TO ABUT. (TYP.)



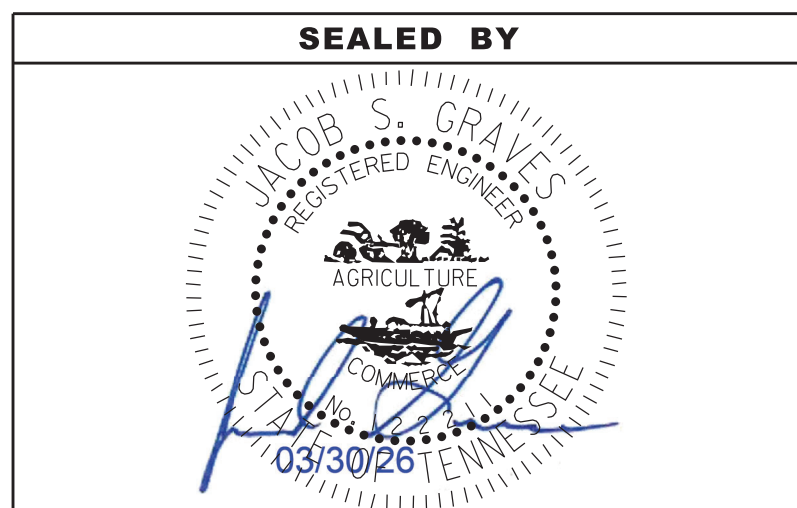
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TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	T1



CP-S10
41-040-10

CP-S11
41-040-11



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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS

GARNERS CK. RD.
SCALE: 1"= 40'

TRAFFIC CONTROL SIGNS

M.U.T.C.D. NO.	DESCRIPTION	SIZE	NO.	ITEM NO. 712-06 (S.F)
W20-1	ROAD WORK AHEAD	48" X 48"	2	32
W11-V4	TRUCKS ENTERING HIGHWAY	48" X 48"	2	32
G20-2	END ROAD WORK	36" X 18"	2	9
				73

TRAFFIC CONTROL LEGEND

SYMBOL	ITEM
	WORK ZONE
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	TEMPORARY BARRICADE (TYPE III)

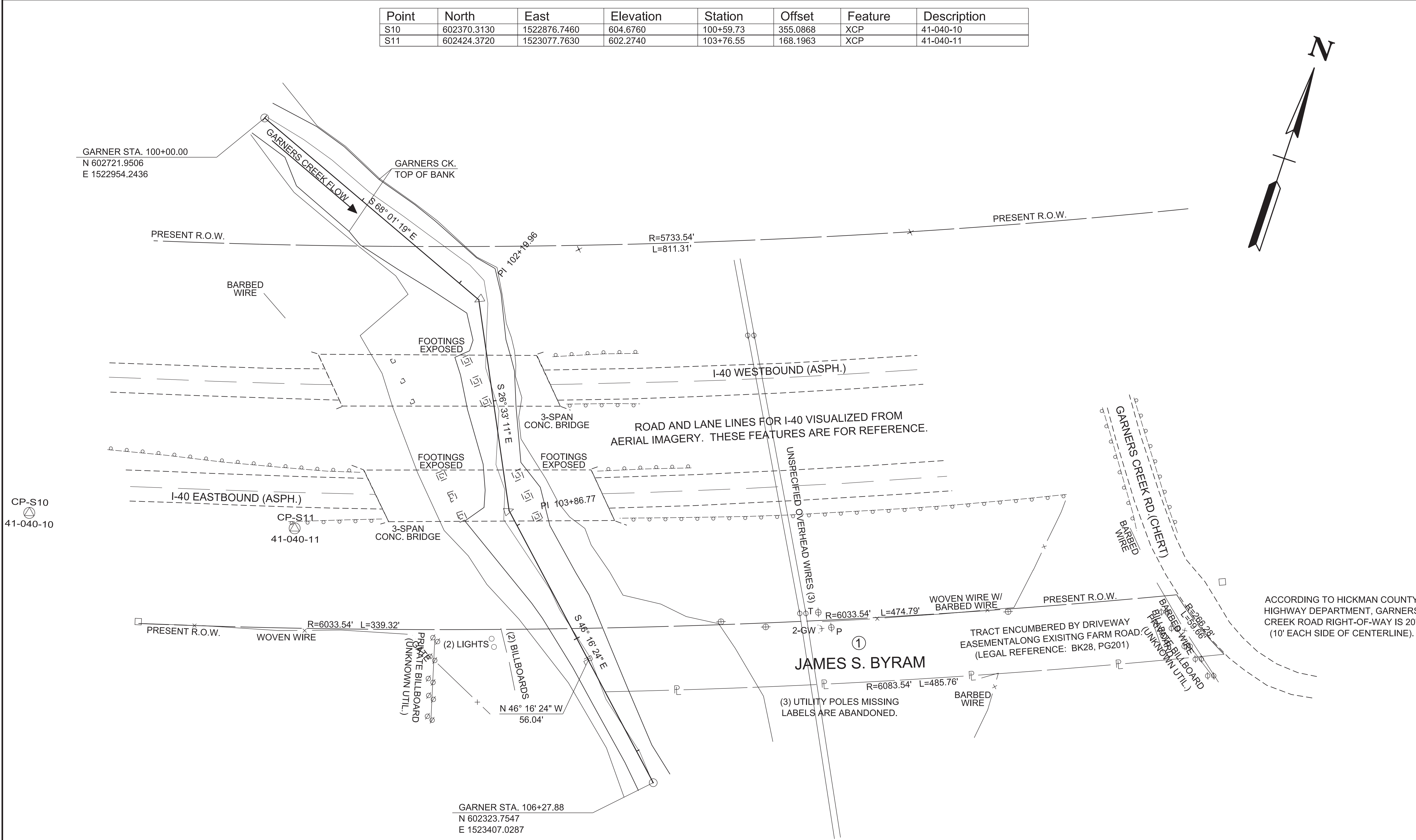
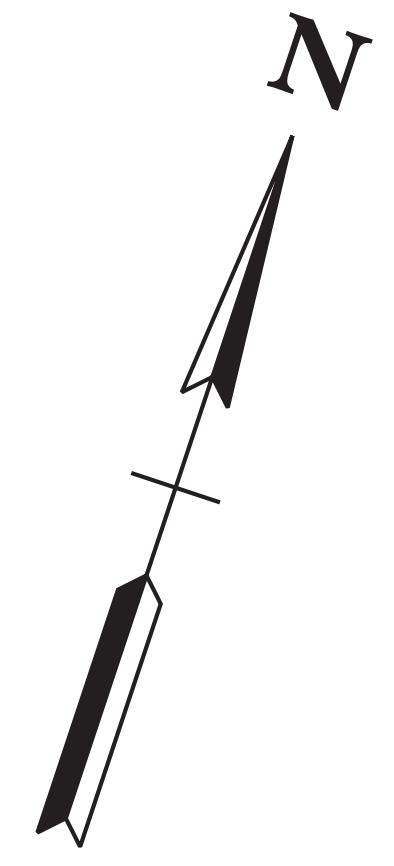
TRAFFIC CONTROL QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY
712-01	TRAFFIC CONTROL	LS	1
712-06	SIGNS (CONSTRUCTION)	S.F.	73
712-07.03	TEMPORARY BARRICADE TYPE III	L.F.	20

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Point	North	East	Elevation	Station	Offset	Feature	Description
S10	602370.3130	1522876.7460	604.6760	100+59.73	355.0868	XCP	41-040-10
S11	602424.3720	1523077.7630	602.2740	103+76.55	168.1963	XCP	41-040-11

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	411040-M3-004	U1-1



ACCORDING TO HICKMAN COUNTY HIGHWAY DEPARTMENT, GARNERS CREEK ROAD RIGHT-OF-WAY IS 20'. (10' EACH SIDE OF CENTERLINE).

TRACT ENCUMBERED BY DRIVEWAY EASEMENT ALONG EXISTING FARM ROAD (LEGAL REFERENCE: BK28, PG201)

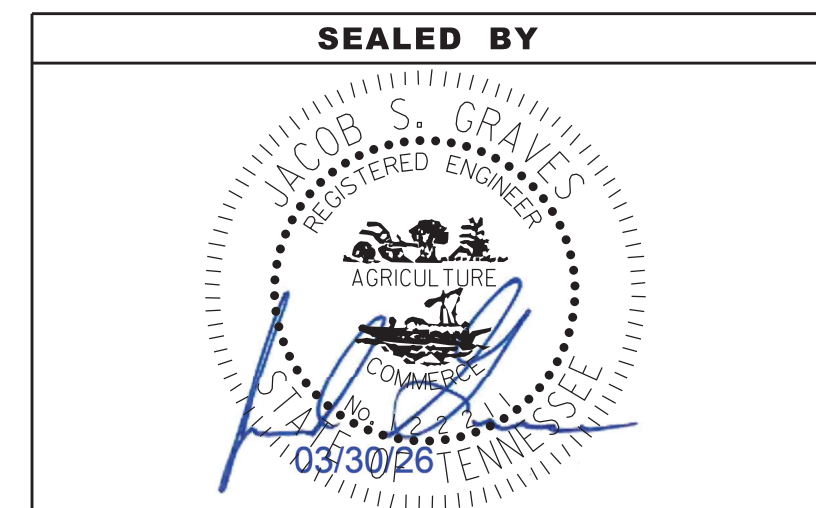
JAMES S. BYRAM
 (3) UTILITY POLES MISSING LABELS ARE ABANDONED.

GARNER STA. 106+27.88
 N 602323.7547
 E 1523407.0287

CP-S10
 41-040-10

CP-S11
 41-040-11

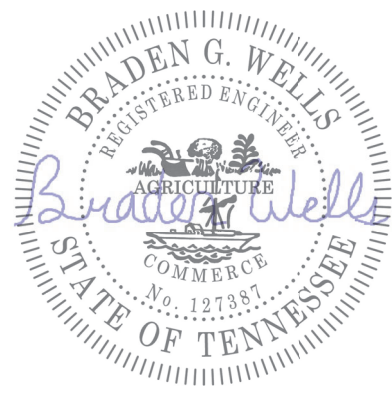
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COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00008 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 18B.

**STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION**

EXISTING UTILITIES
 STA. 101+63 TO STA. 104+77
 SCALE: 1"= 40'



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:
Braden Wells Date: 2026.04.09 13:02:41 -05'00'

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

GRESHAM SMITH
222 2ND AVE SOUTH
NASHVILLE, TN 37201
BRADEN WELLS, P.E. NO. 127387

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

DWG. NAME	DWG. NO.
SIGNATURE SHEET	BRIDGE - SIGN1
ESTIMATED BRIDGE QUANTITIES	BR-132-332
BRIDGE GENERAL NOTES	BR-132-333
LAYOUT OF BRIDGE TO BE REPAIRED	BR-132-334
GABION WALL DETAILS AND NOTES	BR-132-335

YEAR	PROJECT NO.	SHEET NO.
2026	411040-3-004	BRIDGE - SIGN 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

CONST. NO.:

PROJECT NO.	YEAR	SHEET NO.	
411040-M3-004	2026		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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-	-	-	-
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-	-	-	-

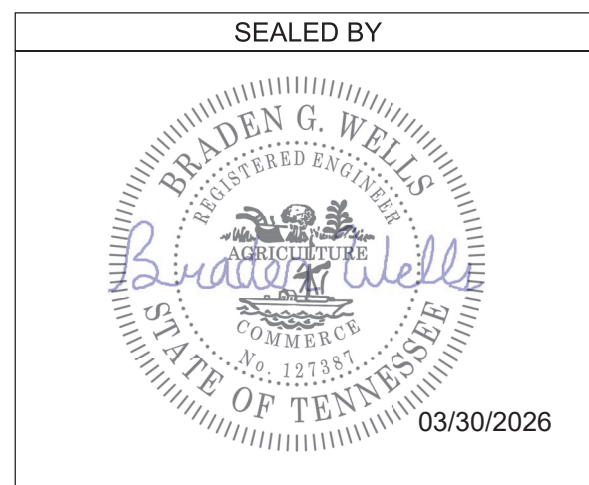
ESTIMATED BRIDGE QUANTITIES				
FOOTNOTE	ITEM NO.	DESCRIPTION	UNIT	411040-3-004 QUANTITY
(1)	203-08	CHANNEL EXCAVATION (UNCLASSIFIED)	C. Y.	256
(2)	303-01.03	GRANULAR BACKFILL (RETAINING WALLS)	TON	1729
(3)	709-05.06	MACHINED RIP-RAP (CLASS A-1)	TON	88
(4)	709-10.01	GABIONS (GARNERS CK, 9'-0" X 6'-0", ABUT. NO. 2)	C. Y.	553
(5)	740-10.03	GEOTEXTILE (TYPE III) (EROSION CONTROL)	S. Y.	1184

FOOTNOTES:

- (1) ITEM NO. INCLUDES COSTS OF ALL LABOR, MATERIALS, AND DISPOSAL OF STREAM BED SEDIMENT TO EXCAVATE STREAMBED TO APPROX. EL. 571.00 OR 1'-0" ABOVE WATER LEVEL AS SHOWN ON DWG. NO. BR-132-334 AND SHEETS 6-9.
- (2) ITEM NO. TO INCLUDES COSTS OF ALL LABOR AND MATERIAL TO PLACE GRANULAR BACKFILL BEHIND RETAINING WALL TO THE LIMITS SHOWN AND ACCORDING TO DETAILS AND NOTES ON DWG. NO. BR-132-335.
- (3) ITEM NO. TO INCLUDES COSTS OF ALL LABOR AND MATERIAL TO PLACE MACHINED RIP-RAP NEAR ABUTMENT NO. 2 OF THE LEFT AND RIGHT LANE BRIDGES WITHIN THE BRIDGE WIDTH LIMITS AND ACCORDING TO DETAILS AND NOTES ON DWG. NO. BR-132-335.
- (4) ITEM NO. TO INCLUDE COSTS OF ALL LABOR AND MATERIALS TO EXCAVATE EXISTING RIP-RAP SLOPE TO THE NECESSARY LIMITS, CONSTRUCT THE GABION BASKET WALL SECTIONS, AND REPLACE EXISTING RIP-RAP SLOPE TO THE NECESSARY POSITION BEHIND THE GABION BASKETS ACCORDING TO DETAILS ON DWG. BR-132-335 AND SHEET NOS. 13-17.
- (5) ITEM NO. INCLUDES COSTS OF ALL LABOR AND MATERIALS TO PLACE GEOTEXTILE FABRIC AROUND THE NEW GABION WALL, BACKFILL, AND NEAR ABUTMENT NO. 2 (LL & RL) AS DETAILED ON DWG. NO. BR-132-335.

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PIN NO.: 133334.00
 DESIGN BY: GRESHAM SMITH DATE: 11/2022
 DRAWN BY: B. WELLS DATE: 11/2022
 SUPERVISED BY: R. LOVETT DATE: 11/2022
 CHECKED BY: R. LOVETT DATE: 11/2022



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 ESTIMATED BRIDGE
 QUANTITIES
 INTERSTATE 40
 OVER GARNERS CK.
 BR. NO. 41-10040-1.53 (LL&RL)
 FED. ID. NO. 41100400027
 FED. ID. NO. 41100400028
 HICKMAN COUNTY
 2026

BR-132-332

GENERAL NOTES

SPECIFICATIONS & LOADING

- (1) **CONSTRUCTION SPECIFICATIONS:** STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (APRIL 1, 2026 EDITION), AND THE 4TH EDITION (2017) AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS WITH INTERIMS.
- (2) **DESIGN SPECIFICATIONS:** 10TH EDITION (2024) AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS WITH INTERIMS, AND THE 2ND EDITION (2011) AASHTO GUIDE SPECIFICATIONS FOR LRFD SEISMIC BRIDGE DESIGN WITH INTERIMS.

MISCELLANEOUS GENERAL NOTES

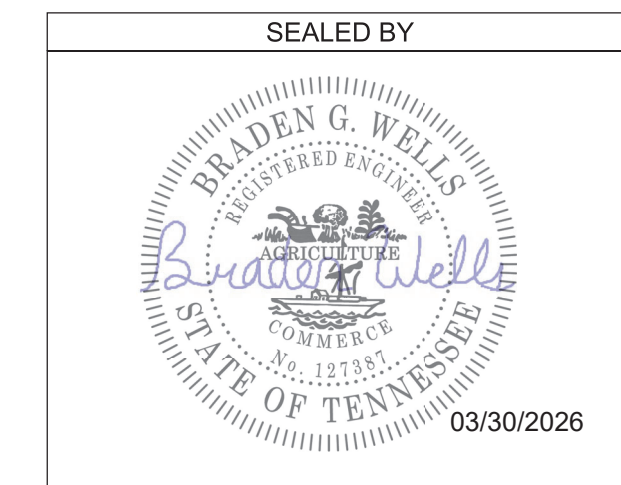
- (3) **SHOP DRAWINGS:** REFER TO SECTION 105.02 OF THE STANDARD SPECIFICATIONS. IF USING PAPER COPIES, SHOP DRAWINGS ARE TO BE SENT TO THE BRIDGE REPAIR OFFICE IN THE DIVISION OF STRUCTURES, FOR ELECTRONIC SUBMITTALS, SEE SECTION 105.02 OF THE STANDARD SPECIFICATIONS. EACH SHOP DRAWING SHALL CONTAIN IN THE TITLE BLOCK THE FOLLOWING: THE STATE PROJECT NUMBER, COUNTY, BRIDGE NAME, BRIDGE NUMBER (OR STRUCTURE TYPE AND NUMBER), STATION, AND CONTRACT NUMBER. SHOP DRAWINGS WITH TITLE BLOCKS NOT INCLUDING THE FOREGOING IDENTIFICATION WILL BE RETURNED FOR CORRECTION BEFORE ANY REVIEWS FOR APPROVAL ARE CONDUCTED.
- (4) **SPECIAL NOTE TO CONTRACTOR:** CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURES NECESSARY TO ENSURE THAT NO DEBRIS IS DROPPED INTO THE STREAM. ANY DEBRIS WHICH IS ALLOWED TO DROP ON THE BANKS BELOW THE BRIDGE SHALL NOT BE ALLOWED TO ENTER THE STREAM AND SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. COST OF REMOVAL AND DISPOSAL OF DEBRIS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR OTHER ITEMS.

CONST. NO.:

PROJECT NO.	YEAR	SHEET NO.	
411040-M3-004	2026		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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PIN NO.: 133334.00
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 DRAWN BY: B. WELLS DATE: 11/2022
 SUPERVISED BY: R. LOVETT DATE: 11/2022
 CHECKED BY: R. LOVETT DATE: 11/2022



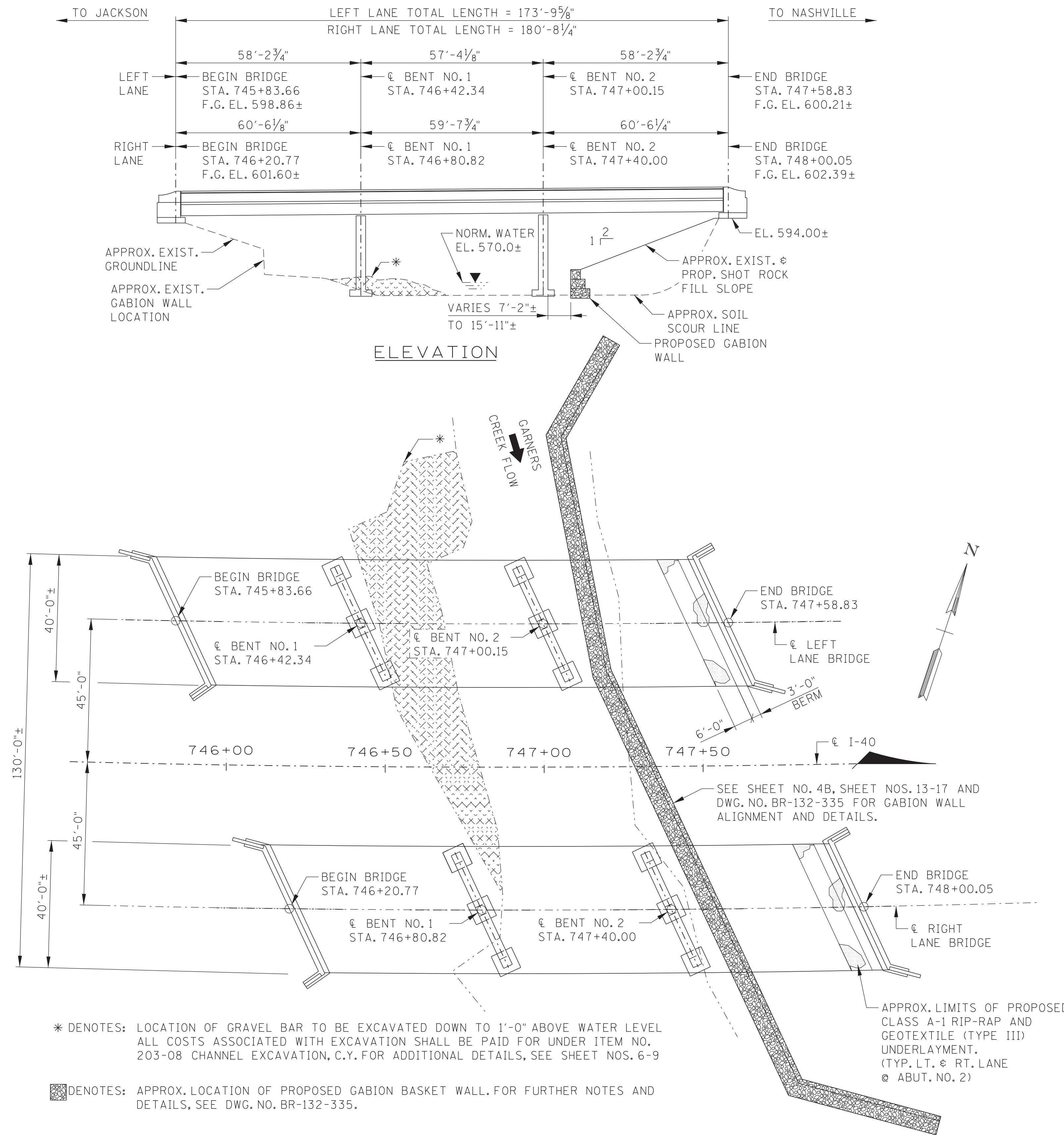
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BRIDGE GENERAL
 NOTES
 INTERSTATE 40
 OVER GARNERS CK.
 BR. NO. 41-10040-1.53 (LL&RL)
 FED. ID. NO. 41100400027
 FED. ID. NO. 41100400028
 HICKMAN COUNTY
 2026

BR-132-333

CONST. NO.:

PROJECT NO.	YEAR	SHEET NO.
411040-M3-004	2026	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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* DENOTES: LOCATION OF GRAVEL BAR TO BE EXCAVATED DOWN TO 1'-0" ABOVE WATER LEVEL. ALL COSTS ASSOCIATED WITH EXCAVATION SHALL BE PAID FOR UNDER ITEM NO. 203-08 CHANNEL EXCAVATION, C.Y. FOR ADDITIONAL DETAILS, SEE SHEET NOS. 6-9

■ DENOTES: APPROX. LOCATION OF PROPOSED GABION BASKET WALL. FOR FURTHER NOTES AND DETAILS, SEE DWG. NO. BR-132-335.

GENERAL SCOPE OF WORK:

1. PROVIDE TRAFFIC CONTROL SIGNING ON GARNERS CREEK ROAD AS SHOWN ON SHEET T1.
2. INSTALL EROSION PREVENTION & SEDIMENT CONTROL (EPSC) MEASURES AS SHOWN ON SHEET 5.
3. CONSTRUCT HAUL ROAD ON EXISTING EASEMENT AS SHOWN ON SHEET 4A.
4. EXCAVATE TOE OF EXISTING SHOT ROCK FILL SLOPE IN 40'-0" LONG MAX SECTIONS PARALLEL TO GARNERS CREEK. STOCKPILE THE EXCAVATED ROCK TO BE USED FOR BACKFILLING THE PROPOSED GABION WALL. PROTECT ANY EXPOSED EXISTING GEOTEXTILE FABRIC AND REINSTALL.
5. INSTALL GABION WALL AND GEOTEXTILE FABRIC SECTIONS AS DETAILED ON DWG. NO. BR-132-334 & BR-132-335 & SHEET NOS. 13-17.
6. BACKFILL EACH WALL SECTION ACCORDING TO DETAILS ON DWG. NO. BR-132-335 WHILE PROTECTING THE NEW GEOTEXTILE FABRIC FROM DAMAGE.
7. INSTALL AND BACKFILL EACH WALL SECTION IN SEQUENCE BEFORE BEGINNING NEXT WALL SECTION.
8. INSTALL ADDITIONAL CLASS A-1 RIP-RAP AND GEOTEXTILE FABRIC IN FRONT OF EACH ABUTMENT AT THE TOP OF THE EXISTING SLOPE AS REQUIRED. SEE DETAIL ON DWG. NO. BR-132-335.
9. EXCAVATE EXISTING STREAMBED MATERIAL AREA AS SHOWN ON DWG. NO. BR-132-334 & SHEET NO. 4A, AND ACCORDING TO THE FOOTNOTES ON DWG. NO. BR-132-332.
10. REMOVE EPSC MEASURES AND RESTORE DISTURBED AREAS TO EXISTING CONDITIONS.

LIST OF DRAWINGS

	DWG. NO.	LAST REV. DATE
ESTIMATED BRIDGE QUANTITIES	BR-132-332	
BRIDGE GENERAL NOTES	BR-132-333	
LAYOUT OF BRIDGE TO BE REPAIRED	BR-132-334	
GABION WALL DETAILS AND NOTES	BR-132-335	

LIST OF STANDARD DRAWINGS

TITLE	DWG. NO.	REV. DATE
GABION ASSEMBLY DETAILS	EC-STR-57	04-01-2008
GABION ASSEMBLY DETAILS	EC-STR-58	04-01-2008

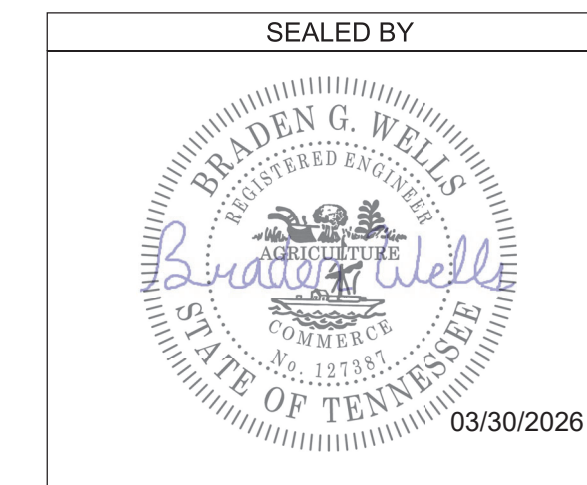
LIST OF REFERENCE DRAWINGS

(TO BE PRINTED WITH PLANS)

DRAWING NO.	ORIGINAL BRIDGE DRAWINGS	EXISTING BRIDGE PLANS	EXISTING BRIDGE REPAIR PLANS
K-31-43 THRU K-31-49			
BR-111-19 THRU BR-111-46			

LIST OF SPECIAL PROVISIONS

NO.	REGARDING	REV. DATE
SP624	RETAINING WALLS	10-07-24



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

LAYOUT OF BRIDGE
TO BE REPAIRED
INTERSTATE 40
OVER GARNERS CK.
BR. NO. 41-10040-1.53 (LL&RL)
FED. ID. NO. 41100400027
FED. ID. NO. 41100400028
HICKMAN COUNTY
2026

BR-132-334

3/30/2026 11:40:21 AM

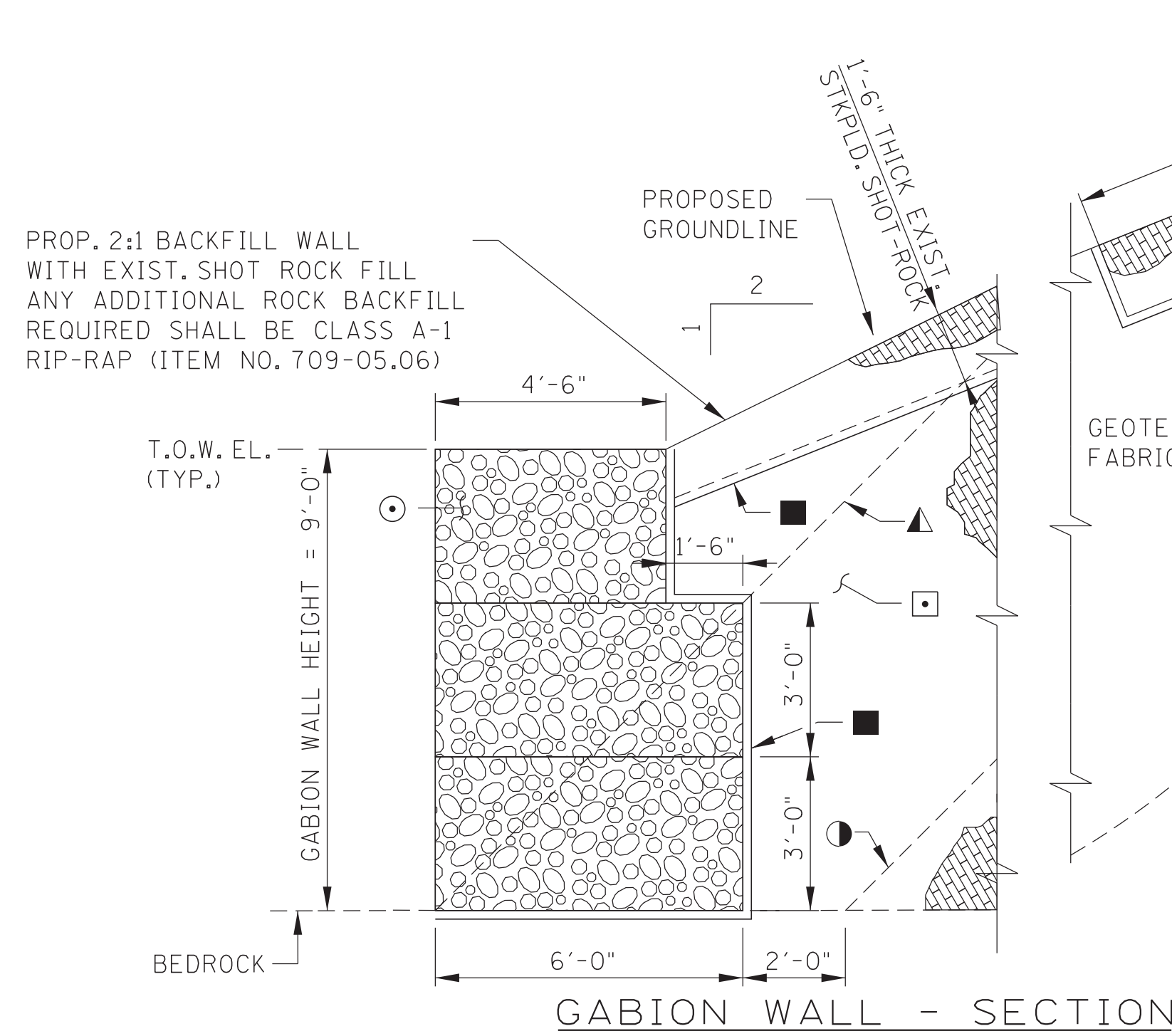
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PIN NO.:	133334.00
DESIGN BY:	GRESHAM SMITH
DRAWN BY:	B. WELLS
SUPERVISED BY:	R. LOVETT
CHECKED BY:	R. LOVETT

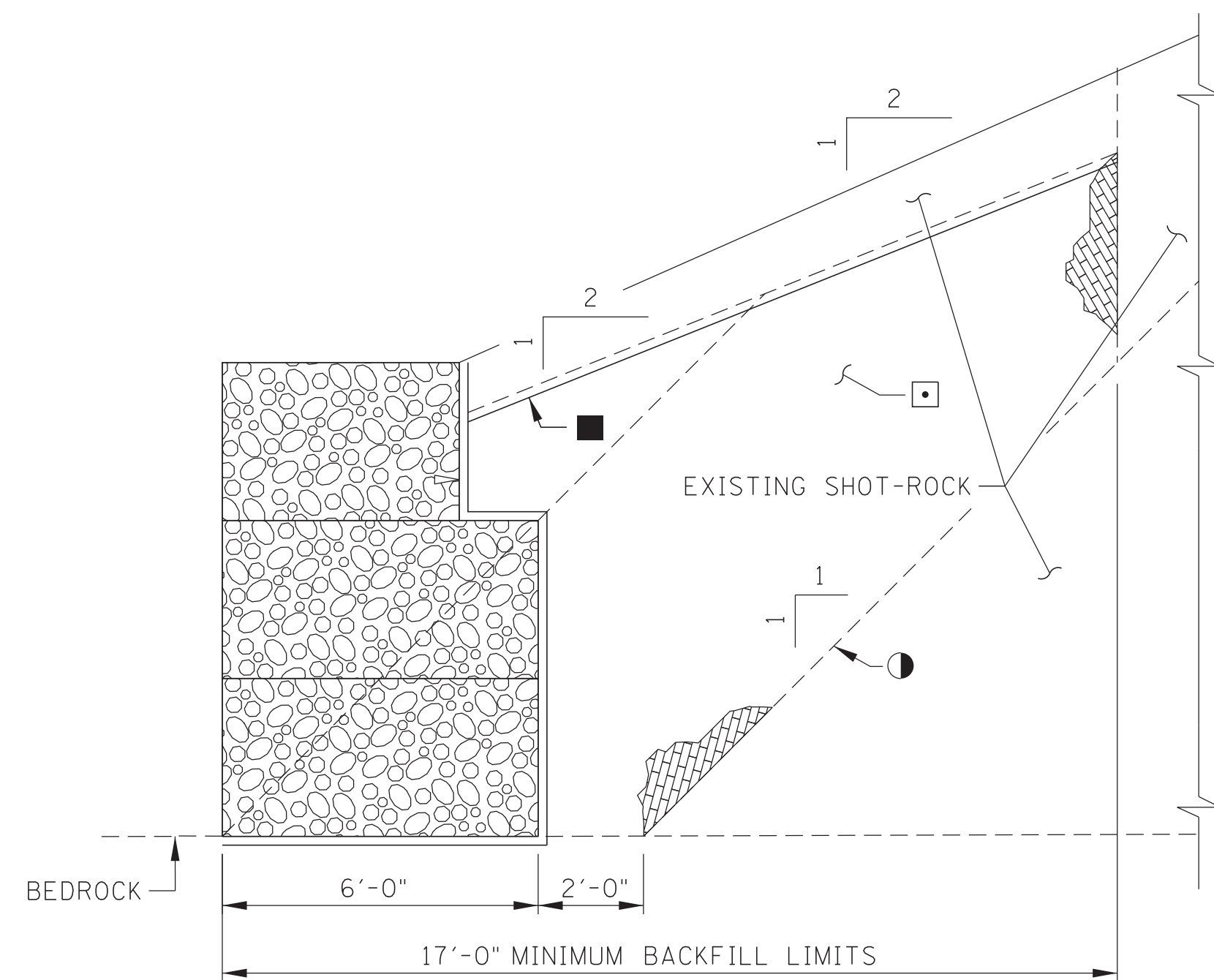


CONST. NO.:

PROJECT NO.	YEAR	SHEET NO.	
411040-M3-004	2026		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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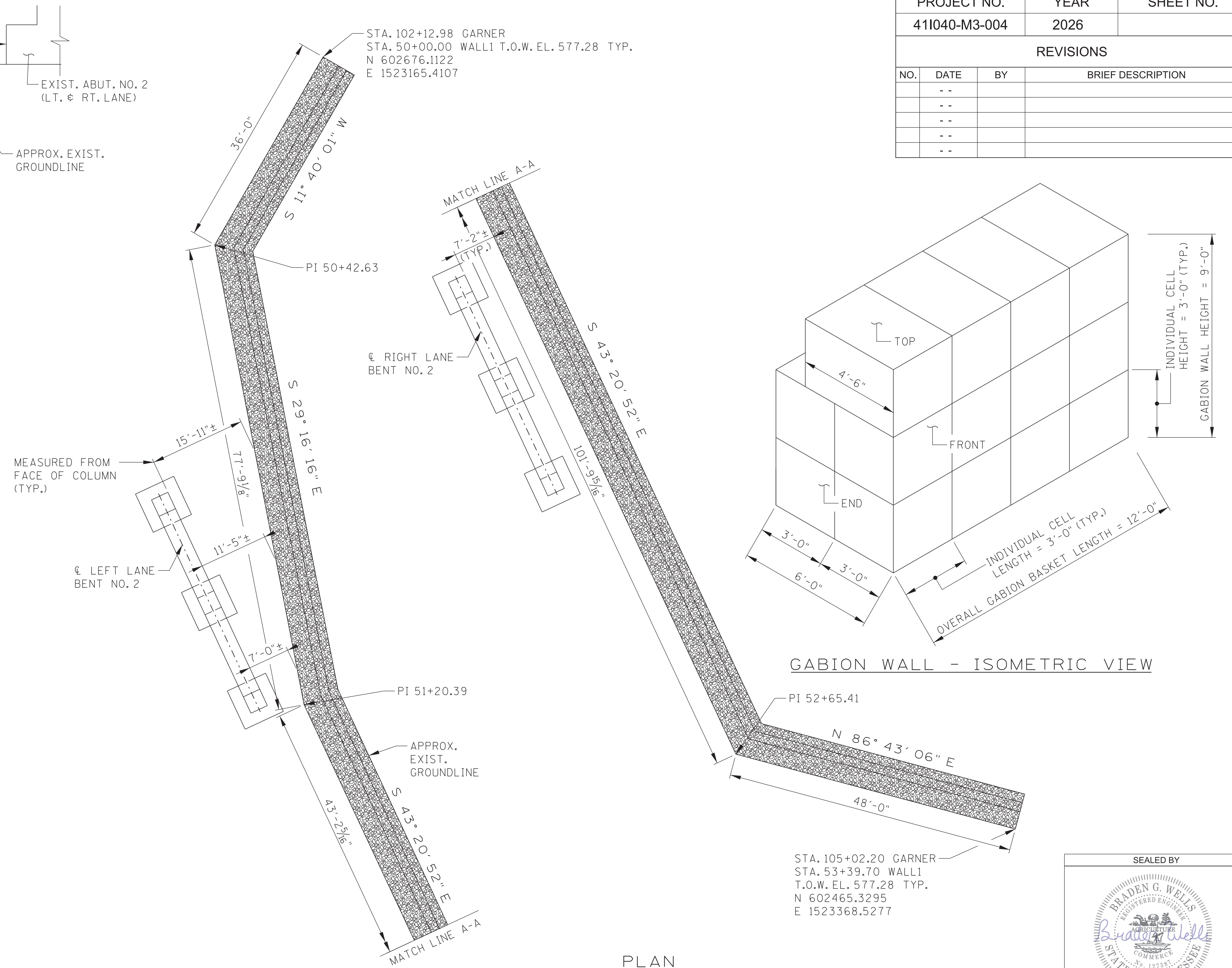


GABION WALL - SECTION

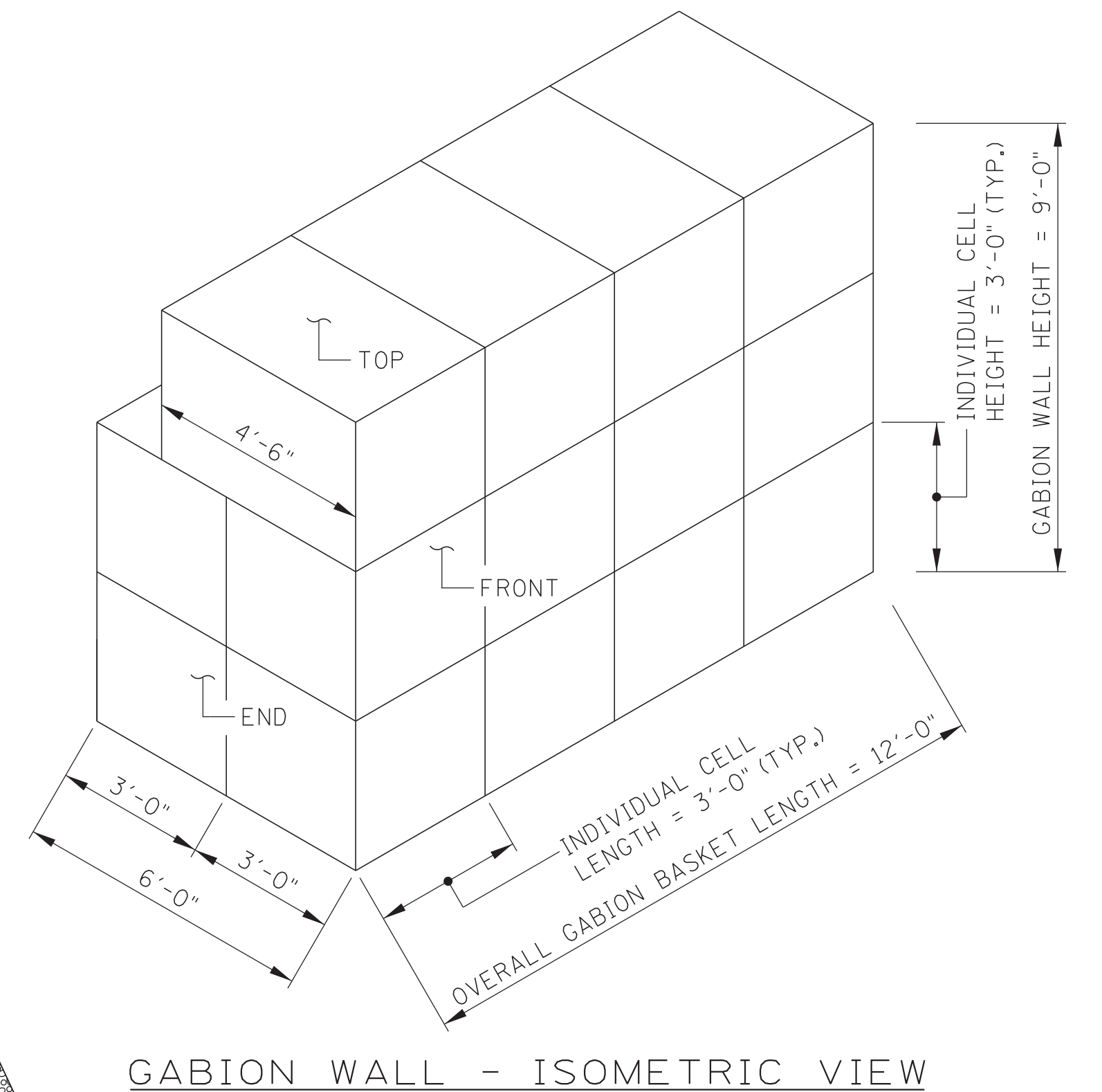


GABION WALL - BACKFILL LIMITS

NOTE: EXISTING SHOT-ROCK SLOPE PRIOR TO EXCAVATION NOT SHOWN FOR CLARITY



PLAN

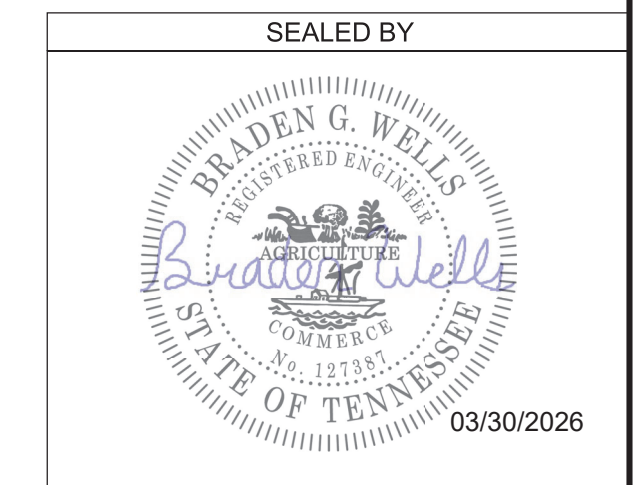


GABION WALL - ISOMETRIC VIEW

NOTES:

- FOR NOTES AND DETAILS REGARDING BASKET CONNECTIONS AND DIAPHRAGMS, SEE TDOT STD. DWG. EC-STR-57 AND EC-STR-58
- DENOTES: MACHINED RIP-RAP (CLASS A-1), SHALL BE USED AS FILL FOR THE GABION BASKETS. MACHINED RIP-RAP TO FILL GABION BASKETS SHALL BE PAID FOR UNDER ITEM NO. 709-10.01 GABIONS, C.Y.
- DENOTES: GRANULAR BACKFILL MATERIAL AND PLACEMENT BEHIND GABION BASKETS SHALL HAVE AN ANGLE OF INTERNAL FRICTION GREATER THAN OR EQUAL TO 35° AND A UNIT WEIGHT LESS THAN OR EQUAL TO 135 PCF. ALL LABOR AND MATERIALS TO PLACE GRANULAR BACKFILL SHALL BE PAID FOR UNDER ITEM NO. 303-01.03, GRANULAR BACKFILL (RETAINING WALLS), TON.
- DENOTES: GEOTEXTILE FABRIC SHALL LINE THE ENTIRE BOTTOM AND BACK FACE OF THE GABION BASKETS. SHALL ALSO LINE BOTTOM OF RIP-RAP PLACED ON TOP OF BACKFILL BEHIND GABION BASKETS, AND BOTTOM OF RIP-RAP PLACED NEAR ABUTMENTS AS SHOWN. ALL COSTS ASSOCIATED WITH THE PLACEMENT OF THE GEOTEXTILE FABRIC SHALL BE PAID FOR UNDER ITEM NO. 740-10.03 GEOTEXTILE (TYPE III) (EROSION CONTROL), SQ. YD.
- ▲ DENOTES: APPROX. 1:1± EXIST. SHOT ROCK ABUT. FILL SLOPE
- DENOTES: EXCAVATE EXIST. ROCK SLOPE AS REQ'D TO INSTALL GABION BASKETS

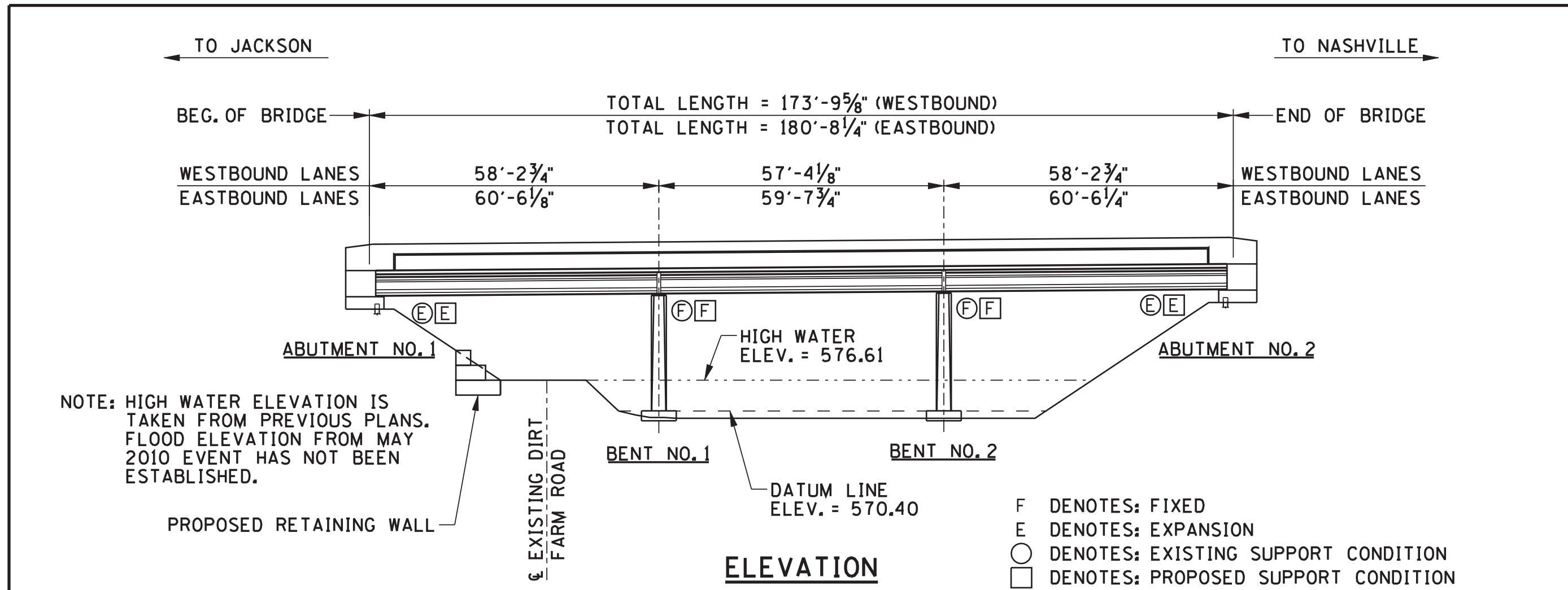
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 SUPERVISED BY: R. LOVETT DATE: 11/2022
 CHECKED BY: R. LOVETT DATE: 11/2022



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

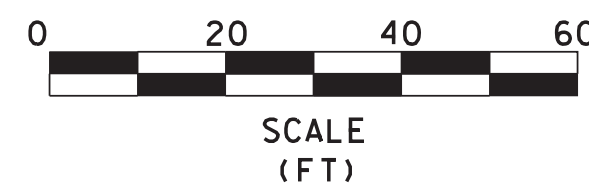
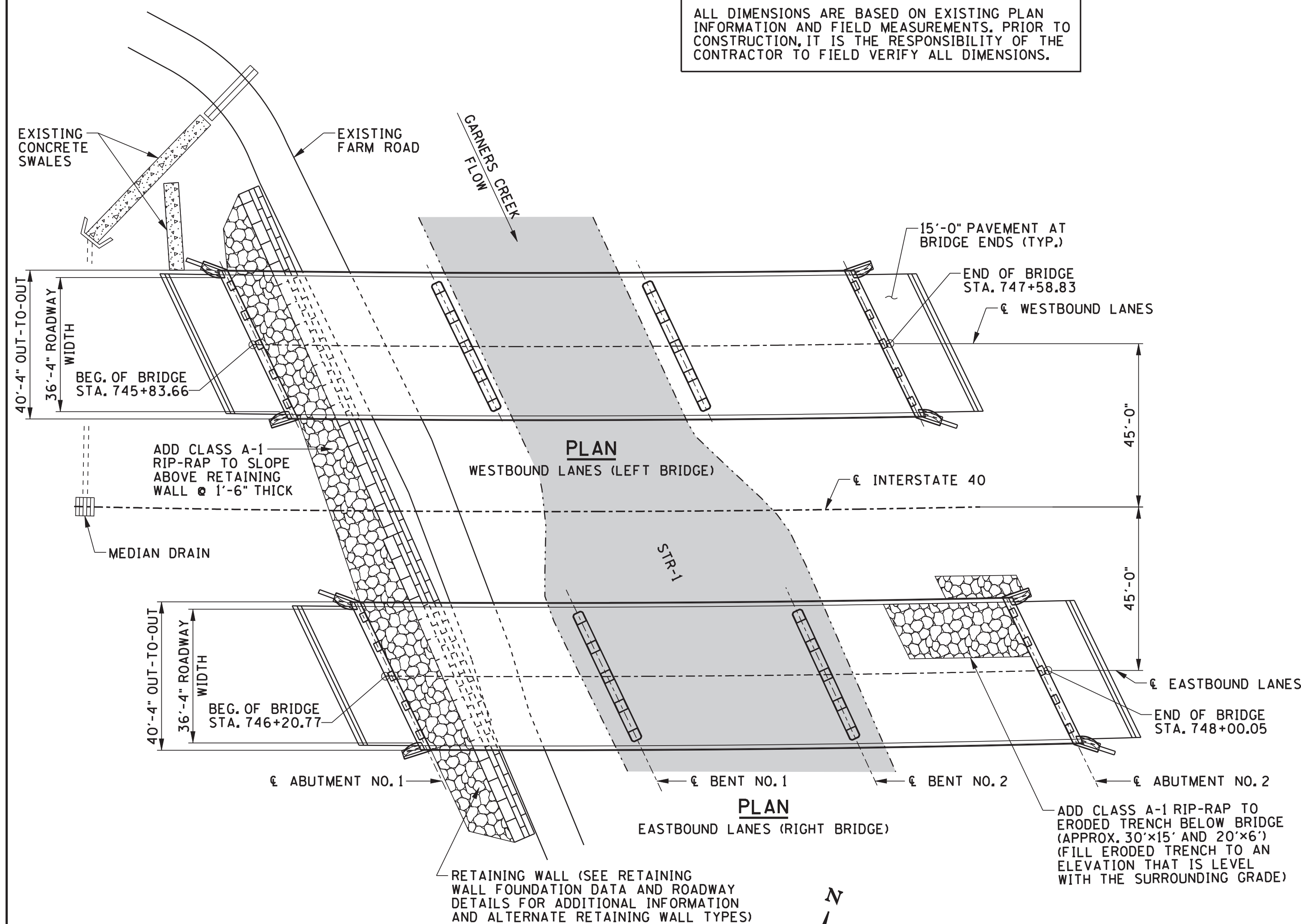
GABION WALL
 DETAILS AND NOTES
 INTERSTATE 40
 OVER GARNERS CK.
 BR. NO. 41-10040-1.53 (LL&R)
 FED. ID. NO. 41100400027
 FED. ID. NO. 41100400028
 HICKMAN COUNTY
 2026

BR-132-335



ELEVATION

ALL DIMENSIONS ARE BASED ON EXISTING PLAN INFORMATION AND FIELD MEASUREMENTS. PRIOR TO CONSTRUCTION, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY ALL DIMENSIONS.



SCOPE OF WORK

1. INSTALL TRAFFIC CONTROL TO PROVIDE ONE LANE OF TRAFFIC ON I-40 DURING CONSTRUCTION PHASES.
2. ADD RETAINING WALLS AT END SLOPES BESIDE DIRT ROAD NEAR WEST END OF LEFT AND RIGHT BRIDGES.
3. TRANSITION ASPHALT ROADWAY APPROACHES AS REQUIRED.
4. REMOVE EXISTING ASPHALT WEARING SURFACE FOR DECK REPAIR.
5. REPAIR BRIDGE DECK (PARTIAL AND FULL DEPTH REPAIRS) AND PROVIDE ASPHALT OVERLAY WITH MEMBRANE.
6. REPAIR OR REPLACE DAMAGED APPROACH GUARDRAIL WITH STANDARD APPURTENANCES.
7. REPLACE DAMAGED GUARDRAIL SECTIONS RUNNING BETWEEN SUBJECT BRIDGE AND I-40 BRIDGE OVER GARNERS CREEK ROAD.
8. REPAIR CONCRETE SPALLS ON PARAPETS, BENTS, AND ABUTMENTS.
9. REMOVE VEGETATION FROM CHANNEL, GROWTH AFFECTING SUBSTRUCTURE, AND FLOOD DEBRIS. NO IN-STREAM WORK PROPOSED.
10. COORDINATE WITH UTILITY OWNERS THAT AFFECT BRIDGE REPAIRS.
11. CLEAR DECK DRAINS.
12. CLEAN, PAINT AND PROTECT EXPOSED STEEL PILES WITH CONCRETE ENCASEMENT.
13. ADD 15 FOOT LONG P.A.B.E. TO ENDS OF EACH BRIDGE AND ALLOW EXPANSION AT FREE END OF P.A.B.E.
14. ADD RIP-RAP TO BADLY ERODED TRENCH AT END OF BRIDGE FILL SLOPE OF RIGHT LANE STRUCTURE.
15. REPLACE PAVEMENT REFLECTORS AFFECTED BY THE REPAVING OF THE APPROACHES.
16. PROVIDE TEMPORARY STONE CONSTRUCTION ACCESS ROAD AT SOUTHWEST CORNER OF RIGHT BRIDGE.

SPECIAL NOTES

1. THE STATE-LISTED ENDANGERED SPECIES, EGG-MIMIC DARTER (ETHEOSTOMA PSEUDOVULATUM), IS KNOWN TO OCCUR IN ADJACENT STREAMS WITHIN A FOUR MILE RADIUS OF THE PROJECT SITE.
2. IN THE EVENT THAT IN-STREAM WORK WILL OCCUR ON THIS PROJECT, THE TENNESSEE WILDLIFE RESOURCE AGENCY (TWRA) HAS INDICATED THAT THEY WILL REQUIRE THAT SWEEPS FOR THE EGG-MIMIC DARTER MUST BE CONDUCTED JUST PRIOR TO IN-STREAM CONSTRUCTION IN THE IMMEDIATE PROJECT CONSTRUCTION FOOTPRINT, AND THE FISH MUST BE RELOCATED UPSTREAM IN SUITABLE HABITAT.
3. MIGRATION BARRIERS MUST BE INSTALLED AND MAINTAINED THROUGHOUT THE IN-STREAM CONSTRUCTION PERIOD TO PREVENT FISH FROM RE-ENTERING THE CONSTRUCTION AREA.
4. TDOT BIOLOGISTS MUST BE NOTIFIED OF THE PRE-CONSTRUCTION MEETING IN ADVANCE SO THAT THEY CAN ARRANGE TO ATTEND.
5. TDOT BIOLOGISTS MUST BE CONTACTED AT LEAST TWO WEEKS BEFORE IN-STREAM CONSTRUCTION BEGINS IN GARNERS CREEK SO THEY CAN COORDINATE FISH SWEEPS.

LIST OF DRAWINGS

DWG. NO.	LAST REV. DATE
LAYOUT OF BRIDGE TO BE REPAIRED (BR. NOS. 41-140-1.53LT & RT)	BR-111-19 12-19-12
ESTIMATED BRIDGE QUANTITIES AND MISC. DETAILS	BR-111-20 12-19-12
BRIDGE GENERAL AND SPECIAL NOTES	BR-111-21 10-3-12
PHASING (BR. NOS. 41-140-1.53LT & RT)	BR-111-22 10-3-12
ABUTMENT NOS. 1 AND 2 LT FOOTING DETAILS	BR-111-23 10-3-12
ABUTMENT NOS. 1 AND 2 RT FOOTING DETAILS	BR-111-24 10-3-12
DECK REPAIR DETAILS	BR-111-25
CONCRETE REPAIRS - ABUTMENT NOS. 1 AND 2 LT	BR-111-26
CONCRETE REPAIRS - BENT NO. 2 LT	BR-111-27
CONCRETE REPAIRS - SLAB AND RAIL (LT LANE BRIDGE)	BR-111-28
CONCRETE REPAIRS - ABUTMENT NOS. 1 AND 2 RT	BR-111-29
CONCRETE REPAIRS - BENT NO. 1 RT	BR-111-30
CONCRETE REPAIRS - BENT NO. 2 RT	BR-111-31
CONCRETE REPAIRS - SLAB AND RAIL (RT LANE BRIDGE)	BR-111-32
CONCRETE REPAIR DETAILS	BR-111-45
BILL OF STEEL	BR-111-46

LIST OF REFERENCE DRAWINGS

DWG. NO.	LAST REV. DATE
EXISTING BRIDGE PLANS	K-31-41 THRU K-31-49
	K-15-60, H-5-110
	SBRM-2-124 THRU SBRM-2-126

LIST OF BRIDGE APPURTENANCES

DWG. NO.	LAST REV. DATE
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STD-1-5 06-01-11
REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLABS	STD-9-1 10-07-08
DETAILS SHOWING GUARDRAIL ATTACHMENT AT BRIDGE ENDS TO EXISTING CONCRETE SLOPE FACE END POST - 1989	SBR-2-131 01-22-02
DETAILS SHOWING GUARDRAIL ATTACHMENT AT BRIDGE ENDS TO EXISTING CONCRETE SLOPE FACE END POST - 1989	SBR-2-132 01-04-96
STANDARD SHOWING DETAILS FOR ATTACHING NEW GUARDRAIL TO EXISTING END OF BRIDGE - 1992	SBR-2-137 11-05-01
STANDARD SHOWING DETAILS FOR ATTACHING NEW GUARDRAIL AT EXISTING BRIDGE END AND ALONG EXISTING BRIDGE RAIL - 1992	SBR-2-138 11-05-01
STANDARD SHOWING DETAILS FOR ATTACHING NEW GUARDRAIL ALONG EXISTING BRIDGE RAILS - 1992	SBR-2-140 11-05-01

LIST OF SPECIAL PROVISIONS

DWG. NO.	LAST REVISION DATE	DESCRIPTION
108B	**	PROJECT COMPLETION AND DISINCENTIVE
624	**	RETAINING WALLS

** DENOTES: CURRENT REVISION DATE AS PER CONTRACT DRAWINGS.

PROJECT NO.	YEAR	SHEET NO.
41002-4113-04	2012	

REVISIONS		
NO.	DATE	BRIEF DESCRIPTION
1	5-4-12	NBP LATEST REVISION DATE
2	10-3-12	LKB UPDATED NOTE 1 AND LATEST REVISION DATE
3	12-19-12	DWT LATEST REVISION DATE



12/19/2012
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

LAYOUT OF BRIDGE TO BE REPAIRED

BRIDGE NOS. 41-140-1.53LT AND 41-140-1.53RT
INTERSTATE 40 OVER GARNERS CREEK
HICKMAN COUNTY
2012

DESIGNED BY: NICHOLAS POWELL
DRAWN BY: ANGELA MOORE
SUPERVISED BY: LANDEL BILBREY
CHECKED BY: JAMIE GILLESPIE

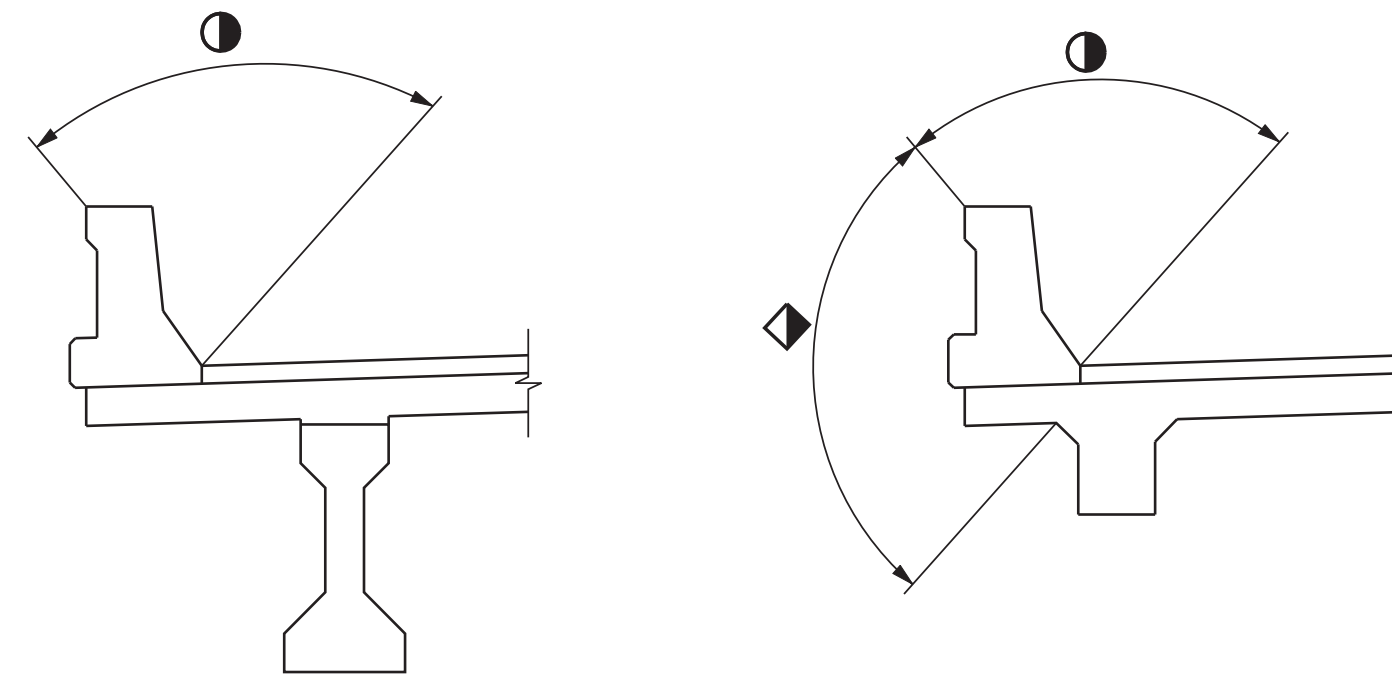
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ESTIMATED BRIDGE QUANTITIES							
ITEM NO.	DESCRIPTION	UNIT	BRIDGE NO. 41-140-1.53LT 41-140-1.53RT	BRIDGE NO. 41-140-1.63LT 41-140-1.63RT	TOTAL		
1	202-04.01	REMOVAL OF STRUCTURES (BR NO. 41-140-1.63LT & RT)	LS	0	1	1	
3	4	502-03.01	CEMENT GROUTING	C.F.	0	400	400
2	602-10.05	BRACING REPAIRS	LS	0.5	0.5	1	
5	602-10.12	BEARING DEVICE (REPAIR)	LS	0	1	1	
6	604-02.03	EPOXY COATED REINFORCING STEEL	LB.	2,038	2,150	4,188	
7	604-03.01	CLASS A CONCRETE (BRIDGES)	C.Y.	0	13	13	
8	604-03.04	PAVEMENT AT BRIDGE ENDS	S.Y.	275	255	530	
	604-04.01	APPLIED TEXTURE FINISH (NEW STRUCTURES)	S.Y.	0	14	14	
9	604-04.02	APPLIED TEXTURE FINISH (EXISTING STRUCTURES)	S.Y.	300	190	490	
3	10	604-10.05	CONCRETE	S.F.	34	55	89
11	604-10.14	REMOVE EXISTING WEARING SURFACE	LS	0.5	0.5	1	
3	604-10.30	BRIDGE DECK REPAIRS (FULL DEPTH OF SLAB)	S.Y.	35	60	95	
	604-10.44	EXPANSION JOINT REPAIRS	L.F.	168	76	244	
3	604-10.50	BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB)	S.Y.	220	175	395	
3	12	604-10.54	CONCRETE REPAIRS	S.F.	140	135	275
13	604-10.55	CONCRETE (FOUNDATION REPAIRS)	C.Y.	25	0	25	
14	617-01	BRIDGE DECK SEALANT	S.Y.	1,675	952	2,627	
15	620-10.01	CONCRETE PARAPET (BRIDGERAIL REPLACEMENT)	L.F.	0	14	14	

FOOTNOTES: 1 2

- 1 INCLUDES THE COST OF REMOVING THE EXISTING EXPANSION JOINT AND A PORTION OF THE SLAB AT ABUTMENT NO.1 ON THE EASTBOUND AND WESTBOUND BRIDGES.
- 2 INCLUDES ALL NECESSARY TEMPORARY SUPPORT OF THE STRUCTURE DURING THE DURATION OF THE PROJECT. ALSO INCLUDES FORMWORK TO STOP DEBRIS FROM FALLING INTO GARNERS CREEK OR ONTO GARNERS CREEK ROAD DURING REPAIRS. BRACING PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
- 3 ITEM SHALL BE BID AS A CONTINGENCY AND MAY BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.
- 4 ITEM TO BE USED TO FILL VOID SPACES BELOW CONCRETE ABUTMENT SLOPES. INCLUDES COST OF CORING HOLES THROUGH EXISTING CONCRETE.
- 5 INCLUDES HAND TOOL CLEANING AND PAINTING 40 EXISTING STEEL BEARINGS ON BRIDGES 41-140-1.63LT AND RT, AT AN APPROXIMATE WEIGHT OF 6,540 LBS. PAYMENT SHALL BE FULL COMPENSATION FOR OBTAINING ALL PERMITS, ALL SURFACE PREPARATION INCLUDING REMOVAL OF PAINT, DISPOSAL AND FURNISHING AND APPLYING THE PAINT, ALL IN ACCORDANCE WITH T.D.O.T. SPECIFICATIONS. SEE PAINT NOTES ON SHEET BR-111-21.
- 6 INCLUDES QUANTITY OF STEEL REQUIRED FOR FOUNDATION REPAIR AND ATTACHMENT OF P.A.B.E. TO THE EXISTING ROADWAY BRACKET ON BRIDGES 41-140-1.53LT AND RT. ALSO INCLUDES THE QUANTITY OF STEEL REQUIRED FOR LOCKING UP THE WESTERN EXPANSION JOINT (ABUTMENT NO.1) ON BRIDGES 41-140-1.63LT AND RT. REBAR QUANTITIES REQUIRED FOR PAVEMENT AT BRIDGE ENDS, AS SHOWN ON STD-1-5, ARE NOT INCLUDED IN THIS ITEM NUMBER.
- 7 INCLUDES THE QUANTITY REQUIRED TO LOCK-UP THE EXISTING EXPANSION JOINTS AT ABUTMENT NO.1 FOR BRIDGES 41-140-1.63LT AND RT. CONCRETE SHALL BE HIGH EARLY STRENGTH. SEE CONCRETE STRENGTH AND CURE TIME NOTES ON SHEET 2A, BRIDGE GENERAL AND SPECIAL NOTES.
- 8 PRIOR TO CONSTRUCTION OF THE PAVEMENT AT BRIDGE ENDS, THE CONTRACTOR SHALL SUBMIT A PROPOSED BILL OF STEEL TO THE ENGINEER FOR APPROVAL. 15'-0" PAVEMENT AT BRIDGE ENDS REQUIRED AT BOTH BRIDGE ENDS. COST OF CONCRETE, REINFORCING STEEL, JOINT MATERIALS, LABOR AND ALL OTHER MATERIALS REQUIRED TO CONSTRUCT THE PAVEMENT AT BRIDGE ENDS AND ASPHALT APPROACH BEAM SHALL BE INCLUDED IN THIS ITEM. SEE STD. DWGS. STD-1-5 AND STD-1-7 FOR QUANTITIES TO BE INCLUDED IN THIS ITEM. STD-1-5 SHALL BE MODIFIED AS NEEDED TO CHANGE THE 24'-0" LENGTH TO 15'-0".
- 9 INCLUDES THE COST OF ALL LABOR AND MATERIALS TO APPLY TEXTURE FINISH TO THE EXISTING PARAPETS.
- 10 INCLUDES THE COST OF ALL LABOR AND MATERIALS REQUIRED TO REPAIR SPALLED AREAS WITH HIGH EARLY STRENGTH CONCRETE.
- 11 INCLUDES REMOVAL AND DISPOSAL OF ASPHALT FROM SURFACE OF BRIDGE ONLY. (ASPHALT DEPTH VARIES FROM 3" TO 4 1/4" ON BRIDGES 41-140-1.53LT AND RT, AND FROM 4" TO 4 1/2" ON BRIDGES 41-140-1.63LT AND RT). BRIDGE DECK SHOULDERS THAT CONTAIN RUMBLE STRIPS SHALL ONLY HAVE THE MINIMUM 1 1/4" REMOVED FOR PHASED CONSTRUCTION PRIOR TO THE PHASING THAT REQUIRES FULL DEPTH REPAIRS OF THAT PORTION OF THE BRIDGE.
- 12 INCLUDES THE COST OF ALL LABOR AND MATERIALS REQUIRED TO REPAIR SPALLED AREAS WITH QUICK-SET PATCHING MATERIALS.
- 13 INCLUDES THE COST OF CLASS "A" CONCRETE, EXCAVATION, FORMING, LABOR AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS TO THE EXISTING ABUTMENTS.
- 14 INCLUDES COST OF ALL LABOR AND MATERIALS TO INSTALL BRIDGE DECK SEALANT. SEAL SHALL BE PLACED OVER FULL AREA OF ALL BRIDGE DECKS BETWEEN THE BEGINNING AND END OF EACH BRIDGE.
- 15 COST INCLUDES REINFORCING STEEL AS SHOWN ON STD-1-1, HIGH EARLY STRENGTH CONCRETE, FORMING, LABOR, GUARDRAIL REMOVAL AND REATTACHMENT, AND ALL OTHER MISCELLANEOUS MATERIALS NECESSARY FOR CONSTRUCTING THE NEW PARAPET RAIL SECTIONS.



BR. NO. 41-140-1.53LT AND RT BR. NO. 41-140-1.63LT AND RT

TYPICAL @ PARAPETS

- ◆ DENOTES: APPLIED TEXTURE FINISH ITEM NO. 604-04.01 MOUNTAIN GRAY NO. 36440
 ● DENOTES: APPLIED TEXTURE FINISH ITEM NO. 604-04.02 WHITE NO. 37886

APPLIED TEXTURE FINISH SKETCH

N.T.S.

- NOTE: COST OF TEXTURE COATING SHALL BE INCLUDED IN ITEM NOS. 604-04.01 AND 604-04.02 AND INCLUDES CONCRETE BRIDGE PARAPETS.
- NOTE: BEFORE APPLYING ANY TEXTURE FINISH, ALL SURFACES SHALL BE COMPLETELY CLEANED OF ALL DEBRIS AND FOREIGN MATERIALS.
- NOTE: THE CONTRACTOR SHALL USE CONTAINMENT SCREENS OR OTHER MEASURES AS NECESSARY TO PREVENT ANY TEXTURE COATING FROM ENTERING THE ENVIRONMENT. CONTAINMENT MEASURES SHALL BE APPROVED BY THE ENGINEER AND COST SHALL BE INCLUDED IN ITEMS BID ON.
- NOTE: CONCRETE FINISHING SHALL BE IN ACCORDANCE WITH SECTION 604.22 OF THE TENNESSEE STANDARD SPECIFICATION. A CLASS I FINISH FOLLOWED BY AN APPLIED TEXTURE FINISH SHALL BE USED IN LIEU OF A CLASS II FINISH. THE COLOR OF THE FINISH SHALL BE SIMILAR TO MOUNTAIN GRAY, FEDERAL SPECIFICATION NO. 36440, FEDERAL COLOR STANDARD NO. 595A, EXCEPT THAT THE INSIDE FACE AND TOP OF THE PARAPET SHALL BE WHITE, FEDERAL SPECIFICATION NO. 37886. A COLOR SAMPLE SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL. NO TEXTURE FINISH SHALL BE APPLIED PRIOR TO COMPLETION OF PAVING AND HAUL OPERATIONS AT THE BRIDGE SITE. THE APPLIED TEXTURE FINISH SHALL BE MEASURED AND PAID FOR UNDER ITEM NOS. 604-04.01 OR 604-04.02.

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
41002-4113-04	2012	2	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	10-3-12	LKB	UPDATED QUANTITY FOR ITEM NOS. 604-03.01, 604-10.44 AND 617-01.
			REMOVED ITEM NOS. 603-02.01, 603-02.02, 603-05.20 AND 603-05.21.
			ADDED ITEM NO. 602-10.12, REVISED NOTE 5, DELETED NOTE 6 AND RENUMBERED NOTES
2	12-19-12	DWT	REMOVED ITEM NO. 604-05.31 AND FOOTNOTE 10. RENUMBERED FOOTNOTES.

DESIGNED BY NICHOLAS POWELL DATE _____
 DRAWN BY ANGELA MOORE DATE _____
 SUPERVISED BY LANDEL BILBREY DATE _____
 CHECKED BY JAMIE GILLESPIE DATE _____



12/19/2012

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 ESTIMATED BRIDGE QUANTITIES
 AND MISC. DETAILS

BRIDGE NOS. 41-140-1.53LT AND 41-140-1.53RT
 INTERSTATE 40 OVER GARNERS CREEK
 AND
 BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
 INTERSTATE 40 OVER GARNERS CREEK ROAD
 HICKMAN COUNTY
 2012

GENERAL NOTES

- (1) SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION. (MARCH 2006 EDITION)
- (2) DESIGN SPECIFICATIONS: AASHTO 17TH EDITION - WITH ADDENDA.
- (3) LOADING: HS20-44
- (4) DEMOLITION: THE CONTRACTOR SHALL TAKE SPECIAL CARE TO PROTECT ANY PARTS OF THE STRUCTURE THAT ARE NOT TO BE REMOVED SPECIFICALLY. THE CONTRACTOR IS NOT ALLOWED TO USE A HYDRAULIC RAM MOUNTED ON A BACKHOE (COMMONLY CALLED A HOE RAM) OR OTHER SIMILARLY HEAVY EQUIPMENT FOR CONCRETE REMOVAL. PNEUMATIC HAMMERS MAY BE USED TO REMOVE UNSOUND CONCRETE. FOR FULL DEPTH OF CONCRETE SLAB REMOVAL EXCEPT OVER BEAMS, THE MAXIMUM HAMMER SIZE IS 90 POUND CLASS. FOR PARTIAL DEPTH OF CONCRETE SLAB REMOVAL AND ANY WORK OVER BEAMS, THE MAXIMUM HAMMER SIZE IS 60 POUND CLASS. SAWING OR CUTTING OF THE CONCRETE IS ACCEPTABLE SO LONG AS ANY SPECIFIED PROJECTION OF THE EXISTING REINFORCING STEEL IS MAINTAINED. ALL DEVICES PROPOSED FOR CONCRETE DEMOLITION SHALL MEET THE APPROVAL OF THE ENGINEER.
- (5) FORMS AND FALSEWORK: ALL CONCRETE FORM WORK AND FALSEWORK SHALL BE REMOVED AFTER REPAIRS ARE COMPLETED. COST OF FORMS, FALSEWORK, AND TEMPORARY SUPPORT REMOVAL SHALL BE INCLUDED IN ITEMS BID ON. THIS WORK SHALL BE COMPLETED BEFORE FINAL PAYMENT IS APPROVED.
- (6) FINISHING CONCRETE SURFACES: CONCRETE FINISHING SHALL BE IN ACCORDANCE WITH SECTION 604.22 OF THE TENNESSEE STANDARD SPECIFICATIONS. A CLASS I FINISH FOLLOWED BY AN APPLIED TEXTURE FINISH SHALL BE USED IN LIEU OF A CLASS II FINISH. NO TEXTURE FINISH SHALL BE APPLIED PRIOR TO COMPLETION OF PAVING AND HAULING OPERATIONS AT THE BRIDGE SITE.
- (7) QUICK SET PATCHING MATERIAL: QUICK SET PATCHING MATERIAL SHALL BE A POLYMER MODIFIED CEMENTIOUS PATCHING MATERIAL. SEE TDOT QUALIFIED PRODUCTS LIST 13, SECTION B.6, FOR ACCEPTABLE PATCHING MATERIAL.
- (8) WELDING: CURRENT AASHTO/AWS D1.5M/D1.5 BRIDGE WELDING CODE AND THE STANDARD SPECIFICATIONS. ALL WELDING TO BE COMPLETED BY AWS CERTIFIED WELDERS. SEE SECTION 602 OF THE STANDARD SPECIFICATIONS.
- (9) REINFORCING STEEL: SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS. REINFORCEMENT SHALL BE ASTM A615 GRADE 60 UNLESS NOTED OTHERWISE.
- (10) SLAB CONSTRUCTION JOINTS: TRANSVERSE SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING: 1) NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM AN INTERIOR SUPPORT. 2) THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 15 FEET OF THE SUPPORT EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.
- (11) MECHANICAL BAR SPLICERS: MUST BE ON THE APPROVED LIST MAINTAINED BY THE DIVISION OF MATERIALS AND TESTS. THE BAR SPLICER SHALL MEET AASHTO STANDARD SPECIFICATIONS FOR MECHANICAL CONNECTION. WHEN EPOXY COATING IS REQUIRED, THE EXPOSED THREADS SHALL BE REPAIRED AFTER SPLICING ACCORDING TO SPECIAL PROVISION 907A. THE COST OF FURNISHING THE BAR SPLICERS, (AND EPOXY COATING WHEN REQUIRED) INCLUDING ALL LABOR AND MATERIALS NECESSARY FOR COMPLETE INSTALLATIONS, SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 604-03.01, CLASS A CONCRETE (BRIDGES), C.Y.
- (12) GROUTED BARS IN DRILLED HOLES: HORIZONTALLY DRILLED HOLES SHALL BE DRILLED 1/2" IN DIAMETER LARGER THAN THE BAR, CLEANED, PACKED WITH NON-SHRINK GROUT AND THE BAR ROTATED (NOT DRIVEN) INTO ITS SEAT. VERTICALLY DRILLED HOLES SHALL BE DRILLED 1/4" IN DIAMETER LARGER THAN THE BAR, CLEANED, PACKED WITH EPOXY GROUT AND BAR DRIVEN TO ITS SEAT. ALL GROUTING MATERIALS SHALL BE APPROVED BY TDOT MATERIALS AND TESTS.
- (13) ANY DAMAGE TO EXISTING STRUCTURES TO BE LEFT IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- (14) SHOP DRAWINGS: SHALL BE SUBMITTED ACCORDING TO THE STANDARD SPECIFICATIONS. SHOP DRAWINGS SHALL BE SUBMITTED TO THE BRIDGE REPAIR OFFICE OF THE DIVISION OF STRUCTURES.
- (15) THE CONTRACTOR SHALL PROVIDE 100% CONVENTIONAL FALL PROTECTION FOR WORKERS INSTALLING DECK ABOVE 15 FEET.
- (16) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING CONSTRUCTION. DESIGN CALCULATIONS AND DETAILS OF TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY DEMOLITION IS BEGUN. COST OF STABILIZING THE STRUCTURE SHALL BE INCLUDED IN ITEM NO. 602-10.05, BRACING REPAIRS, L.S.

CONCRETE STRENGTH AND CURE TIMES NOTES

- (17) NO PNEUMATIC CONCRETE ALLOWED.
- (18) HIGH EARLY STRENGTH CONCRETE (DESIGNATED SPALL AREAS, FULL AND PARTIAL DEPTH DECK REPAIRS, GIRDER REPAIR AREAS, ABUTMENTS, BENTS, PARAPETS, EXPANSION JOINT REPAIRS, AND P.A.B.E.):
 HIGH EARLY STRENGTH CONCRETE - TRAFFIC SHALL NOT BE PERMITTED ON ANY REPAIR AREA UNTIL TEST SPECIMENS ATTAIN A COMPRESSIVE STRENGTH OF 3,000 P.S.I. AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF EIGHTEEN (18) HOURS. THE CONCRETE SHALL ATTAIN A COMPRESSIVE STRENGTH OF 3,000 P.S.I. IN EIGHTEEN (18) HOURS.
 SPECIAL NOTE TO CONTRACTOR CONCERNING CONCRETE: CONCRETE FOR INDICATED REPAIRS SHALL BE HIGH EARLY STRENGTH CONCRETE WITH A COMPRESSIVE STRENGTH OF F'c = 3,000 P.S.I. AT EIGHTEEN (18) HOURS. THE CONTRACTOR SHALL PROVIDE PROOF PRIOR TO BEGINNING WORK THAT THE PROPOSED CONCRETE MIX WILL OBTAIN THE REQUIRED PROPERTIES. PROOF SHALL BE PROVIDED BY AN INDEPENDENT TESTING COMPANY AND SUBMITTED TO THE MATERIALS AND TEST DIVISION OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION FOR APPROVAL.
- (19) CONCRETE CURING: ALL CONCRETE IN REPAIR AREAS SHALL BE CURED ACCORDING TO THE STANDARD SPECIFICATIONS.

SURFACE PREPARATION AND PAINTING NOTES

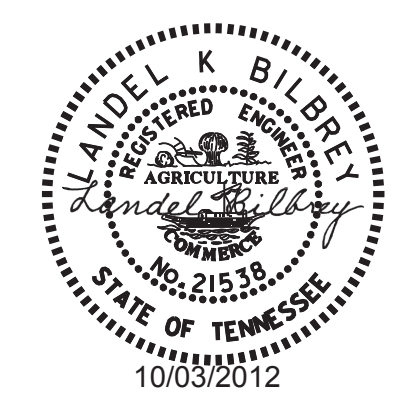
- (20) CLEANING AND PAINTING: ALL STEEL BEARINGS SHALL BE CLEANED IN ACCORDANCE WITH TENNESSEE STANDARD SPECIFICATION 603.05 (A). HAND OR POWER TOOL CLEANING SHALL REMOVE ALL RUST, SCALE, LOOSE PAINT AND DIRT. AFTER CLEANING, THE CLEANED STEEL SHALL BE PAINTED WITH AN APPROVED EPOXY MASTIC PAINT APPLIED AT A MINIMUM 4.0 MILS. THE COLOR OF THE EPOXY MASTIC SHALL BE ALUMINUM OR GREY.
- (21) SEE THE TENNESSEE DEPARTMENT OF TRANSPORTATION'S QUALIFIED PRODUCTS LIST FOR ACCEPTABLE BRANDS OF EPOXY MASTIC. ALL PRODUCTS USED, INCLUDING THINNERS, MUST BE SUPPLIED BY THE SAME MANUFACTURER.
- (22) APPLICATION: THE COATING APPLICATION SHALL FOLLOW THE MANUFACTURER'S PRINTED INSTRUCTIONS AND SHALL HAVE THESE INSTRUCTIONS ON SITE DURING THE COURSE OF THE WORK.
- (23) WORKER PROTECTION: OUR MAINTENANCE RECORDS INDICATE THAT THE BEARINGS ON THIS BRIDGE WERE ORIGINALLY PAINTED WITH MATERIALS CONTAINING LEAD AND/OR CHROMATES AND THE CONTRACTOR SHALL BE REQUIRED TO PROCEED ACCORDINGLY TO MAKE ALL MANDATORY SAGEGUARDS PRESCRIBED BY THE STATE AND FEDERAL LAW FOR WORKER PROTECTION AND HAZARDOUS MATERIAL DISPOSAL.
- (24) CONTAINMENT: THE EXISTING PAINT SYSTEM CONTAINS BASIC LEAD SILICO CHROMATE PAINT WHICH HAS BEEN DESIGNATED AS A HAZARDOUS MATERIAL. THE CONTRACTOR SHALL CONTAIN AND DISPOSE OF ALL PAINT REMOVAL WASTE IN ACCORDANCE WITH SECTION 603 OF STANDARD SPECIFICATIONS.

SPECIAL NOTES

- (25) SPECIAL NOTE FOR UTILITIES: IT IS INTENDED THAT THE COST OF MATERIALS AND LABOR NECESSARY FOR THE COMPLETE INSTALLATION OF UTILITIES SHOULD BE BORNE BY OTHERS AND SHALL NOT BE PAID FOR AS PART OF THIS CONTRACT. THE CONTRACTOR SHALL COOPERATE WITH OTHERS IN THE INSTALLATION OF UTILITIES WITH NO ADDITIONAL COMPENSATION ALLOWED THE CONTRACTOR.
- (26) SPECIAL NOTE CONCERNING VEGETATION REMOVAL: THE CONTRACTOR SHALL REMOVE ANY VEGETATION DIRECTLY ATTACHED TO THE SUBSTRUCTURE. THE CONTRACTOR SHALL ALSO CLEAR ANY VEGETATION NOT ATTACHED BUT AFFECTING THE SUBSTRUCTURE OR WHICH WILL AFFECT THE SUBSTRUCTURE IN THE FUTURE. ANY COST FOR VEGETATION REMOVAL WILL BE INCLUDED IN OTHER ITEMS BID ON.
- (27) THE STATE ENDANGERED EGG-MIMIC DARTER (THEOSTOMA PSEUDOVULATUM) HAS BEEN DOCUMENTED IN THE PINEY RIVER WATERSHED AND ITS TRIBUTARIES.
- (28) THE TENNESSEE WILDLIFE RESOURCES AGENCY REQUESTS THAT A FISH SURVEY BE CONDUCTED BY A QUALIFIED BIOLOGIST, WHO POSSESSES A TWRA SCIENTIFIC COLLECTION PERMIT, TO DETERMINE THE PRESENCE OR ABSENCE OF THE EGG-MIMIC DARTER AT THE PROPOSED PROJECT LOCATION.
- (29) IF THE SURVEY DETERMINES THAT THE EGG-MIMIC DARTER IS PRESENT, WE REQUEST THAT IN-STREAM CONSTRUCTION BE PROHIBITED DURING THE PERIOD OF MARCH 1ST THROUGH MAY 31ST (THE SPAWNING PERIOD FOR THE EGG-MIMIC DARTER).
- (30) MIGRATION BARRIERS MUST BE INSTALLED TO PREVENT RE-ENTRY OF THE FISH INTO THE PROJECT FOOTPRINT.
- (31) FISH SWEEPS MUST BE CONDUCTED PRIOR TO THE INSTALLATION OF THE MIGRATION BARRIERS WHERE THE BARRIERS ARE TO BE INSTALLED AND THE AREA BETWEEN THE BARRIERS BE SWEEPED AFTER THE BARRIERS ARE INSTALLED, AND THE FISH RELOCATED IN SUITABLE HABITAT UPSTREAM.
- (32) CONSTRUCTION FORCES MUST TAKE THE NECESSARY PRECAUTIONS TO PREVENT FRESH CONCRETE FROM COMING INTO CONTACT WITH THE FLOWING STREAM WATER, SINCE FRESH CONCRETE IS TOXIC TO AQUATIC LIFE.
- (33) TDOT BIOLOGISTS MUST BE NOTIFIED OF THE PRE-CONSTRUCTION MEETING IN ADVANCE SO THAT THEY CAN ARRANGE TO ATTEND.
- (34) TDOT BIOLOGISTS MUST BE CONTACTED AT LEAST TWO WEEKS BEFORE IN-STREAM CONSTRUCTION BEGINS IN GARNERS CREEK SO THEY CAN COORDINATE FISH SWEEPS.
- (35) CLIFF SWALLOW NESTS WERE DISCOVERED UNDERNEATH OF THE EXISTING BRIDGE OVER GARNERS CREEK ROAD AT LOG MILE 1.63. DISTURBANCE OF THE NESTS MUST BE PLANNED AROUND THE NESTING SEASON OF THESE BIRDS. NESTS CANNOT BE DISTURBED WHILE EGGS OR YOUNG ARE PRESENT IN THE NEST. THE APPROXIMATE DATE FOR NESTING ACTIVITIES TO BEGIN IS APRIL 15 AND FLEDGING IS JULY 3. NOTE THAT THESE ARE APPROXIMATE DATES AND NESTING COULD OCCUR EARLIER AND LAST UNTIL LATER.

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
41002-4113-04	2012	2A	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	5-4-12	NBP	ADDED NOTES 27 THRU 32
2	10-3-12	LKB	UPDATED NOTE 18 AND NOTES 20-24

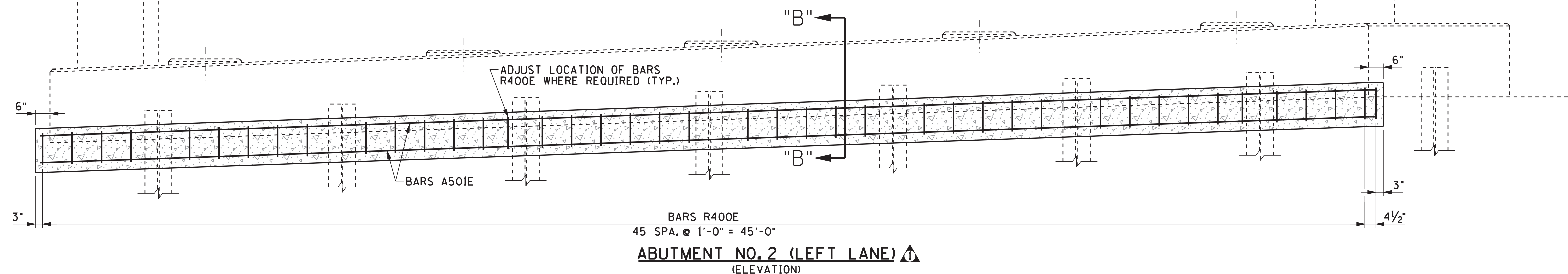
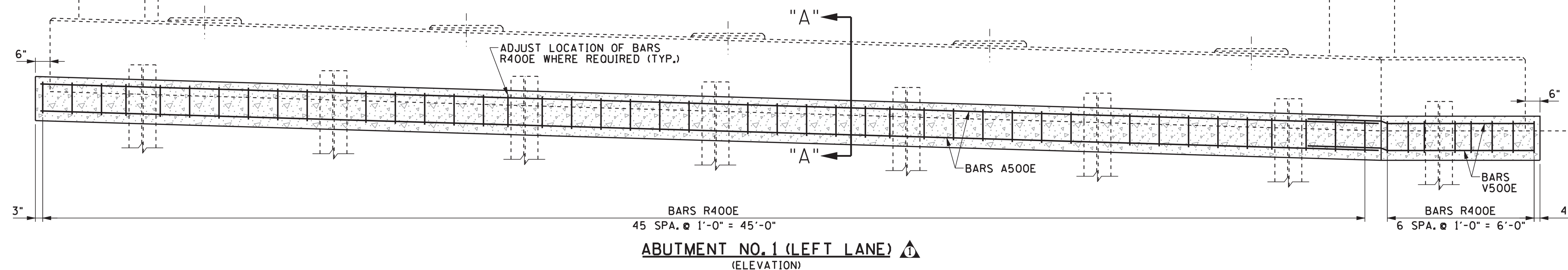
DESIGNED BY _____ NICHOLAS POWELL _____ DATE _____
 DRAWN BY _____ ANGELA MOORE _____ DATE _____
 SUPERVISED BY _____ LANDEL BILBREY _____ DATE _____
 CHECKED BY _____ JAMIE GILLESPIE _____ DATE _____



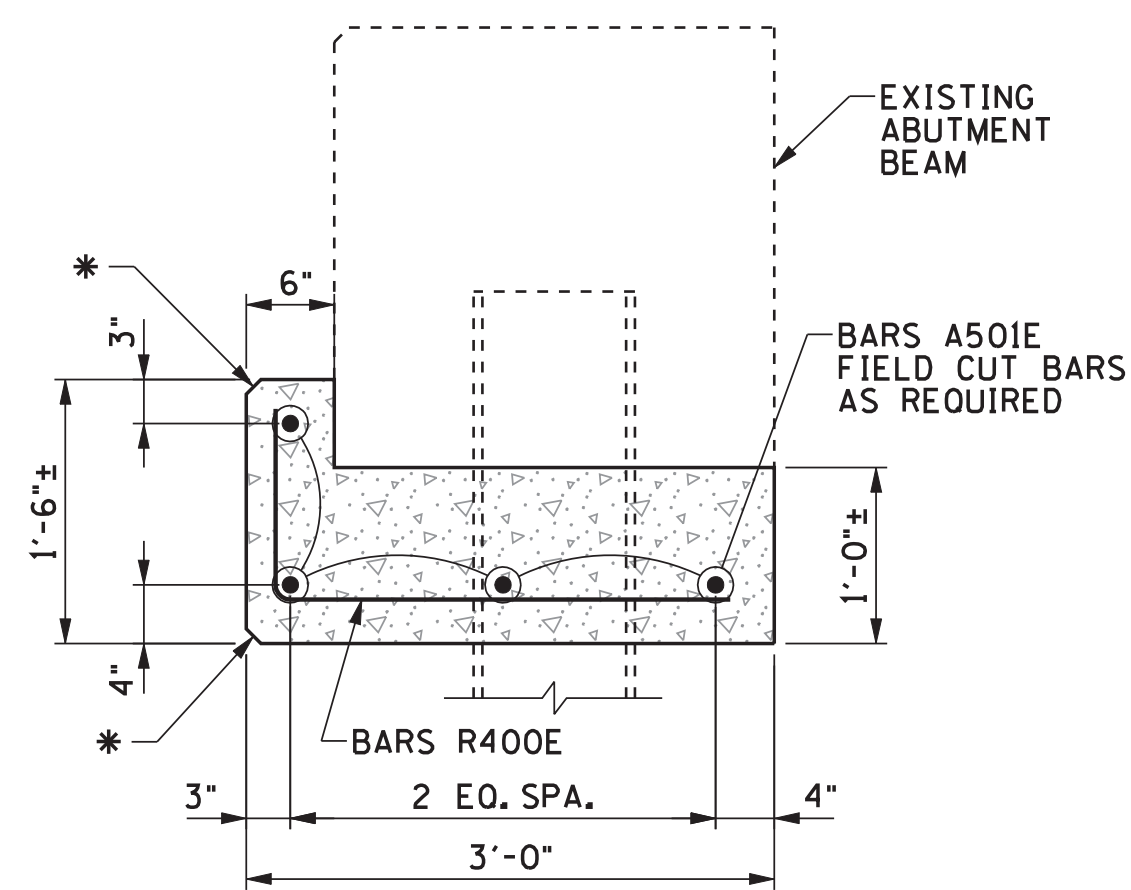
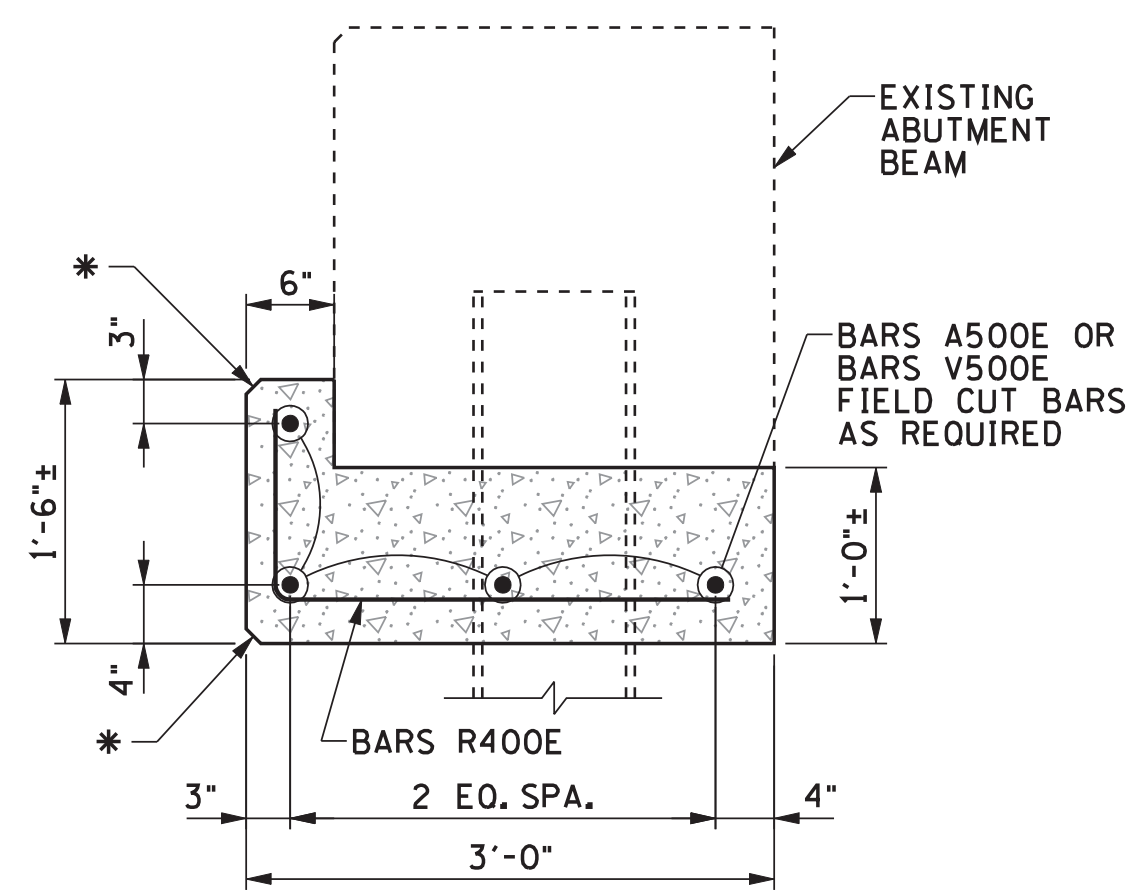
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BRIDGE GENERAL
 AND SPECIAL NOTES
 BRIDGE NOS. 41-140-1.53LT AND 41-140-1.53RT
 INTERSTATE 40 OVER GARNERS CREEK
 AND
 BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
 INTERSTATE 40 OVER GARNERS CREEK ROAD
 HICKMAN COUNTY
 2012

CONST. NO.		
PROJECT NO.	YEAR	SHEET NO.
41002-4113-04	2012	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	10-3-12	LKB	REMOVED PILE NOTE



- NOTES:**
- COST OF CLASS "A" CONCRETE, EXCAVATION, FORMING, LABOR AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS TO THE ABUTMENT FOUNDATIONS, SHALL BE PAID FOR UNDER ITEM NO. 604-10.55, CONCRETE (FOUNDATION REPAIRS), C.Y. ALL WORK MUST MEET WITH THE FULL SATISFACTION OF THE ENGINEER.
 - COST OF ALL EPOXY COATED REINFORCING STEEL SHALL BE PAID FOR UNDER ITEM NO. 604-02.03, EPOXY COATED REINFORCING STEEL, LBS. SEE BILL OF STEEL FOR BAR BENDS.
 - CONTRACTOR SHALL TAKE EXTREME CARE WHEN EXCAVATING SO AS NOT TO DAMAGE THE EXISTING STRUCTURE.
 - CONTRACTOR SHALL EXCAVATE THE MINIMUM AREA NECESSARY FOR REPAIR. VOID SPACES BELOW THE STANDARD DEPTH OF REPAIR SHALL BE FILLED WITH CLASS "A" CONCRETE WITH NO ADDITIONAL EXCAVATION. COST TO BE INCLUDED UNDER ITEM NO. 604-10.55, CONCRETE (FOUNDATION REPAIRS).



ESTIMATED QUANTITIES

CONCRETE (FOUNDATION REPAIRS) (C.Y.)	EPOXY COATED REINFORCING STEEL (BRIDGES) (LB.)
13	664



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ABUTMENT NOS. 1 AND 2 LT
FOOTING DETAILS

BRIDGE NOS. 41-140-1.53LT AND 41-140-1.53RT
INTERSTATE 40 OVER GARNERS CREEK
HICKMAN COUNTY
2012

DESIGNED BY: NICHOLAS POWELL
DRAWN BY: ANGELA MOORE
SUPERVISED BY: LANDEL BILBREY
CHECKED BY: JAMIE GILLESPIE

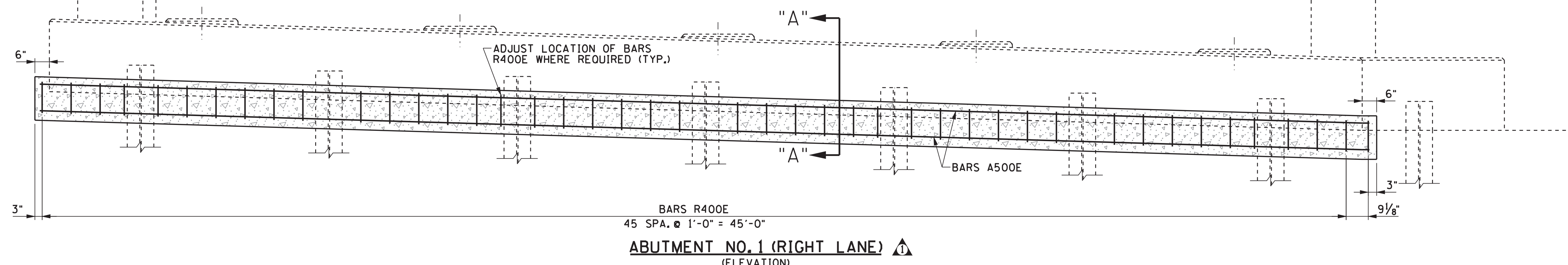


* DENOTES: 1" CHAMFER ON FRONT CORNERS

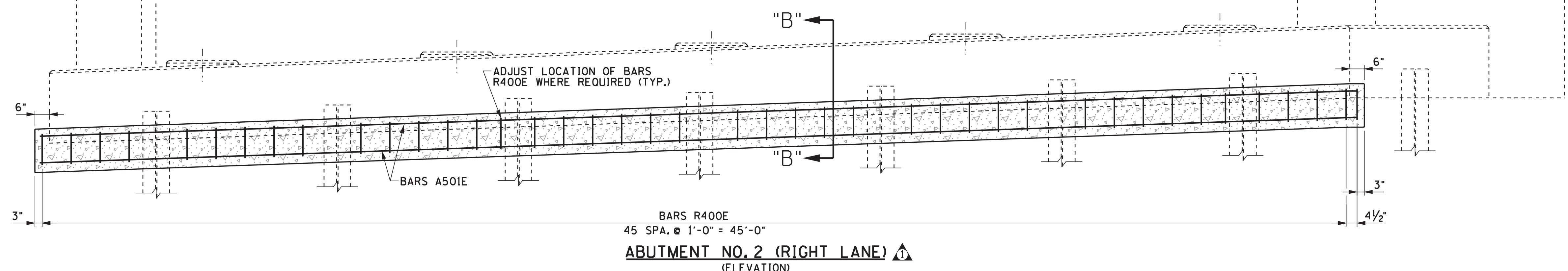
* DENOTES: 1" CHAMFER ON FRONT CORNERS

CONST. NO.		
PROJECT NO.	YEAR	SHEET NO.
41002-4113-04	2012	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	10-3-12	LKB	REMOVED PILE NOTE

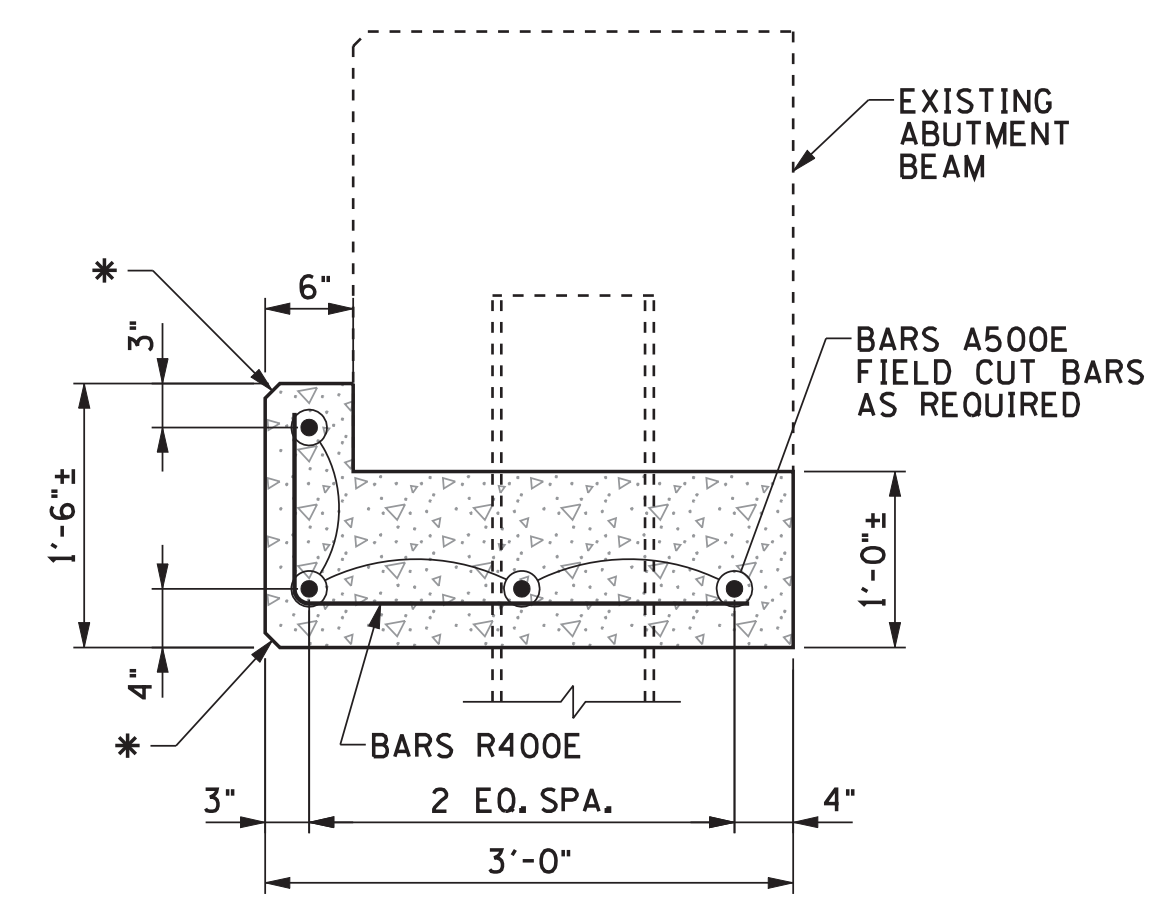


ABUTMENT NO. 1 (RIGHT LANE) Δ
(ELEVATION)

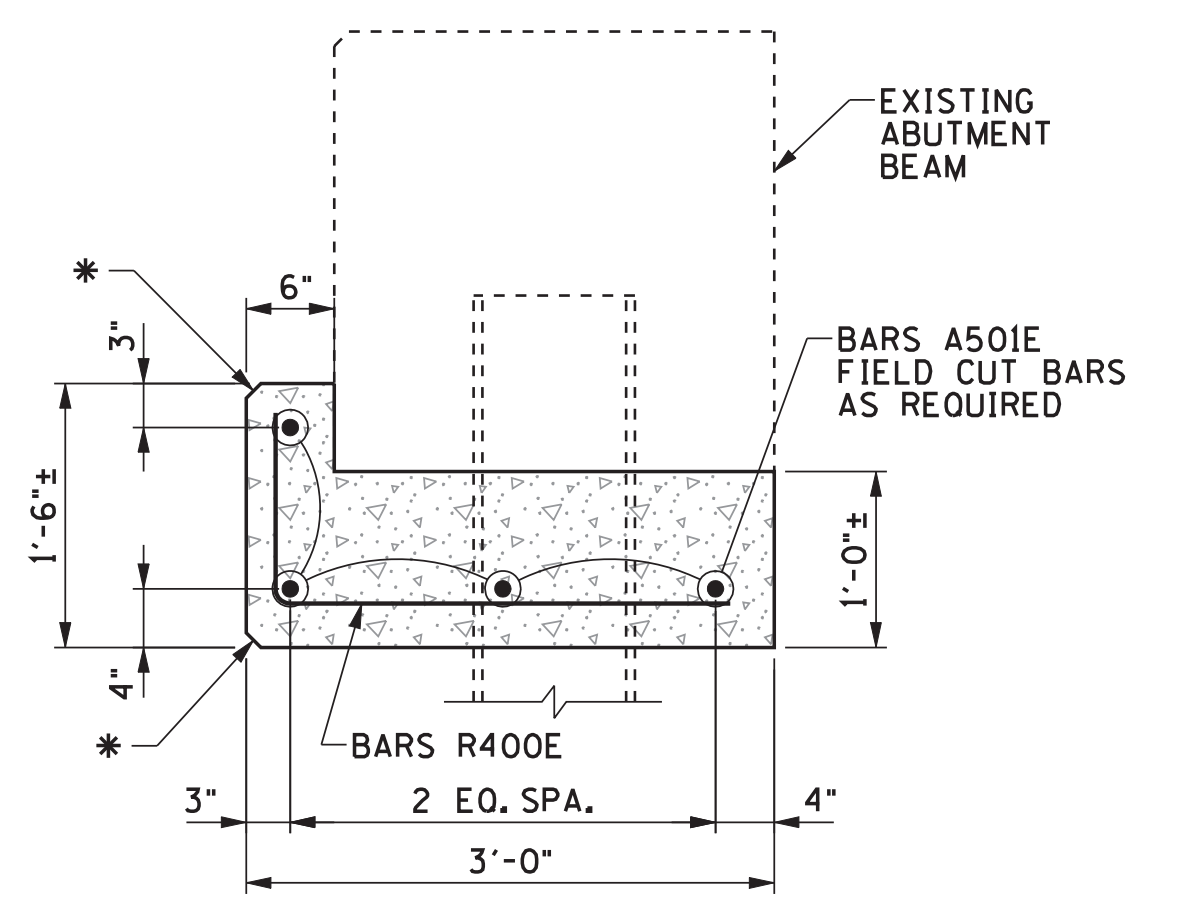


ABUTMENT NO. 2 (RIGHT LANE) Δ
(ELEVATION)

- NOTES:**
1. COST OF CLASS "A" CONCRETE, EXCAVATION, FORMING, LABOR AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS TO THE ABUTMENT FOUNDATIONS, SHALL BE PAID FOR UNDER ITEM NO. 604-10.55, CONCRETE (FOUNDATION REPAIRS), C.Y. ALL WORK MUST MEET WITH THE FULL SATISFACTION OF THE ENGINEER.
 2. COST OF ALL EPOXY COATED REINFORCING STEEL SHALL BE PAID FOR UNDER ITEM NO. 604-02.03, EPOXY COATED REINFORCING STEEL, LBS. SEE BILL OF STEEL FOR BAR BENDS.
 3. CONTRACTOR SHALL TAKE EXTREME CARE WHEN EXCAVATING SO AS NOT TO DAMAGE THE EXISTING STRUCTURE.
 4. CONTRACTOR SHALL EXCAVATE THE MINIMUM AREA NECESSARY FOR REPAIR. VOID SPACES BELOW THE STANDARD DEPTH OF REPAIR SHALL BE FILLED WITH CLASS "A" CONCRETE WITH NO ADDITIONAL EXCAVATION. COST TO BE INCLUDED UNDER ITEM NO. 604-10.55, CONCRETE (FOUNDATION REPAIRS).

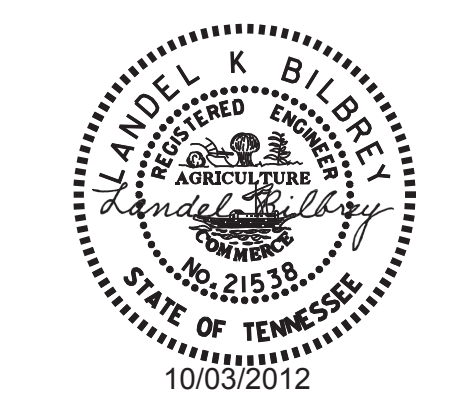


SECTION A-A
N.T.S.



SECTION B-B
N.T.S.

ESTIMATED QUANTITIES	
CONCRETE (FOUNDATION REPAIRS) (C.Y.)	EPOXY COATED REINFORCING STEEL (BRIDGES) (LB.)
12	612



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ABUTMENT NOS. 1 AND 2 RT
FOOTING DETAILS

BRIDGE NOS. 41-140-1.53LT AND 41-140-1.53RT
INTERSTATE 40 OVER GARNERS CREEK
HICKMAN COUNTY
2012

DESIGNED BY: NICHOLAS POWELL
DRAWN BY: ANGELA MOORE
SUPERVISED BY: LANDEL BILBREY
CHECKED BY: JAMIE GILLESPIE



* DENOTES: 1" CHAMFER ON FRONT CORNERS

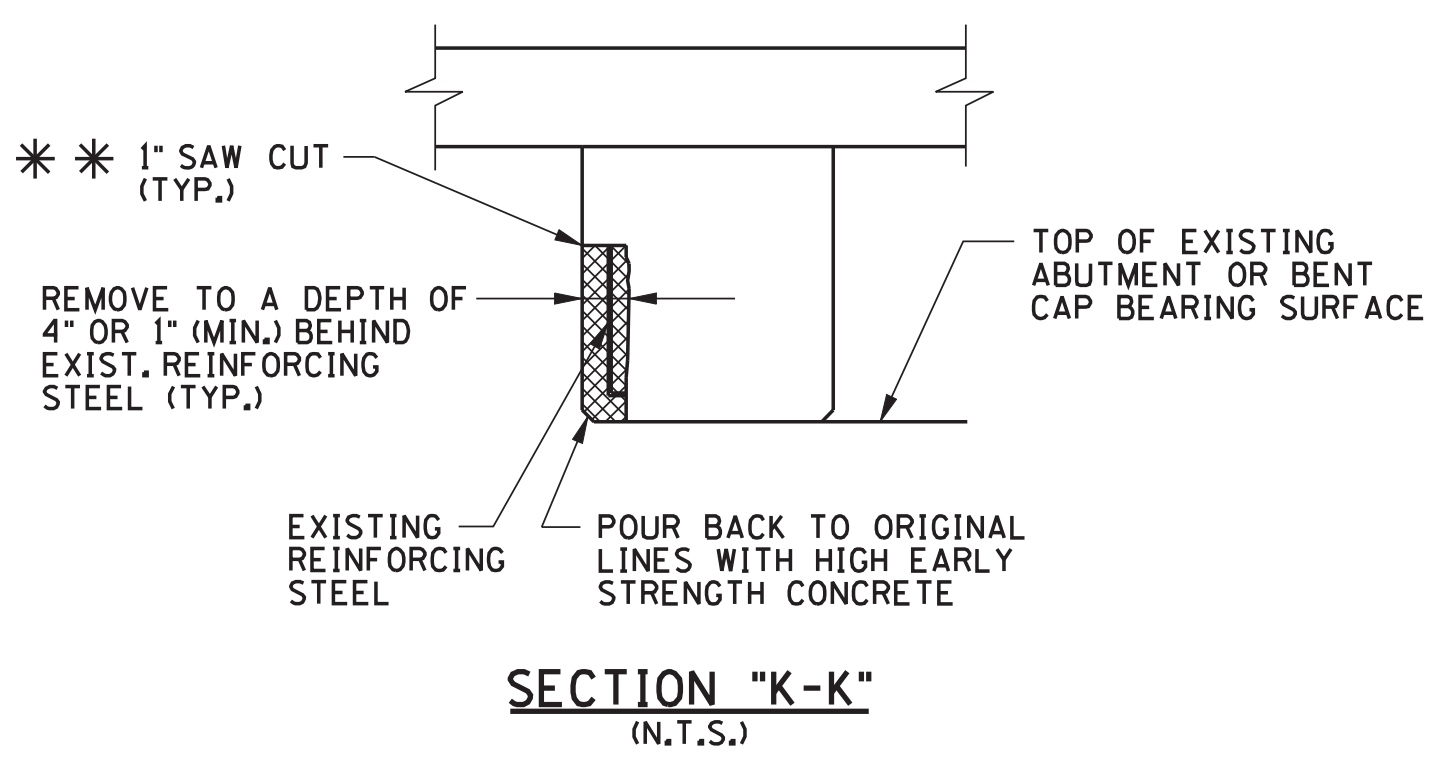
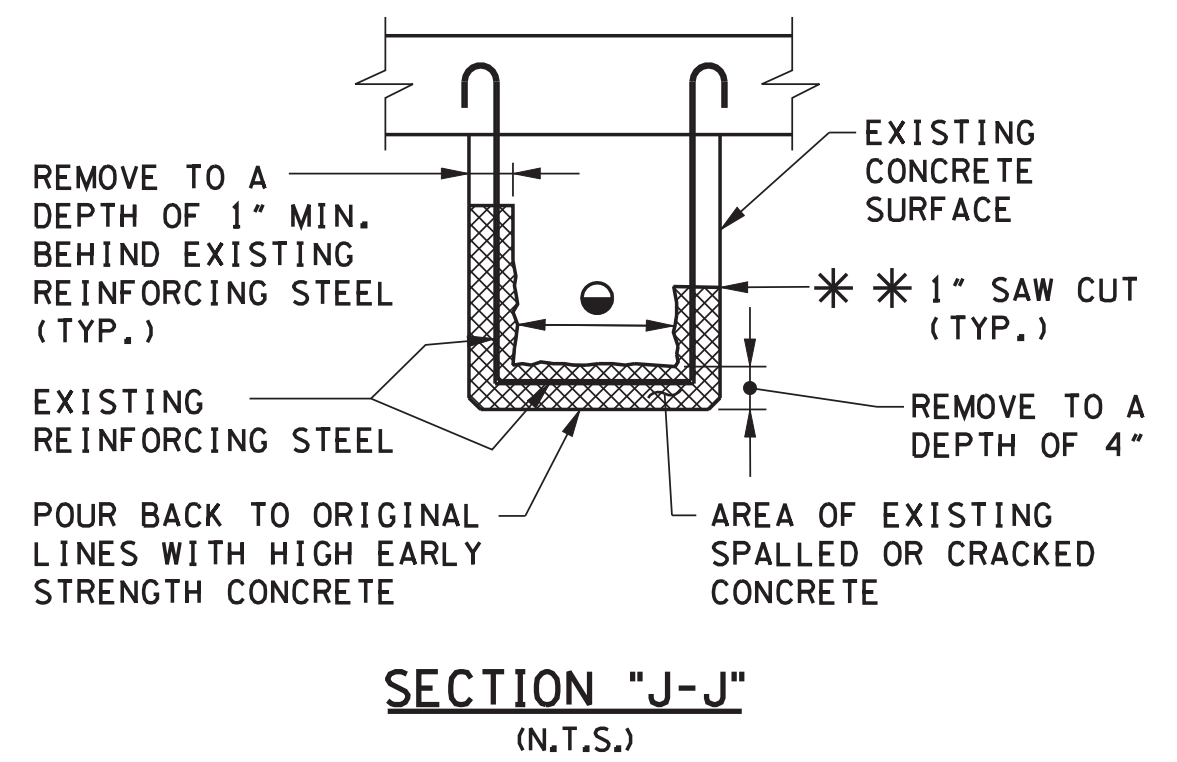
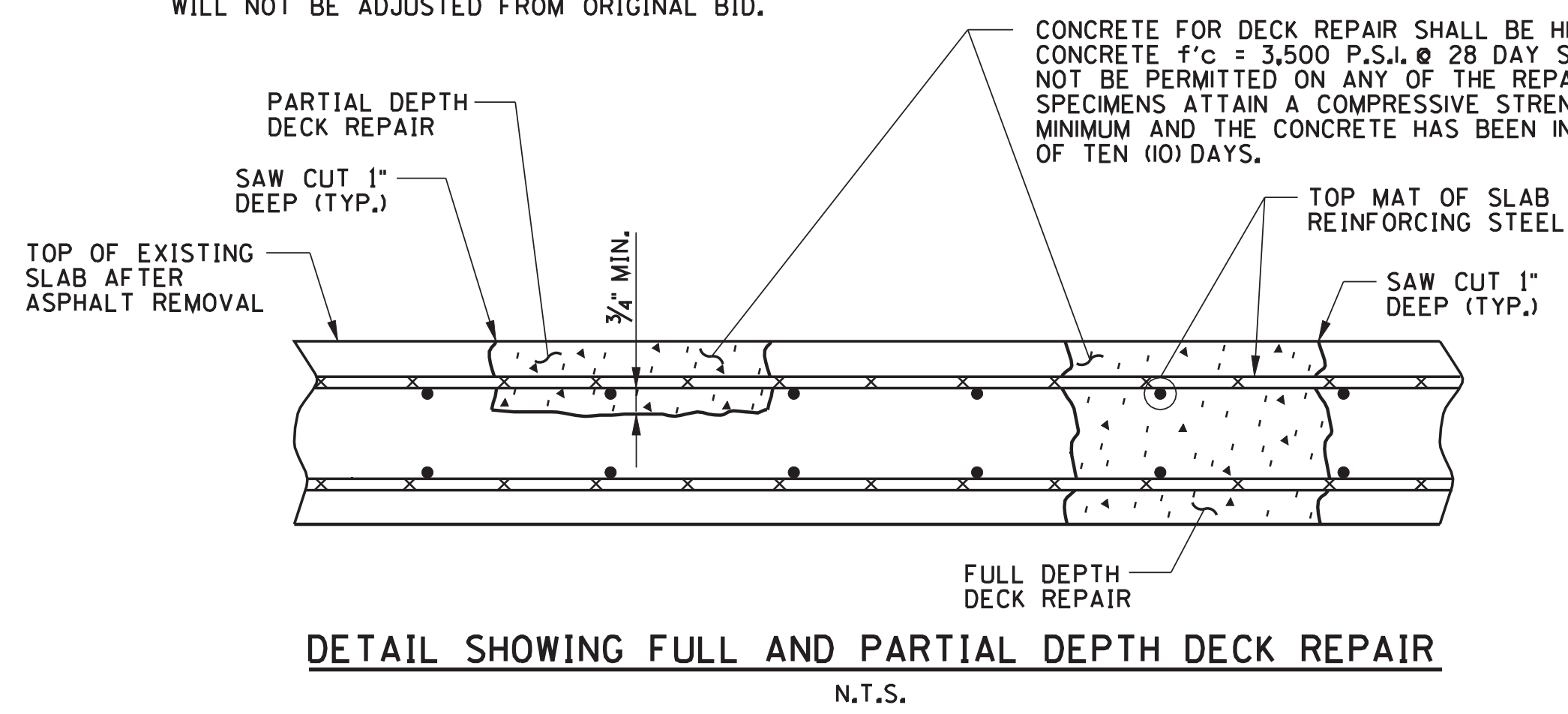
* DENOTES: 1" CHAMFER ON FRONT CORNERS

NOTES:

- REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE TOP BAR OF THE TOP MAT OF REINFORCING STEEL. ALL REINFORCING STEEL IN AREAS OF DECK REPAIR SHALL BE COMPLETELY CLEANED. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE BRIDGE REPAIR OFFICE. THE CONTRACTOR SHALL SCHEDULE AN INSPECTION WITH THE BRIDGE REPAIR OFFICE AFTER THE REMOVAL OF THE ASPHALT WEARING SURFACE. THIS INSPECTION, TO DETERMINE AREAS OF DECK REPAIR, SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST THREE (3) DAYS IN ADVANCE. DECK REPAIR WILL BE PAID FOR UNDER ITEM NO. 604-10.50, BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), AND ITEM NO. 604-10.30, BRIDGE DECK REPAIR (FULL DEPTH OF SLAB). DURING PARTIAL DEPTH REPAIRS, SHOULD DETERIORATED CONCRETE BE ENCOUNTERED WHICH APPEARS TO RUN FULL DEPTH IN THE SLAB, THE ENGINEER MAY DESIGNATE THESE AREAS TO BE REPAIRED UNDER ITEM NO. 604-10.30. POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL AND FULL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS: 1) (PARTIAL DEPTH REPAIRS) PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 POUND CLASS SHALL NOT BE USED. 2) (FULL DEPTH REPAIRS) PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 90 POUND CLASS SHALL NOT BE USED. ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 60 POUND PNEUMATIC HAMMERS. 3) CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.
- ITEM NO. 604-10.30 AND 604-10.50 SHALL BE BID WITH THE CONTINGENCY THAT THIS ITEM MAY BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.
- IN THE AREAS OF FULL DEPTH REPAIR, THE EXISTING BEAMS AND INTERIOR OVERHANGS (CAUSED BY PHASE CONSTRUCTION) MUST BE ADEQUATELY BRACED DURING ALL PHASES OF CONSTRUCTION. DETAILS OF ANY TEMPORARY SUPPORT SYSTEM AND DESIGN CALCULATIONS SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY FULL OR PARTIAL DECK REPAIR IS BEGUN. COST TO BE INCLUDED IN ITEM NO. 602-10.05 BRACING REPAIRS, L.S.
- THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD ALL EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE OF ITEMS BID ON.
- COST OF FRAMING, CONCRETE, ABRASIVE BLASTING, LABOR AND CONCRETE REMOVAL BY PNEUMATIC HAMMERS AND ANY MISCELLANEOUS MATERIALS TO COMPLETE THE PARTIAL AND FULL DEPTH REPAIRS SHALL BE INCLUDED IN ITEM NUMBERS 604-10.30 AND 604-10.50.
- ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETERIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.
- SEE CONCRETE REPAIR DETAILS SHEET FOR REPAIR QUANTITIES FOR BEAMS AND DIAPHRAGMS. ALL DECK REPAIR QUANTITIES SHALL BE DESIGNATED BY THE BRIDGE REPAIR OFFICE AS STATED IN NOTE NO. 1 ABOVE AND SHALL BE INCLUDED IN ITEM NO. 604-10.30 OR 604-10.50.

NOTE:

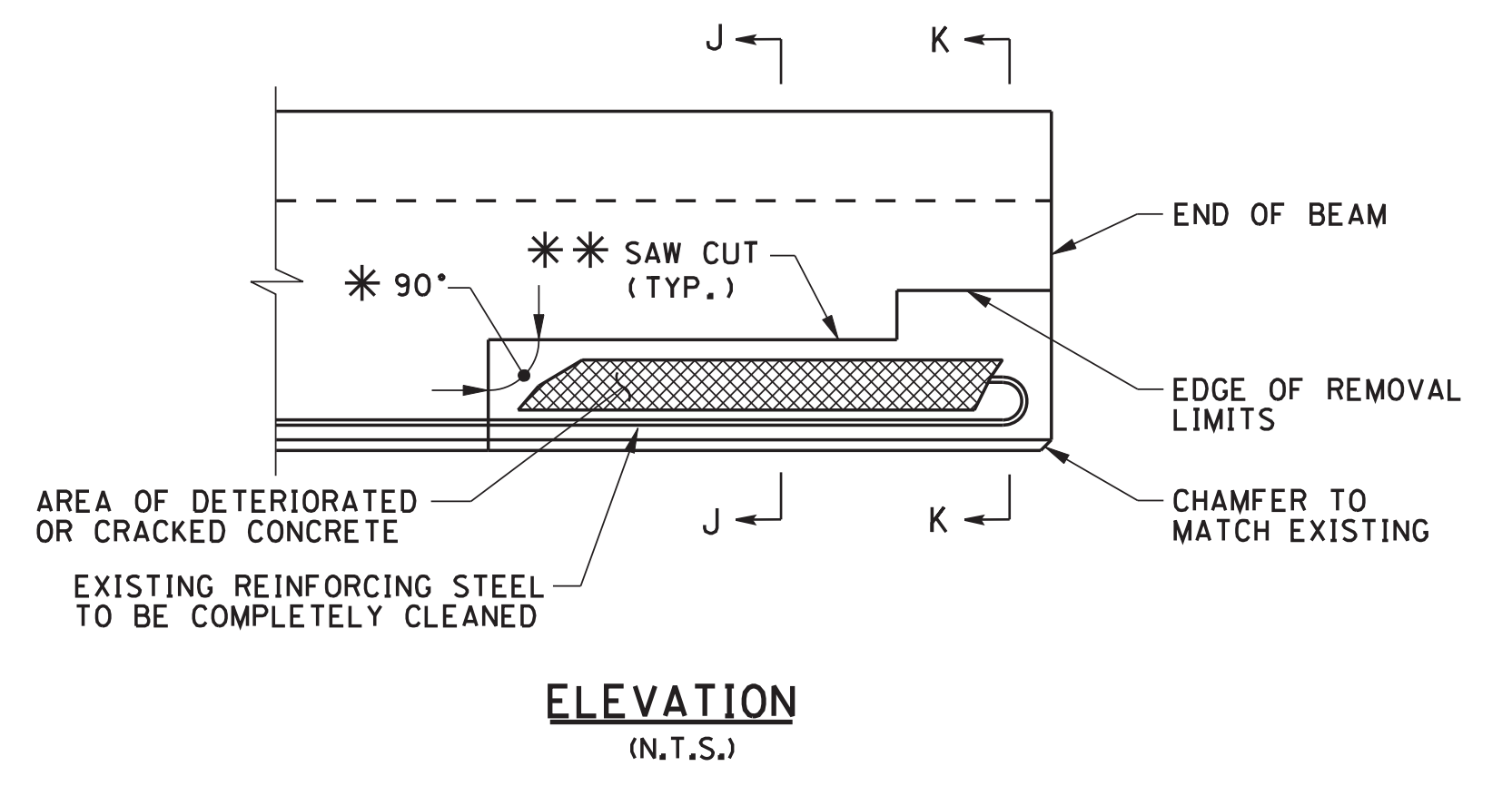
FULL AND PARTIAL DEPTH REPAIR AREAS TO BE DETERMINED BY TDOT BRIDGE REPAIR OFFICE AFTER THE REMOVAL OF THE EXISTING ASPHALT WEARING SURFACE. THE QUANTITIES SHOWN IN THE ESTIMATED BRIDGE QUANTITIES ARE AN ESTIMATE ONLY AND SHALL BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER. UNIT PRICE WILL NOT BE ADJUSTED FROM ORIGINAL BID.



SPECIAL NOTES:

- FULL DEPTH REPAIR AREAS SHALL BE LIMITED TO APPROXIMATELY 15 L.F. PER BEAM SPACING BAY LENGTH, PER REPAIR OPERATION. THIS AREA MUST BE FULLY CURED BEFORE ANY ADJACENT AREAS CAN BE REMOVED.
- FULL DEPTH REPAIRS SHALL BE COMPLETED IN NON-ADJOINING BEAM SPACING BAYS (CENTERLINE OF BEAM TO CENTERLINE OF BEAM OR PHASE CONSTRUCTION JOINT).

CONST. NO.		PROJECT NO.		YEAR		SHEET NO.	
		41002-4113-04		2012			
REVISIONS							
NO.	DATE	BY	BRIEF DESCRIPTION				



- NOTE:**
- * SAW CUTS IN BEAMS SHALL BE MADE AS DEEP AS PRACTICABLE UP TO A MAXIMUM OF 1". EXTREME CAUTION SHALL BE TAKEN TO PREVENT DAMAGE TO EXISTING REINFORCEMENT APPROXIMATE CLEARANCE TO REINFORCEMENT SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR PRIOR TO SAW CUTTING.
 - * * LIMITS AND LOCATION OF REPAIRS TO BE DESIGNATED BY THE T.D.O.T. ENGINEER. TEMPORARY BRACING MAY BE REQUIRED AND INCLUDED IN ITEM NO. 604-10.05.
 - IF WIDTH IS LESS THAN 9" THEN REMOVE CONCRETE FOR WIDTH OF GIRDER. COST TO BE INCLUDED IN PRICE BID FOR ITEM NO. 604-10.05, CONCRETE, S.F.

TYPICAL BEAM REPAIR DETAILS (N.T.S.)



04/06/2012
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

DECK REPAIR DETAILS

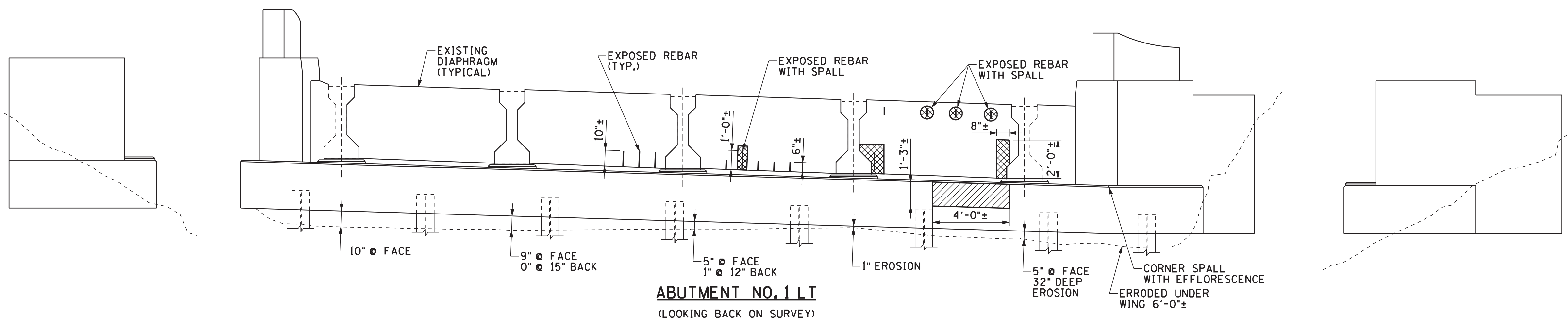
BRIDGE NOS. 41-140-1.53LT AND 41-140-1.53RT
INTERSTATE 40 OVER GARNERS CREEK
AND
BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
INTERSTATE 40 OVER GARNERS CREEK ROAD
HICKMAN COUNTY
2012

DESIGNED BY _____ NICHOLAS POWELL _____ DATE _____
DRAWN BY _____ ANGELA MOORE _____ DATE _____
SUPERVISED BY _____ LANDEL BILBREY _____ DATE _____
CHECKED BY _____ JAMIE GILLESPIE _____ DATE _____

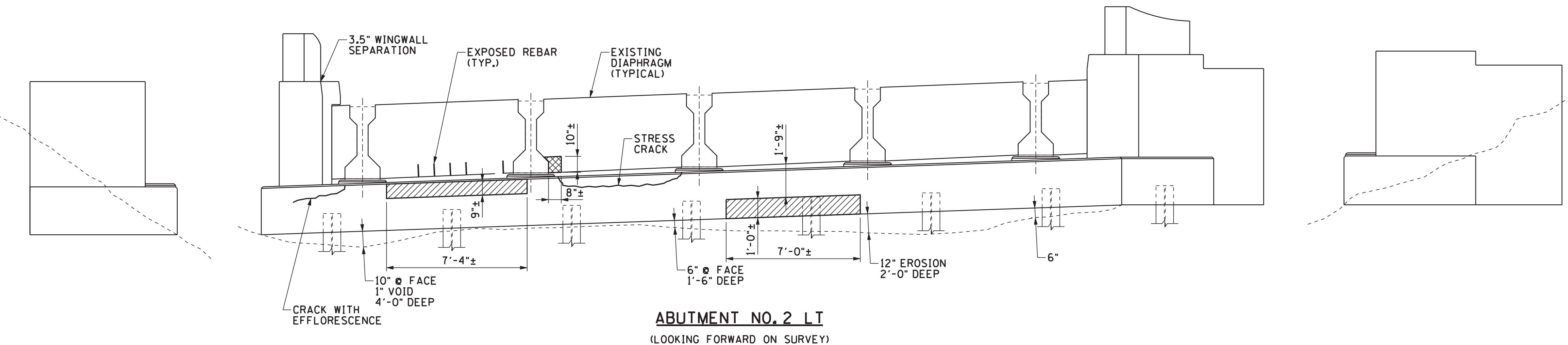


CONST. NO.	
PROJECT NO.	41002-4113-04
YEAR	2012
SHEET NO.	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



ABUTMENT NO. 1 LT
(LOOKING BACK ON SURVEY)



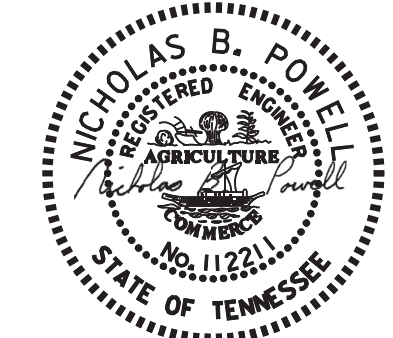
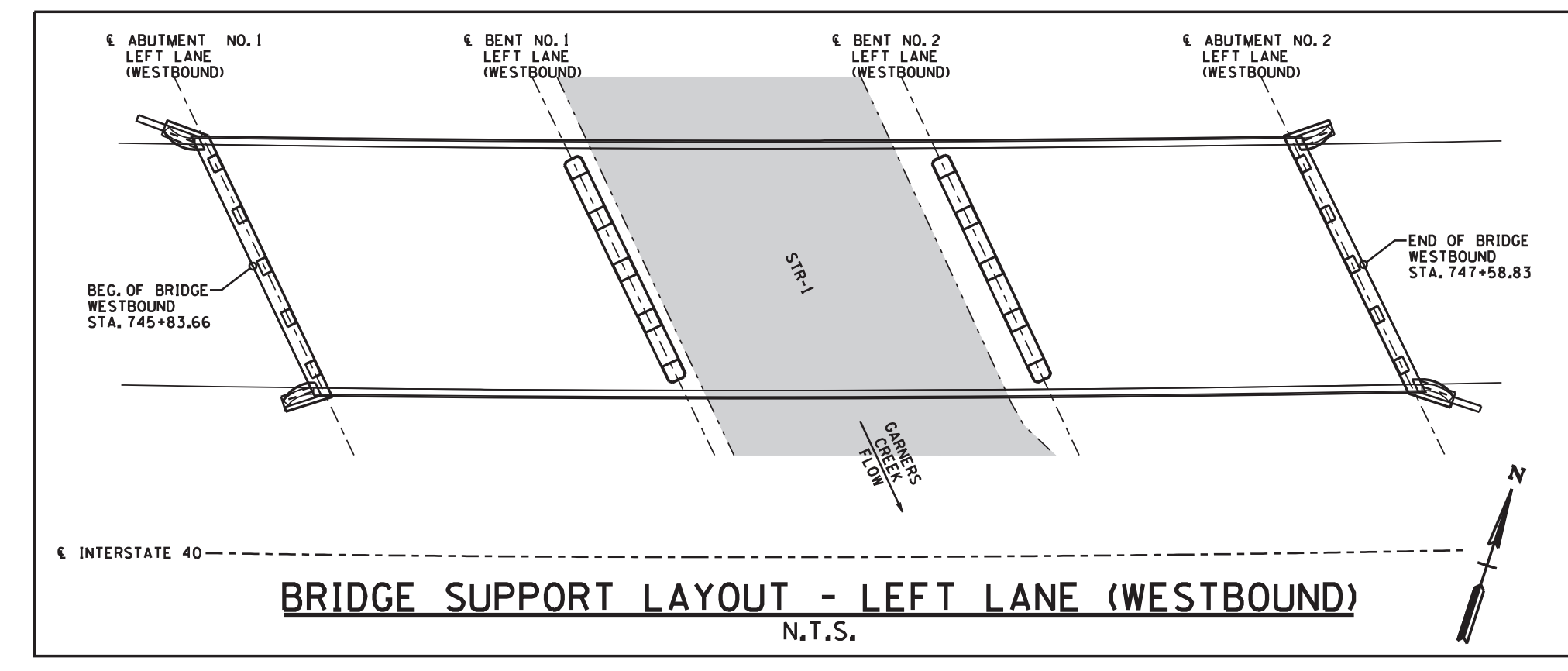
ABUTMENT NO. 2 LT
(LOOKING FORWARD ON SURVEY)

LEGEND

SPALLED SURFACES	
DELAMINATED SURFACES	
GROUND LEVEL	

- NOTES:**
- (1) REMOVE ALL DEBRIS AROUND BENTS AND ABUTMENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
 - (2) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETERIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.

NOTE: INFORMATION AND DIMENSIONS FOR CONCRETE REPAIRS ARE PROVIDED FOR ESTIMATING ONLY. TDOT ENGINEER SHALL DELINEATE ACTUAL AREAS TO BE REPAIRED.



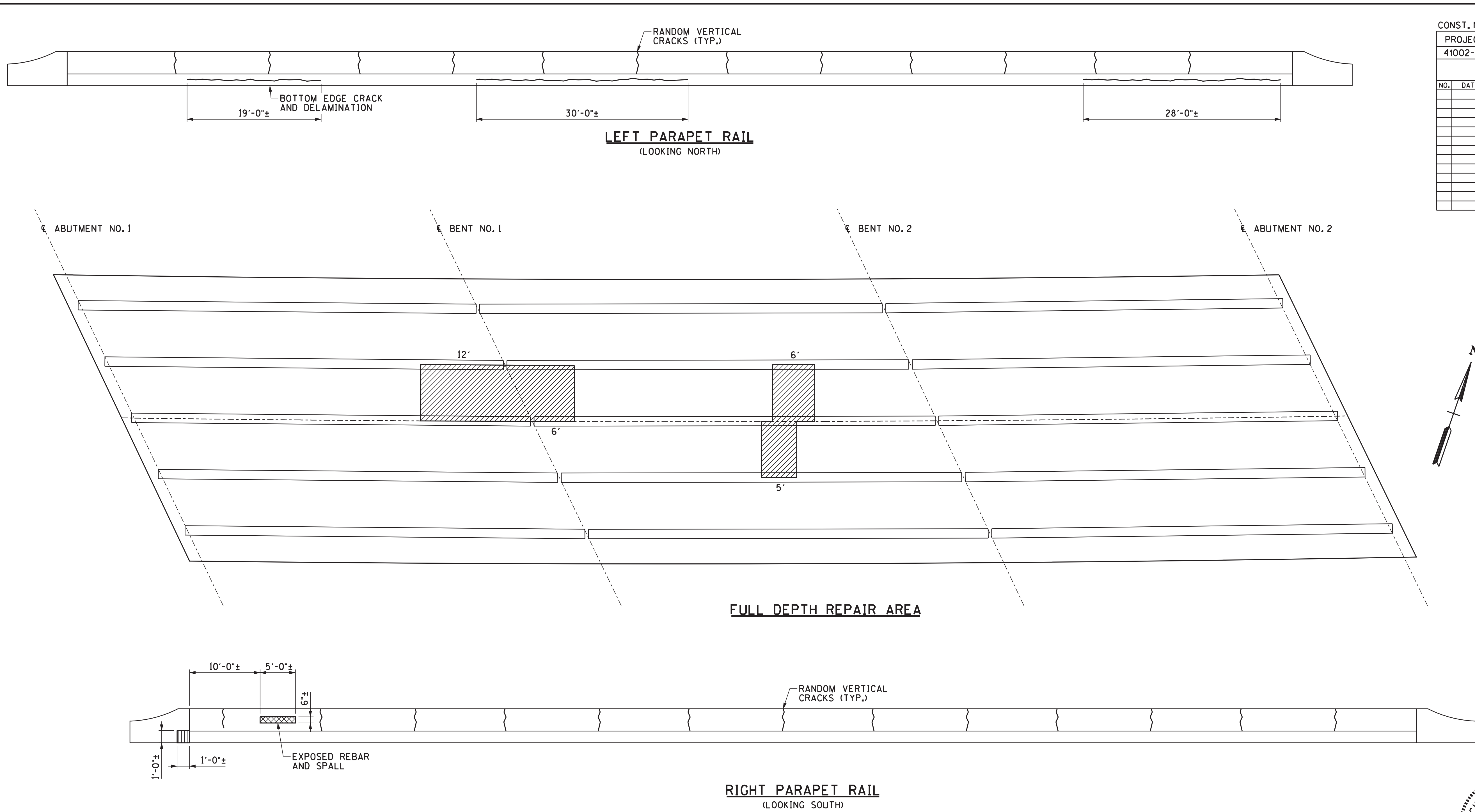
04/06/2012
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIRS
ABUTMENT NOS. 1 AND 2 LT
BRIDGE NOS. 41-140-1.53LT AND 41-140-1.53RT
INTERSTATE 40 OVER GARNERS CREEK
HICKMAN COUNTY
2012

DESIGNED BY NICHOLAS POWELL DATE _____
DRAWN BY ANGELA MOORE DATE _____
SUPERVISED BY LANDEL BILBREY DATE _____
CHECKED BY JAMIE GILLESPIE DATE _____

JA
JAMIE GILLESPIE, INC.

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
41002-4113-04	2012		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



- NOTES:**
- (1) REMOVE ALL DEBRIS AROUND BENTS AND ABUTMENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
 - (2) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETEIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.
 - (3) PARAPET RAIL DISTRESSES REFLECT A VISUAL INSPECTION ONLY. ANY ADDITIONAL DISTRESSES DISCOVERED BY THE TDOT INSPECTOR SHALL BE REPAIRED AT THE UNIT PRICE BID.

- DENOTES: ESTIMATED FULL DEPTH REPAIR AREAS
- DENOTES: SPALLED SURFACES
- DENOTES: DELAMINATED SURFACES

ESTIMATED QUANTITIES	
ITEM NO. 604-10.30 FULL DEPTH S.Y.	ITEM NO. 604-10.50 PARTIAL DEPTH S.Y.
25	100

NOTE: INFORMATION AND DIMENSIONS FOR CONCRETE REPAIRS ARE PROVIDED FOR ESTIMATING ONLY. TDOT ENGINEER SHALL DELINEATE ACTUAL AREAS TO BE REPAIRED.

NOTE: SEE CONCRETE REPAIR DETAILS SHEET FOR REPAIR QUANTITIES FOR BEAMS AND DIAPHRAGMS. ALL DECK REPAIR QUANTITIES SHALL BE DESIGNATED BY THE BRIDGE REPAIR OFFICE AS STATED IN NOTE NO. 1 ON THE DECK REPAIR DETAILS SHEET AND SHALL BE INCLUDED IN ITEM NO. 604-10.30 OR 604-10.50.

DESIGNED BY: NICHOLAS POWELL DATE: _____
 DRAWN BY: ANGELA MOORE DATE: _____
 SUPERVISED BY: LANDEL BILBREY DATE: _____
 CHECKED BY: JAMIE GILLESPIE DATE: _____



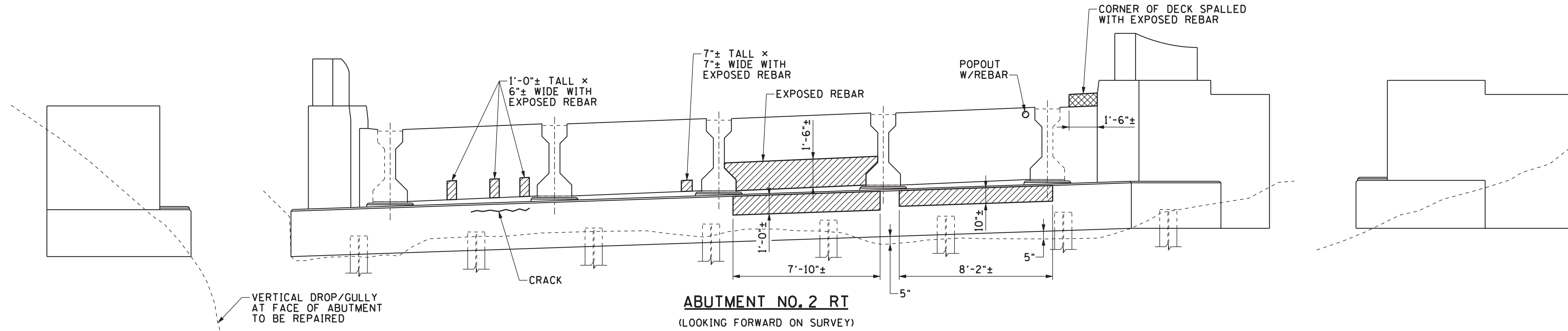
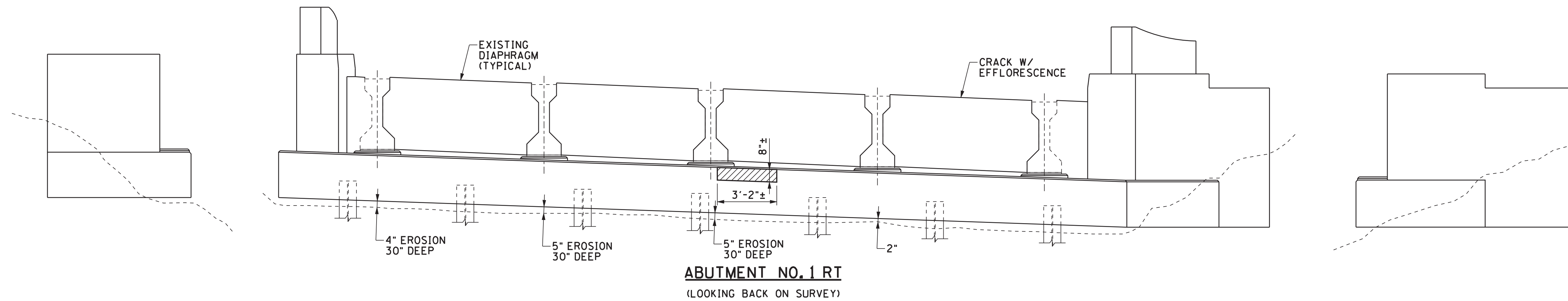
04/06/2012
 STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIRS
 SLAB & RAIL (LT LANE BRIDGE)

BRIDGE NOS. 41-140-1.53LT AND 41-140-1.53RT
 INTERSTATE 40 OVER GARNERS CREEK
 HICKMAN COUNTY
 2012

CONST. NO.		
PROJECT NO.	YEAR	SHEET NO.
41002-4113-04	2012	

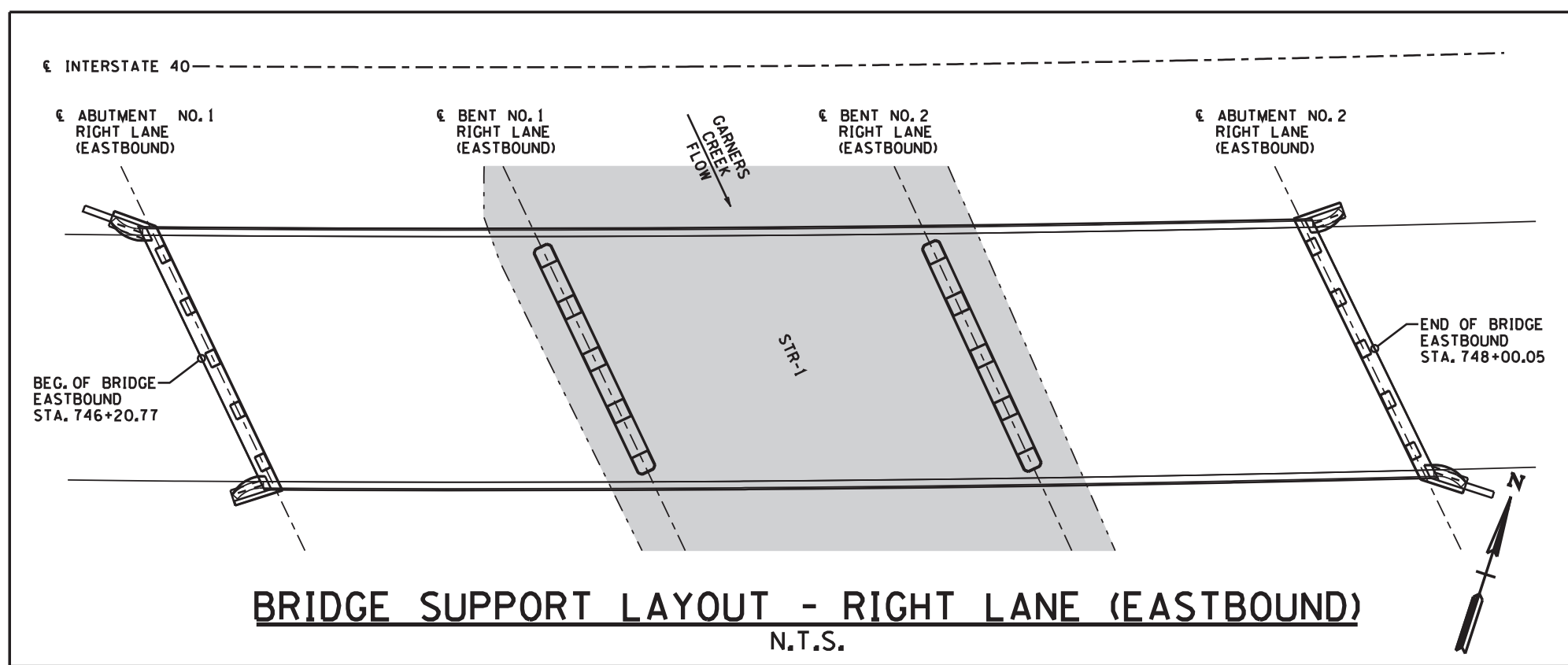
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



LEGEND	
SPALLED SURFACES	
DELAMINATED SURFACES	
GROUND LEVEL	

NOTE: INFORMATION AND DIMENSIONS FOR CONCRETE REPAIRS ARE PROVIDED FOR ESTIMATING ONLY. TDOT ENGINEER SHALL DELINEATE ACTUAL AREAS TO BE REPAIRED.

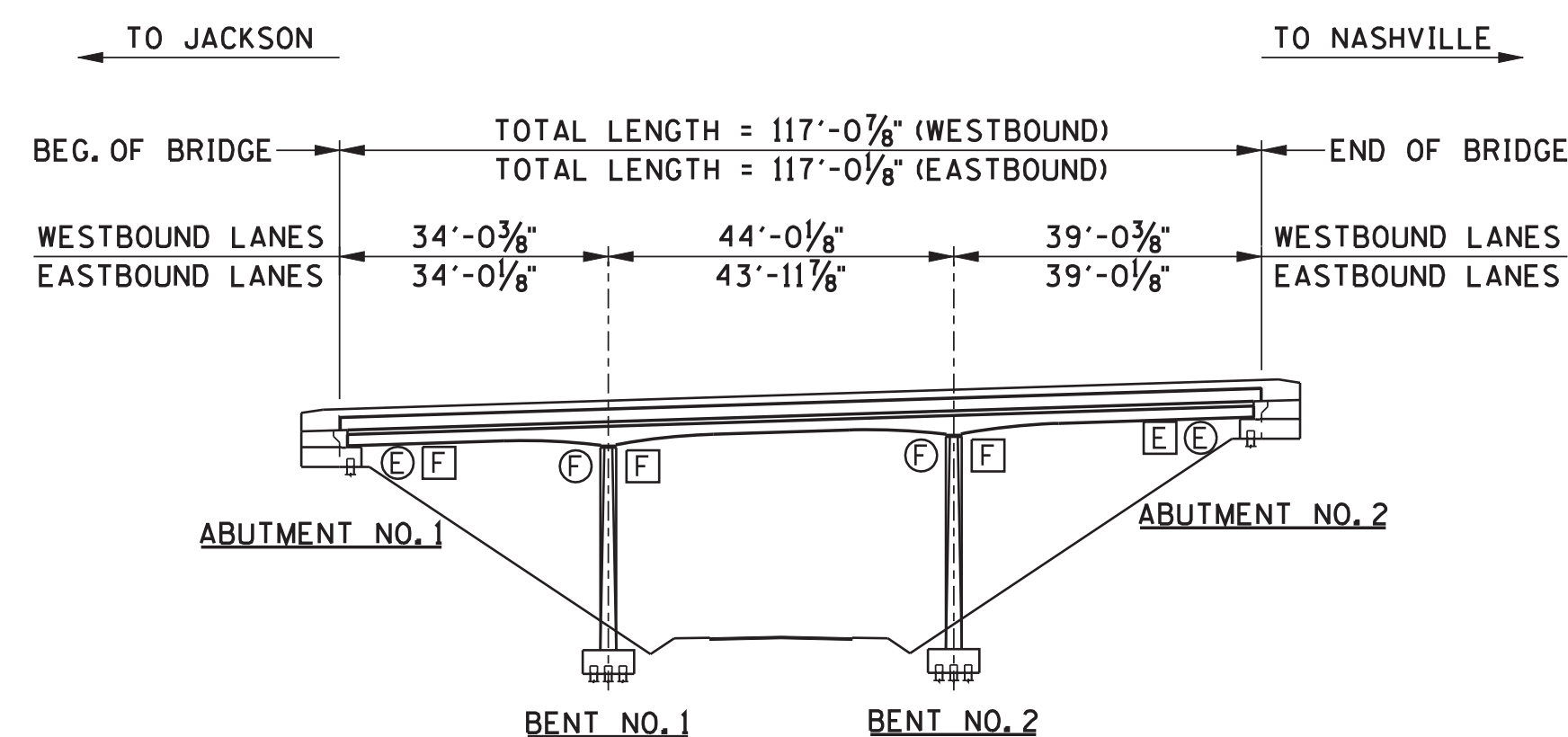
- NOTES:**
- (1) REMOVE ALL DEBRIS AROUND BENTS AND ABUTMENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
 - (2) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETERIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIRS
ABUTMENT NOS. 1 AND 2 RT
BRIDGE NOS. 41-140-1.53LT AND 41-140-1.53RT
INTERSTATE 40 OVER GARNERS CREEK
HICKMAN COUNTY
2012

DESIGNED BY NICHOLAS POWELL DATE _____
DRAWN BY ANGELA MOORE DATE _____
SUPERVISED BY LANDEL BILBREY DATE _____
CHECKED BY JAMIE GILLESPIE DATE _____

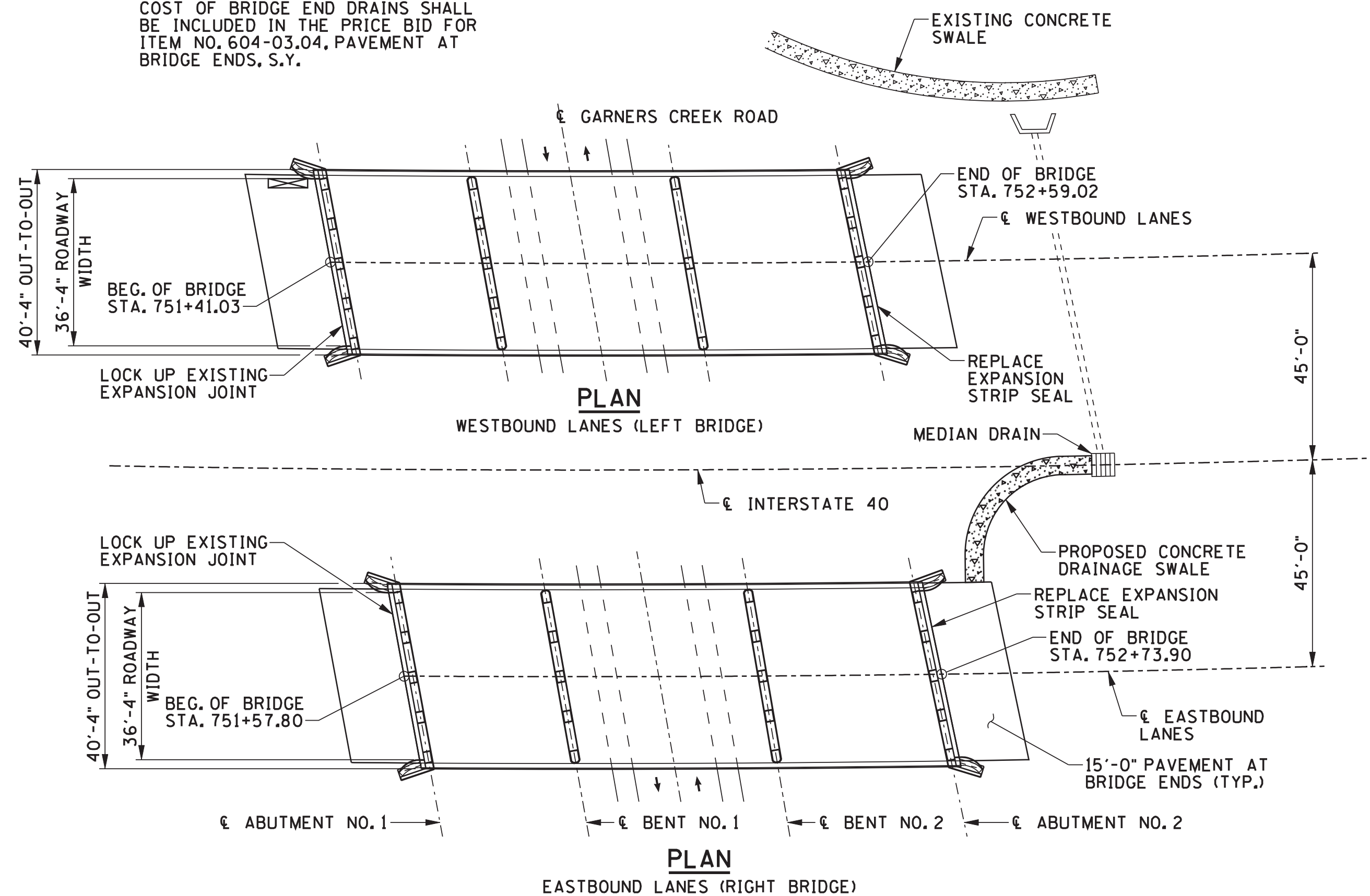


ELEVATION

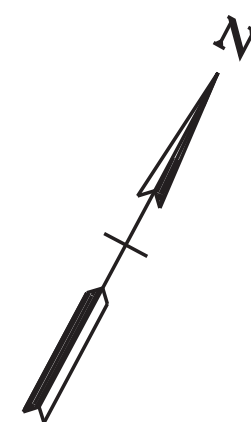
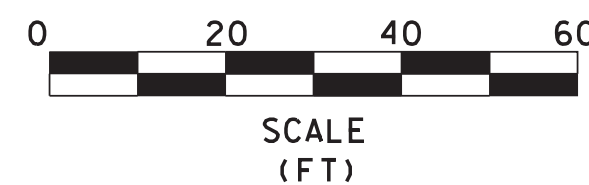
F DENOTES: FIXED
 E DENOTES: EXPANSION
 ○ DENOTES: EXISTING SUPPORT CONDITION
 □ DENOTES: PROPOSED SUPPORT CONDITION

ALL DIMENSIONS ARE BASED ON EXISTING PLAN INFORMATION AND FIELD MEASUREMENTS. PRIOR TO CONSTRUCTION, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY ALL DIMENSIONS.

▨ DENOTES: BRIDGE END DRAINS (2'-0" X 8'-7"). COST OF BRIDGE END DRAINS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-03.04, PAVEMENT AT BRIDGE ENDS, S.Y.



PLAN
 WESTBOUND LANES (LEFT BRIDGE)
PLAN
 EASTBOUND LANES (RIGHT BRIDGE)



SCOPE OF WORK

- INSTALL TRAFFIC CONTROL TO PROVIDE ONE LANE OF TRAFFIC ON I-40 DURING CONSTRUCTION PHASES.
- TRANSITION ASPHALT ROADWAY APPROACHES AS REQUIRED.
- REPLACE DAMAGED GUARDRAIL SECTIONS RUNNING BETWEEN SUBJECT BRIDGE AND I-40 BRIDGE OVER GARNERS CREEK.
- REMOVE EXISTING ASPHALT WEARING SURFACE FOR DECK REPAIR.
- REPLACE SAFETY FENCE BETWEEN LEFT AND RIGHT STRUCTURES AT BOTH ENDS.
- REPAIR BRIDGE DECK (PARTIAL AND FULL DEPTH REPAIRS) AND PROVIDE ASPHALT OVERLAY WITH MEMBRANE.
- REPLACE EXPANSION STRIP SEAL AT HIGH ABUTMENT AND ELIMINATE STRIP SEAL AT LOW ABUTMENT. SEE SHEET 2F AND BR-111-35 FOR DETAILS.
- ADD END OF BRIDGE DRAIN AT NORTHWEST CORNER OF LEFT BRIDGE.
- ADD CONCRETE SWALE DRAINAGE SYSTEM AT NORTHEAST CORNER OF RIGHT BRIDGE TO CAPTURE AND DIVERT FLOW BACK TO MEDIAN DRAIN.
- ADD 15 FOOT LONG P.A.B.E. (BOTH ENDS), MAKE P.A.B.E. 1'-0" WIDER AT NORTHEAST CORNER OF BRIDGE TO WORK IN CONJUNCTION WITH THE NEW CONCRETE DRAINAGE SWALE.
- REPLACE PAVEMENT REFLECTORS AFFECTED BY THE REPAVING OF THE APPROACHES.
- CONSTRUCT NEW GUARDRAIL AT BRIDGE ENDS WHERE REQUIRED.
- REPAIR CONCRETE SPALLS ON PARAPETS, BENTS, AND ABUTMENTS.
- FILL VOIDS WITH FLOWABLE FILL AT VARIOUS HOLES AROUND ABUTMENTS.
- SOUND SLOPE PAVEMENT. WHERE VOIDS ARE DETECTED, DRILL AND GROUT WITH FLOWABLE FILL.
- ADD UNDERPASS APPROACH AND SUBSTRUCTURE PROTECTION GUARDRAILS.
- COORDINATE WITH UTILITY OWNERS THAT AFFECT BRIDGE REPAIRS.
- CLEAR DECK DRAINS.
- HAND TOOL CLEAN AND PAINT STEEL BEARINGS.

SPECIAL NOTE

- CLIFF SWALLOW NESTS WERE DISCOVERED UNDERNEATH OF THE EXISTING BRIDGE OVER GARNERS CREEK ROAD AT LOG MILE 1.63. DISTURBANCE OF THE NESTS MUST BE PLANNED AROUND THE NESTING SEASON OF THESE BIRDS. NESTS CANNOT BE DISTURBED WHILE EGGS OR YOUNG ARE PRESENT IN THE NEST. THE APPROXIMATE DATE FOR NESTING ACTIVITIES TO BEGIN IS APRIL 15 AND FLEDGING IS JULY 3. NOTE THAT THESE ARE APPROXIMATE DATES AND NESTING COULD OCCUR EARLIER AND LAST UNTIL LATER.

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
41002-4113-04	2012		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	10-3-12	LKB	REVISED NOTES 1 AND 7 AND LATEST REVISION DATE
2	12-19-12	DWT	LATEST REVISION DATE

LIST OF DRAWINGS

	DWG. NO.	LAST REV. DATE
LAYOUT OF BRIDGE TO BE REPAIRED (BR NOS. 41-140-1.63LT & RT)	BR-111-33	12-19-12
PHASING (BR. NOS. 41-140-1.63LT & RT)	BR-111-34	10-3-12
SUPERSTRUCTURE - EXPANSION JOINT REPLACEMENT	BR-111-35	10-3-12
CONCRETE REPAIR - ABUTMENT NOS. 1 AND 2 LT SOUNDING OF CONCRETE SLOPES	BR-111-36	
CONCRETE REPAIR - ABUTMENT NOS. 1 AND 2 RT SOUNDING OF CONCRETE SLOPES	BR-111-37	
CONCRETE REPAIRS - ABUTMENT NOS. 1 AND 2 LT	BR-111-38	
CONCRETE REPAIRS - BENT NO. 1 LT	BR-111-39	
CONCRETE REPAIRS - BENT NO. 2 LT	BR-111-40	
CONCRETE REPAIRS - SLAB AND RAIL (LT LANE BRIDGE)	BR-111-41	
CONCRETE REPAIRS - ABUTMENT NOS. 1 AND 2 RT	BR-111-42	
CONCRETE REPAIRS - BENT NO. 2 RT	BR-111-43	
CONCRETE REPAIRS - SLAB AND RAIL (RT LANE BRIDGE)	BR-111-44	
CONCRETE REPAIR DETAILS	BR-111-45	
BILL OF STEEL	BR-111-46	

LIST OF REFERENCE DRAWINGS

	DWG. NO.
EXISTING BRIDGE PLANS	K-31-50 THRU K-31-56, K-15-60 BR-37-49, BR-37-51, BR-37-52 BR-65-96 THRU BR-65-101

LIST OF BRIDGE APPURTENANCES

	DWG. NO.	LAST REV. DATE
BRIDGE RAILING CONCRETE PARAPET	STD-1-1	07-31-00
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STD-1-5	06-01-11
BRIDGE END DRAIN W/ PABE	STD-1-6	04-28-97
BRIDGE END DRAIN DETAILS 2'x8'x7" AND 4'x8'x7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-7	07-31-00
BRIDGE END DRAIN 2'x8'x7" W/ PABE	STD-1-8	05-01-95
REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLABS	STD-9-1	10-07-08
GENERAL NOTES AND DETAILS FOR EXPANSION JOINT REPLACEMENT CONSTRUCTION TYPES "A" THRU "J" - 1991	SBR-2-115	01-04-96
GENERAL DETAILS FOR STRIP SEAL EXPANSION JOINT REPLACEMENT CONSTRUCTION DETAILS TYPES "A" THRU "J" - 1991	SBR-2-116	01-04-96
STRIP SEAL EXPANSION JOINT REPLACEMENT CONSTRUCTION DETAILS TYPE "E" AND "F" - 1991	SBR-2-119	05-30-96
DETAILS SHOWING GUARDRAIL ATTACHMENT AT BRIDGE ENDS TO EXISTING CONCRETE SLOPE FACE END POST - 1989	SBR-2-131	01-22-02
DETAILS SHOWING GUARDRAIL ATTACHMENT AT BRIDGE ENDS TO EXISTING CONCRETE SLOPE FACE END POST - 1989	SBR-2-132	01-04-96
STANDARD SHOWING DETAILS FOR ATTACHING NEW GUARDRAIL TO EXISTING END OF BRIDGE - 1992	SBR-2-137	11-05-01
STANDARD SHOWING DETAILS FOR ATTACHING NEW GUARDRAIL AT EXISTING BRIDGE END AND ALONG EXISTING BRIDGE RAIL - 1992	SBR-2-138	11-05-01
STANDARD SHOWING DETAILS FOR ATTACHING NEW GUARDRAIL ALONG EXISTING BRIDGE RAILS - 1992	SBR-2-140	11-05-01

LIST OF SPECIAL PROVISIONS

DWG. NO.	LAST REVISION DATE	DESCRIPTION
108B	**	PROJECT COMPLETION AND DISINCENTIVE

** DENOTES: CURRENT REVISION DATE AS PER CONTRACT DRAWINGS.



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

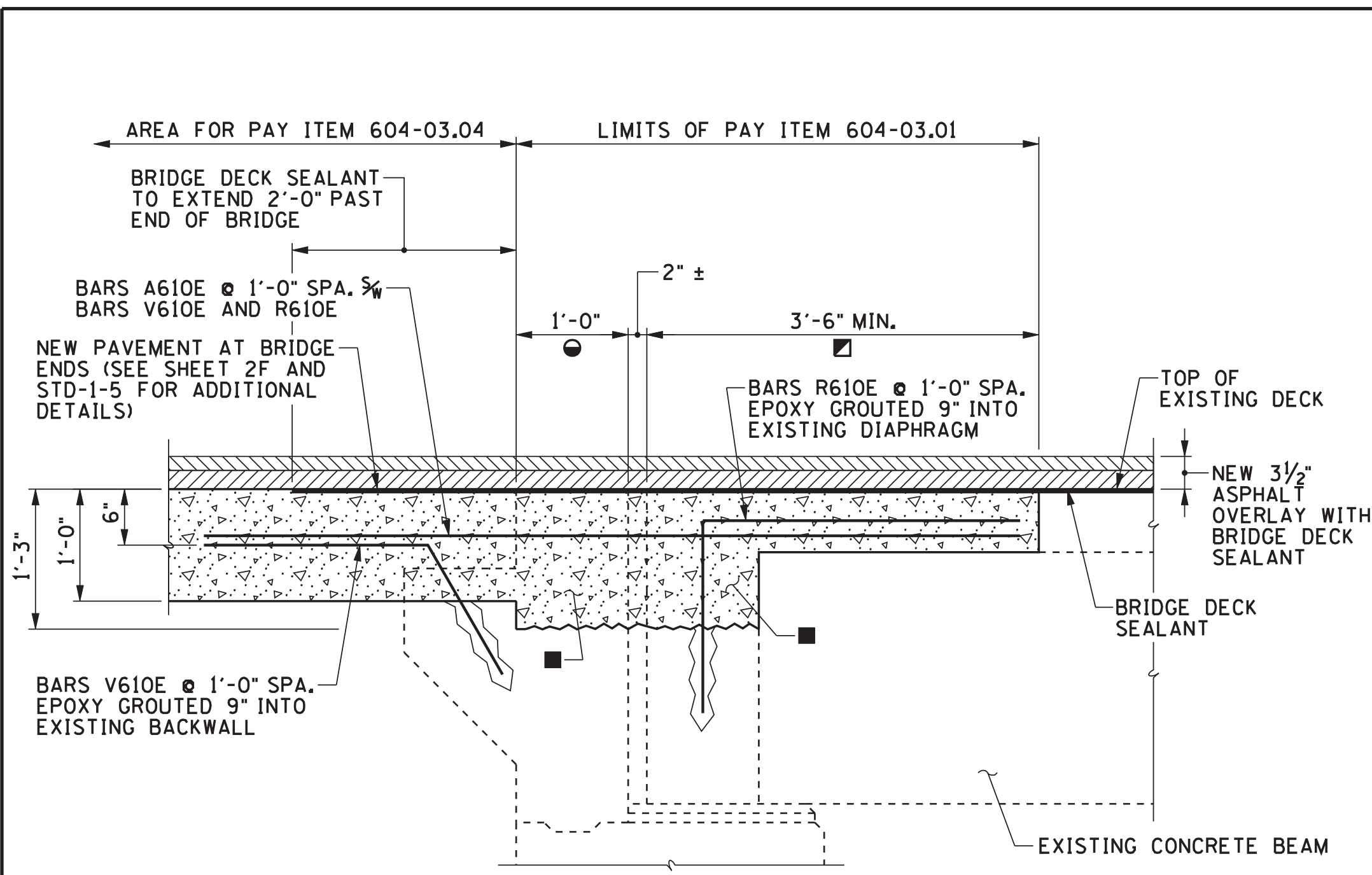
LAYOUT OF BRIDGE
 TO BE REPAIRED

BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
 INTERSTATE 40 OVER GARNERS CREEK ROAD
 HICKMAN COUNTY
 2012

DESIGNED BY: NICHOLAS POWELL
 DRAWN BY: ANGELA MOORE
 SUPERVISED BY: LANDEL BILBREY
 CHECKED BY: JAMIE GILLESPIE



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
41002-4113-04	2012		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	10-3-12	LKB	REVISED EXPANSION JOINT REPAIR DETAIL AND NOTES AND SECTION OF TYPE "E" EXPANSION JOINT REMOVAL & JOINT LOCK UP DETAIL



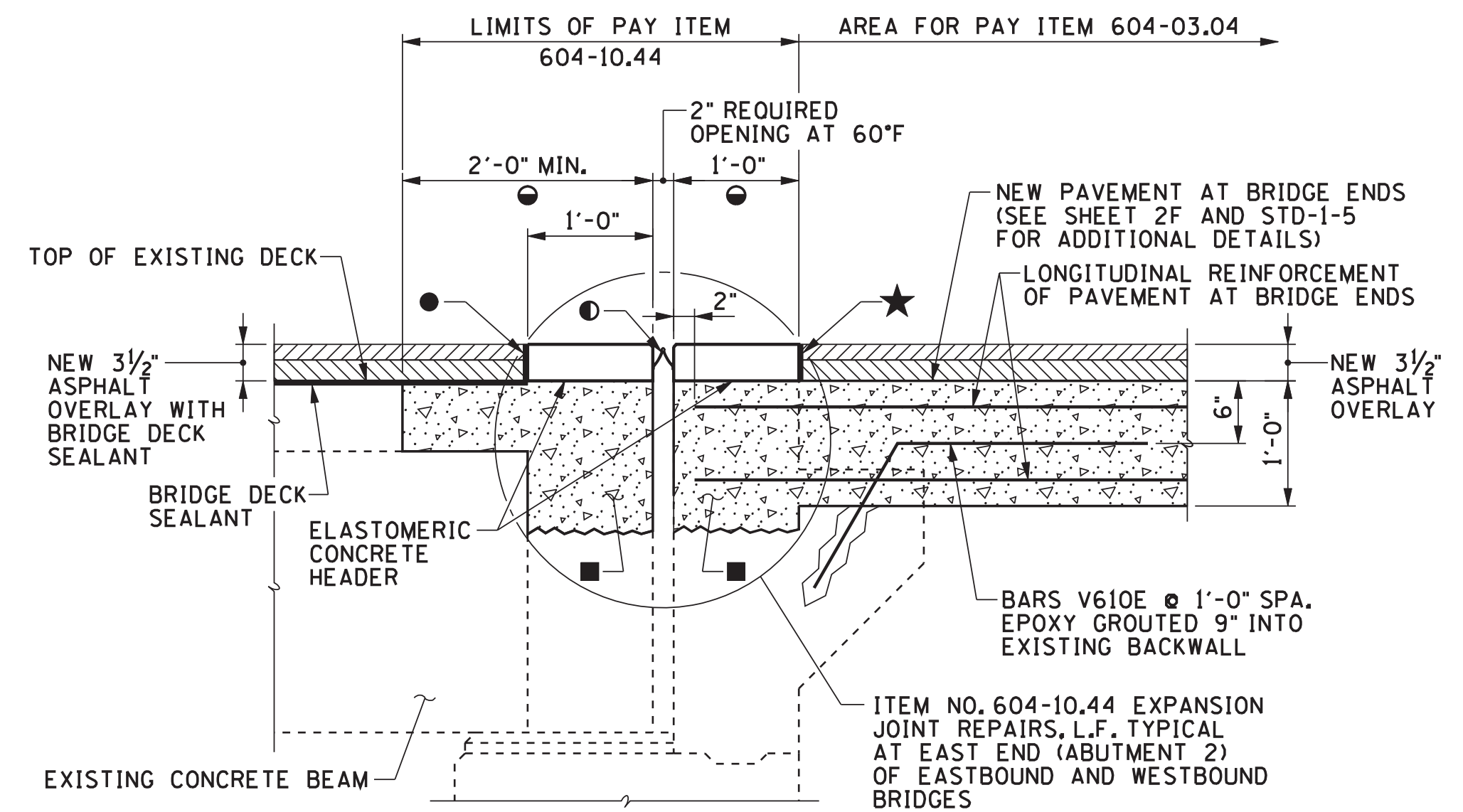
SECTION OF TYPE "E" EXPANSION JOINT REMOVAL AND JOINT LOCK UP

TYPICAL AT WEST END (ABUTMENT 1) OF EASTBOUND AND WESTBOUND BRIDGES
N.T.S.

- DENOTES: NEW BRIDGE DECK SEALANT MEMBRANE REQUIRED WITH MASTIC AS RECOMMENDED BY MANUFACTURER OF MEMBRANE. SEE STANDARD SPECIFICATIONS, ARTICLE 906.04.
- DENOTES: LIMITS OF EXISTING CONCRETE DECK REMOVAL AND RECONSTRUCTION.
- DENOTES: LIMITS OF EXISTING CONCRETE DECK AND PARAPET RAIL REMOVAL AND RECONSTRUCTION.
- DENOTES: EXISTING REINFORCING STEEL TO REMAIN AND BE BLAST CLEANED PRIOR TO POURING NEW CONCRETE.

NOTES:

1. PROVISIONS SHALL BE MADE BY THE CONTRACTOR TO ENSURE THAT THE VERTICAL ALIGNMENT OF THE CONCRETE SLAB END SURFACES CONFORM TO THE EXISTING ROADWAY PROFILE.
2. CONCRETE FOR EXPANSION JOINT REPAIR SHALL BE HIGH EARLY STRENGTH CONCRETE $f'c = 3,500$ P.S.I. AT 28 DAY STRENGTH. TRAFFIC LOADS SHALL NOT BE PERMITTED ON ANY OF THE REPAIRED AREAS UNTIL TEST CYLINDERS ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 3,000 P.S.I. AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF EIGHTEEN (18) HOURS.
3. THE COST OF REMOVING PORTIONS OF THE EXISTING BACKWALL AND DECK SLAB AND BLAST CLEANING EXISTING REINFORCING STEEL WITHIN THE LIMITS SHOWN SHALL BE INCLUDED IN ITEM NOS. 202-04.01, REMOVAL OF STRUCTURES (BR. NO. 41-140-1.63LT AND RT). COST OF REINFORCING STEEL SHALL BE INCLUDED IN ITEM NO. 604-02.03, EPOXY COATED REINFORCING STEEL, LB. COST OF HIGH EARLY STRENGTH CONCRETE, FORMING, LABOR, AND ALL OTHER MISCELLANEOUS MATERIALS NECESSARY FOR COMPLETING THE JOINT LOCK UP SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-03.01, CLASS "A" CONCRETE (BRIDGES), C.Y.
4. NEW EXPANSION JOINT LOCK UP SHALL BE CONTINUOUS ACROSS THE ENTIRE BRIDGE WIDTH. SEE PARAPET RAIL REPLACEMENT DETAIL THIS SHEET FOR ADDITIONAL DETAILS. PAVEMENT AT BRIDGE ENDS SHALL ABUT EXISTING WINGPOST.



EXPANSION JOINT REPAIR

TYPICAL AT EAST END (ABUTMENT 2) OF EASTBOUND AND WESTBOUND BRIDGES
(SEE SECTION "C-C" ON SHEET 2F)
N.T.S.

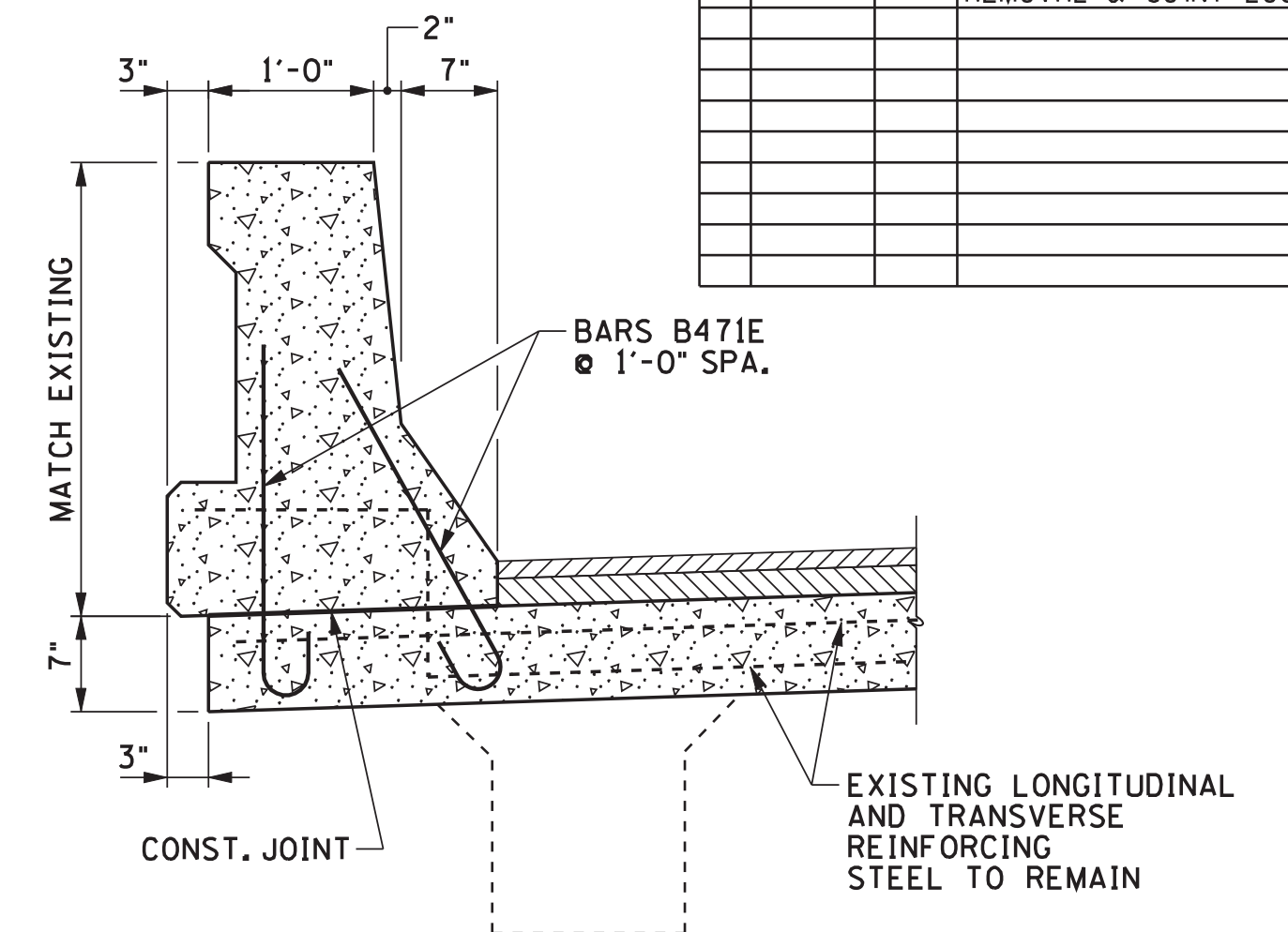
- DENOTES: NEW BRIDGE DECK SEALANT MEMBRANE REQUIRED WITH MASTIC AS RECOMMENDED BY MANUFACTURER OF MEMBRANE. SEE STANDARD SPECIFICATIONS, ARTICLE 906.04.
- DENOTES: WABO SPS-225 EXPANSION JOINT OR APPROVED EQUAL
- DENOTES: LIMITS OF EXISTING CONCRETE REMOVAL AND RECONSTRUCTION.
- DENOTES: EXISTING REINFORCING STEEL TO REMAIN AND BE BLAST CLEANED PRIOR TO POURING NEW CONCRETE.
- ★ DENOTES: MASTIC AS RECOMMENDED BY MANUFACTURER OF MEMBRANE. SEE STANDARD SPECIFICATIONS, ARTICLE 906.04. COST TO BE BID UNDER ITEM NO. 617-01.

NOTES:

1. EXPANSION GAP WILL CHANGE WITH TEMPERATURE AND THE CONTRACTOR SHALL COMPENSATE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
2. TOP 6" OF JOINT SHALL BE BLAST CLEANED TO REMOVE LATTANCE AND OTHER DELETERIOUS MATERIAL BEFORE INSTALLATION OF EXPANSION JOINT. ALL DEBRIS AND EXISTING JOINT MATERIAL SHALL BE REMOVED FOR THE FULL DEPTH OF THE EXISTING JOINT MATERIAL.

RESETTING EXISTING EXPANSION JOINT:

1. INCLUDES FURNISHING ALL MATERIALS AND EQUIPMENT, COMPLETE INSTALLATION AND IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. DETAILED SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER PRIOR TO CONSTRUCTION. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP AND PERFORMANCE OF THE INSTALLED JOINT.
2. COST OF EXPANSION JOINT REPAIRS TO BE INCLUDED IN ITEM NO. 604-10.44, EXPANSION JOINT REPAIRS, L.F.



PARAPET RAIL REPLACEMENT DETAIL

TYPICAL FOR 3'-6" AT WEST END (ABUTMENT 1) OF EASTBOUND AND WESTBOUND BRIDGES
N.T.S.

NOTES:

1. REGARDLESS OF THE DIMENSIONS SHOWN ABOVE, THE NEW PARAPET RAIL SHALL MATCH THE EXISTING PARAPET RAIL.
2. FOR ADDITIONAL REINFORCING STEEL IN PARAPET, SEE STD-1-1.
3. EXISTING LONGITUDINAL REINFORCING STEEL IN PARAPET RAIL SHALL BE CLEANED AND REINCORPORATED INTO NEW PARAPET. BARS A401E, AS SHOWN IN STD-1-1, SHALL BE SPLICED TO EXISTING LONGITUDINAL REINFORCING STEEL. BARS HP401E SHALL BE INSTALLED AT 1'-0" SPA, AS SHOWN IN STD-1-1.
4. PROVISIONS SHALL BE MADE FOR NEW GUARDRAIL INSERTION ASSEMBLY/ATTACHMENT PLATE WHEN FORMING NEW PARAPET RAIL ON BRIDGE 41-140-1.63RT.
5. EXISTING WINGPOST AND WINGWALLS SHALL REMAIN AND SHALL NOT BE REPLACED. NEW PARAPET RAIL SHALL BE REPLACED FOR THE 3'-6" LIMITS OF THE JOINT LOCK UP.
6. INSTALL ONE (1) LAYER OF FELT PAPER BETWEEN THE CONSTRUCTION JOINT OF THE EXISTING WING PARAPET AND THE NEW PARAPET RAIL.
7. CONCRETE FOR EXPANSION JOINT REPAIR SHALL BE HIGH EARLY STRENGTH CONCRETE $f'c = 3,500$ P.S.I. AT 28 DAY STRENGTH. TRAFFIC SHALL NOT BE PERMITTED ON THE ADJACENT LANE TO THE REPAIRED AREAS UNTIL TEST CYLINDERS ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 3,000 P.S.I. AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF EIGHTEEN (18) HOURS.
8. THE COST OF REMOVING PORTIONS OF THE EXISTING PARAPET RAIL AND BLAST CLEANING EXISTING LONGITUDINAL REINFORCING STEEL SHALL BE INCLUDED IN ITEM NO. 202-04.01, REMOVAL OF STRUCTURES (BR. NO. 41-140-1.63LT AND RT). COST OF BARS B471E SHALL BE INCLUDED IN ITEM NO. 604-02.03, EPOXY COATED REINFORCING STEEL, LB. COST OF ALL OTHER REINFORCING STEEL, HIGH EARLY STRENGTH CONCRETE, FORMING, LABOR, AND ALL OTHER MISCELLANEOUS MATERIALS NECESSARY FOR CONSTRUCTING THE NEW PARAPET RAIL SECTIONS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 620-10.01, CONCRETE PARAPET (BRIDGE RAIL REPLACEMENT), L.F.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE
EXPANSION JOINT REPLACEMENT

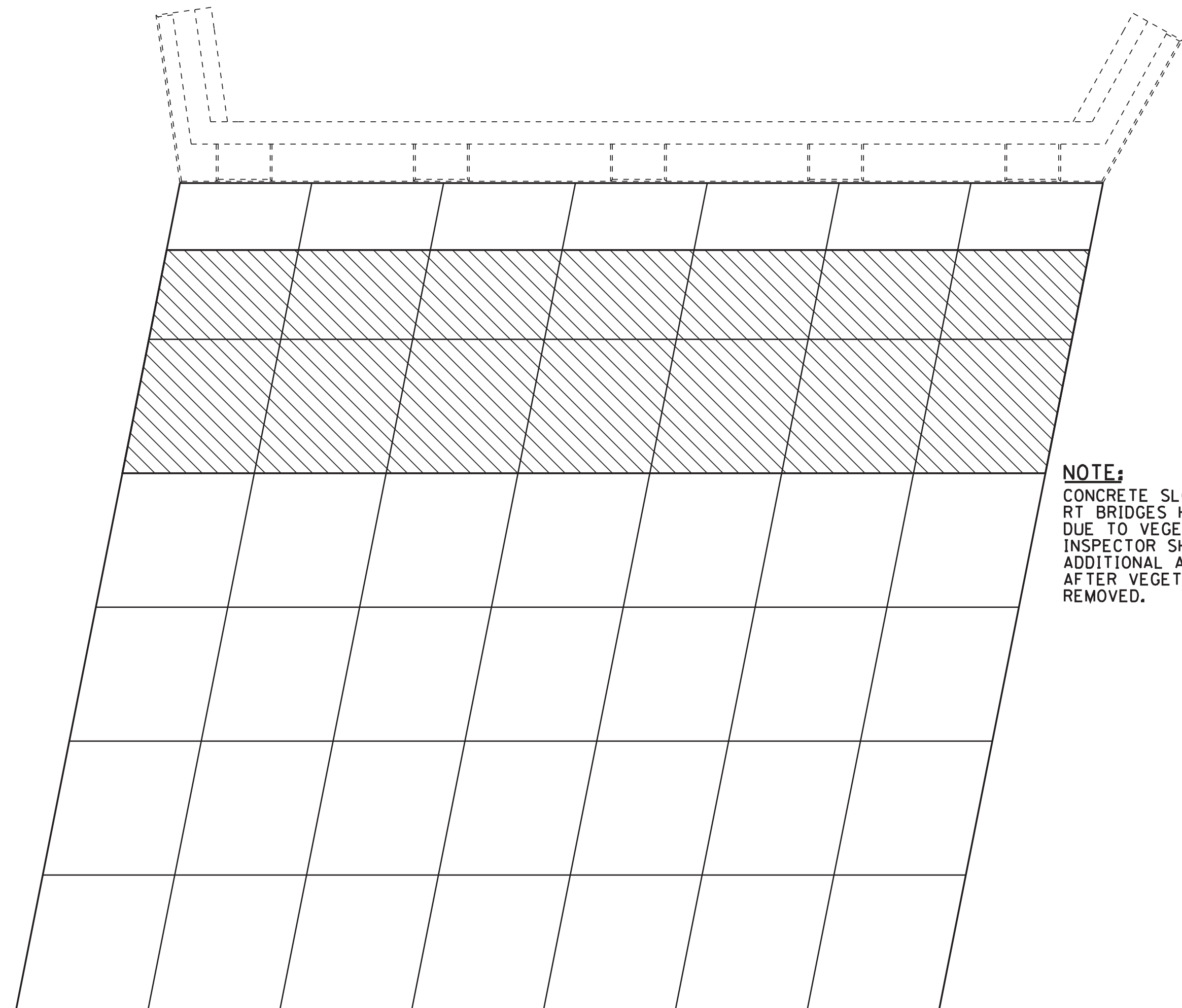
BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
INTERSTATE 40 OVER GARNERS CREEK ROAD
HICKMAN COUNTY
2012

DESIGNED BY: NICHOLAS POWELL
DRAWN BY: ANGELA MOORE
SUPERVISED BY: LANDEL BILBREY
CHECKED BY: JAMIE GILLESPIE

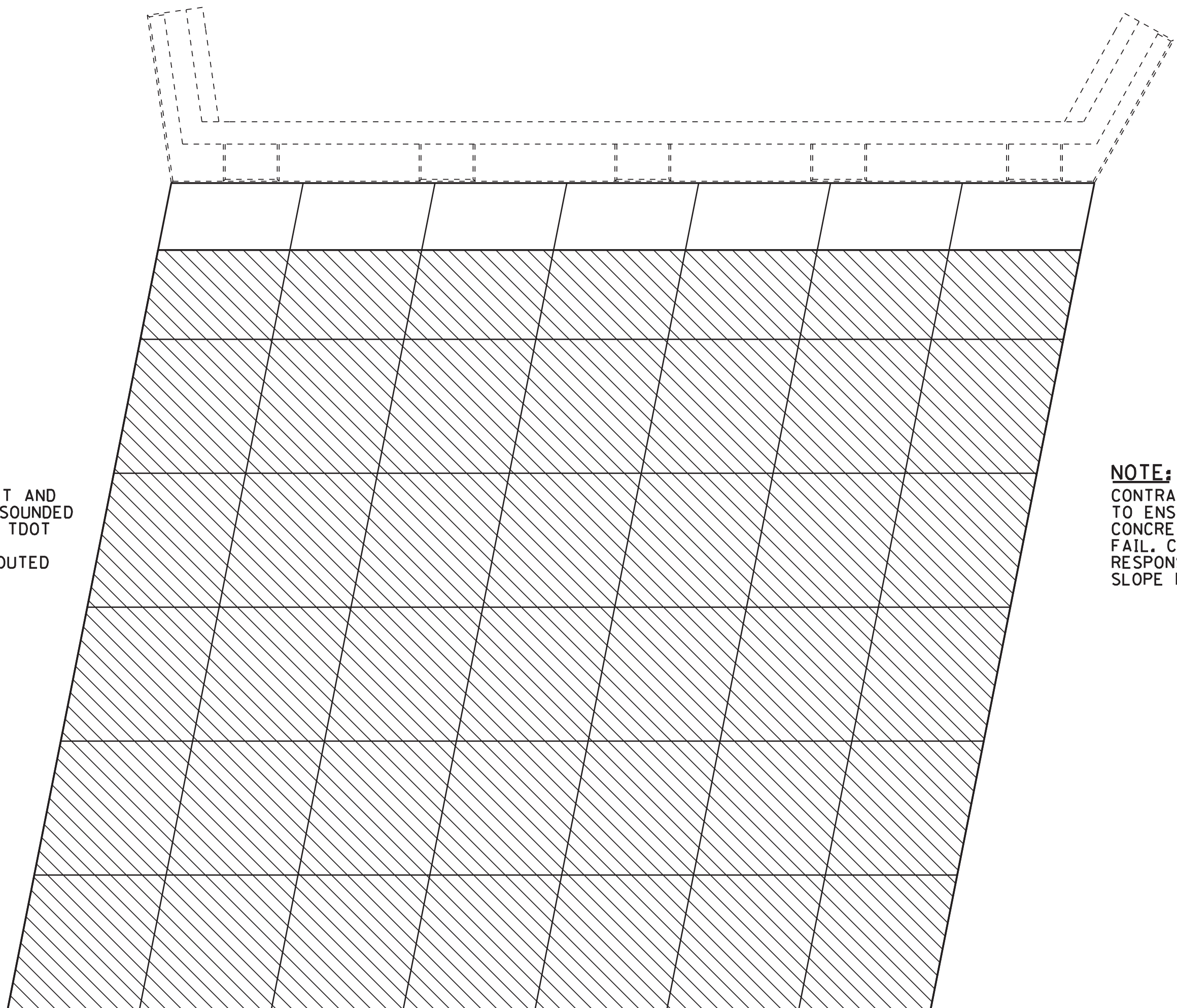


CONST. NO.		
PROJECT NO.	YEAR	SHEET NO.
41002-4113-04	2012	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



ABUTMENT NO. 1 RT
(LOOKING BACK ON SURVEY)



ABUTMENT NO. 2 RT
(LOOKING FORWARD ON SURVEY)

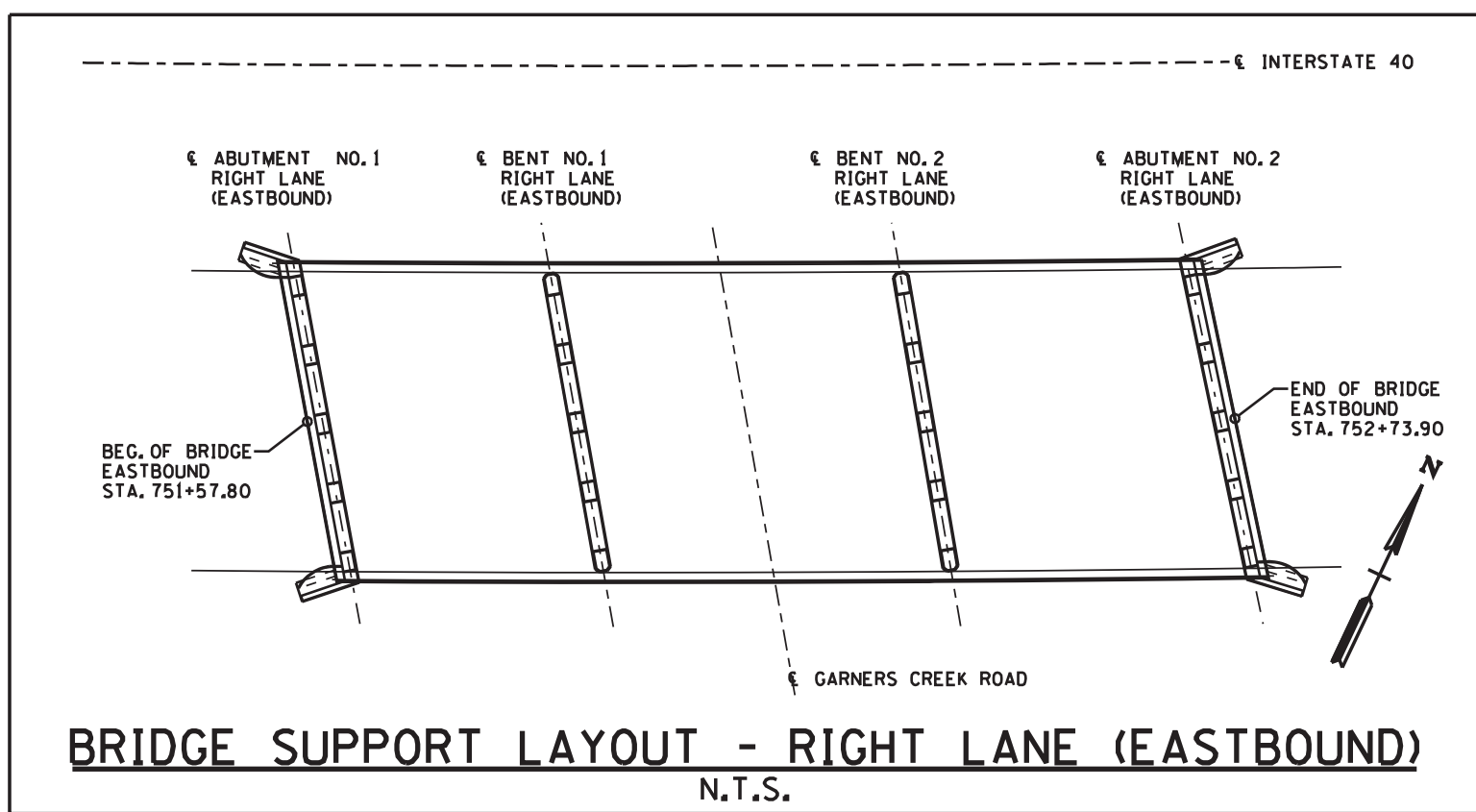
NOTE:
CONCRETE SLOPES BETWEEN LT AND RT BRIDGES HAVE NOT BEEN SOUNDED DUE TO VEGETATION GROWTH. TDOT INSPECTOR SHALL DESIGNATE ADDITIONAL AREAS TO BE GROUTED AFTER VEGETATION HAS BEEN REMOVED.

NOTE:
CONTRACTOR SHALL PHASE GROUTING TO ENSURE THAT THE EXISTING CONCRETE SLOPE PAVING DOES NOT FAIL. CONTRACTOR WILL BE RESPONSIBLE FOR ANY DAMAGES TO SLOPE FACE.

LEGEND	
SPALLED SURFACES	
DELAMINATED SURFACES	
AREAS TO BE GROUTED	

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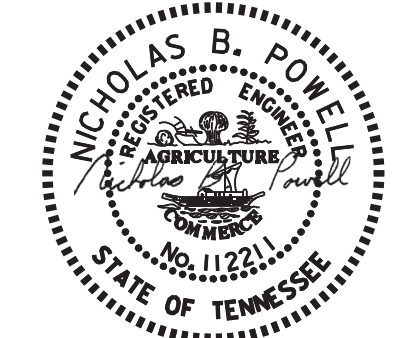
- NOTES:**
- (1) TDOT ENGINEER SHALL DESIGNATE AREAS OF CONCRETE SLOPES TO BE CORED AND GROUTED. COST OF CORES AND GROUTING IS TO BE INCLUDED IN ITEM NO. 502-03.01 CEMENT GROUTING, C.F.
 - (2) REMOVE ALL DEBRIS AROUND BENTS AND ABUTMENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
 - (3) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETERIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.



BRIDGE SUPPORT LAYOUT - RIGHT LANE (EASTBOUND)
N.T.S.

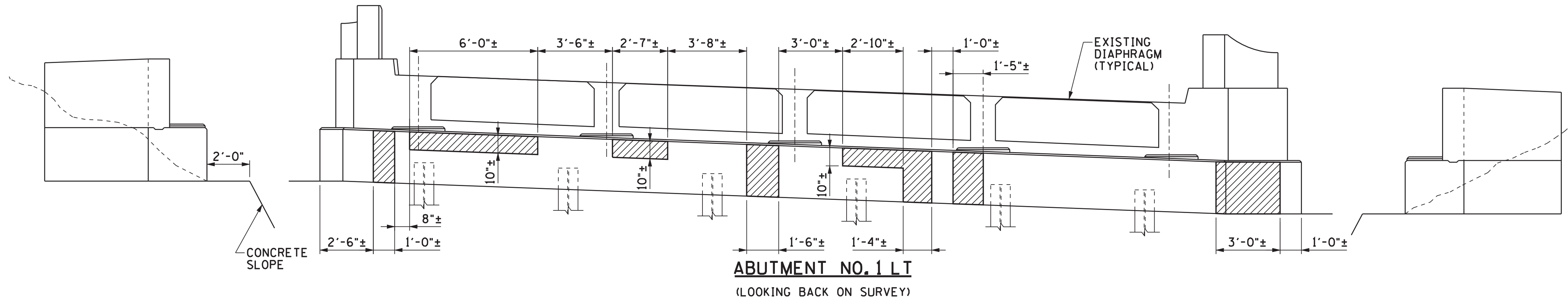
DESIGNED BY: NICHOLAS POWELL
 DRAWN BY: ANGELA MOORE
 SUPERVISED BY: LANDEL BILBREY
 CHECKED BY: JAMIE GILLESPIE

DATE: _____
 DATE: _____
 DATE: _____
 DATE: _____

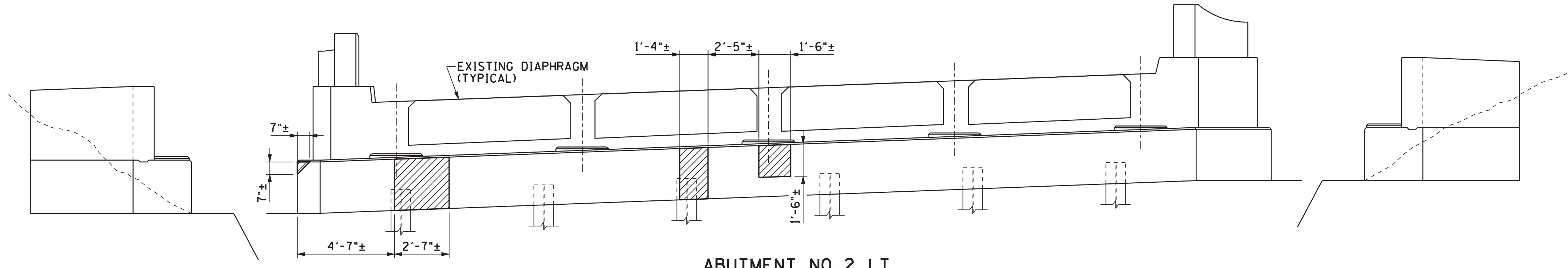


04/06/2012
 STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 CONCRETE REPAIRS
 ABUTMENT NOS. 1 AND 2 RT
 SOUNDING OF CONCRETE SLOPES
 BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
 INTERSTATE 40 OVER GARNERS CREEK ROAD
 HICKMAN COUNTY
 2012

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41002-4113-04	2012		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



ABUTMENT NO. 1 LT
(LOOKING BACK ON SURVEY)

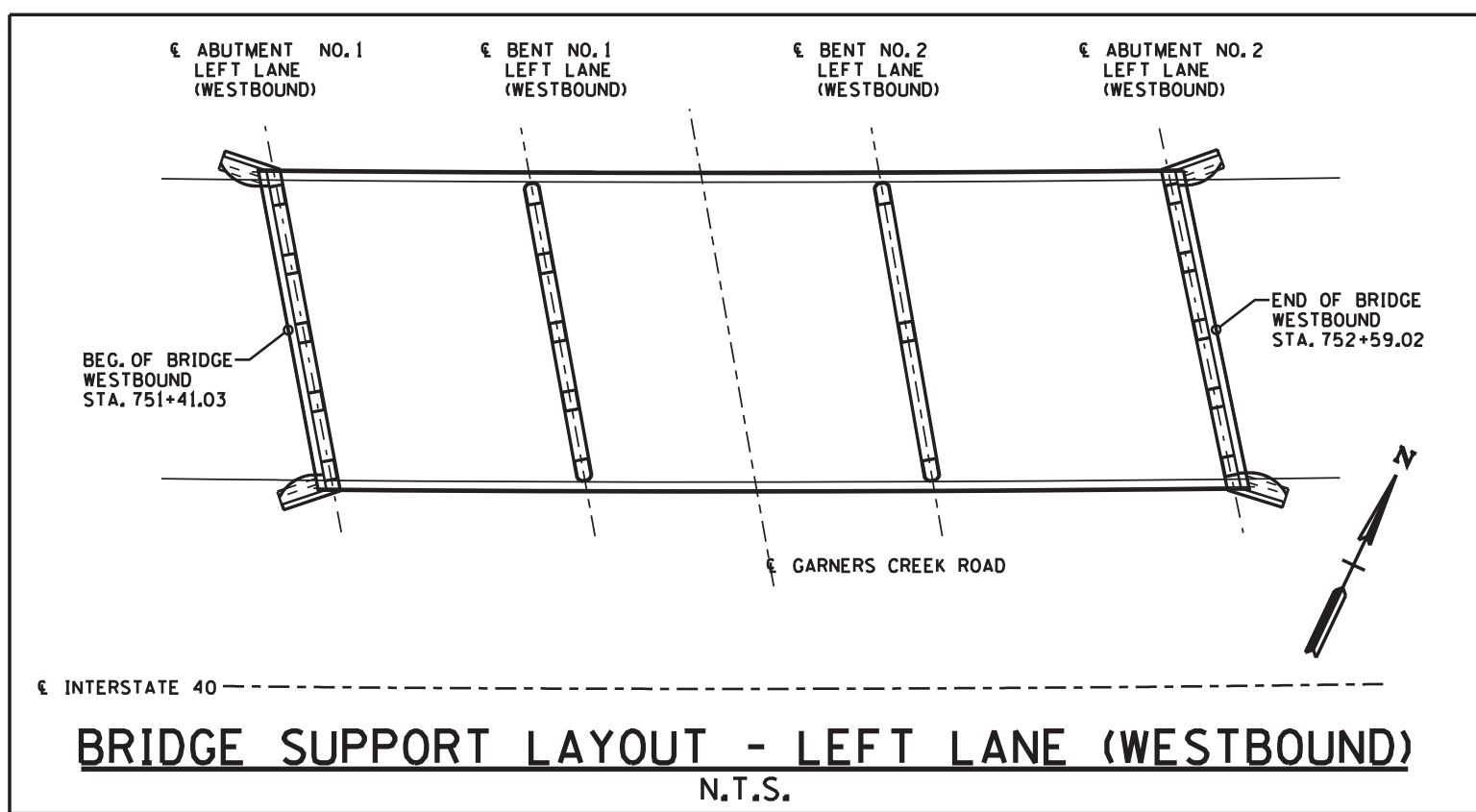


ABUTMENT NO. 2 LT
(LOOKING FORWARD ON SURVEY)

LEGEND	
SPALLED SURFACES	
DELAMINATED SURFACES	
GROUND LEVEL	

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BRIDGE SUPPORT LAYOUT - LEFT LANE (WESTBOUND)
N.T.S.



04/06/2012
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

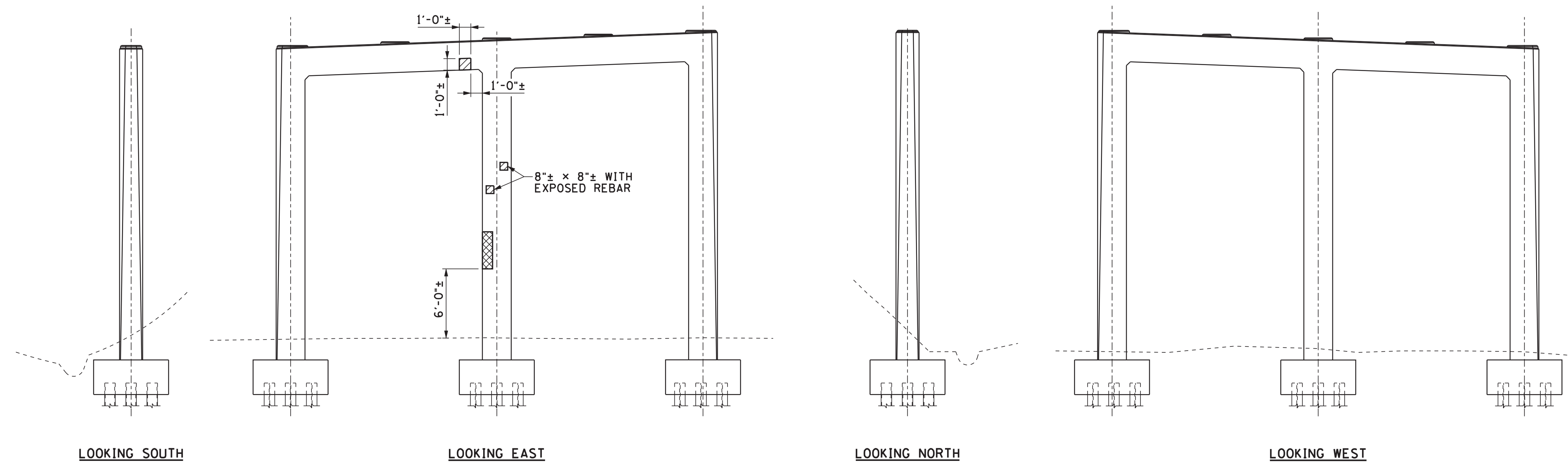
CONCRETE REPAIRS
ABUTMENT NOS. 1 AND 2 LT

BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
INTERSTATE 40 OVER GARNERS CREEK ROAD
HICKMAN COUNTY
2012

DESIGNED BY: NICHOLAS POWELL
DRAWN BY: ANGELA MOORE
SUPERVISED BY: LANDEL BILBREY
CHECKED BY: JAMIE GILLESPIE

DATE: _____
DATE: _____
DATE: _____
DATE: _____

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
41002-4113-04	2012		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

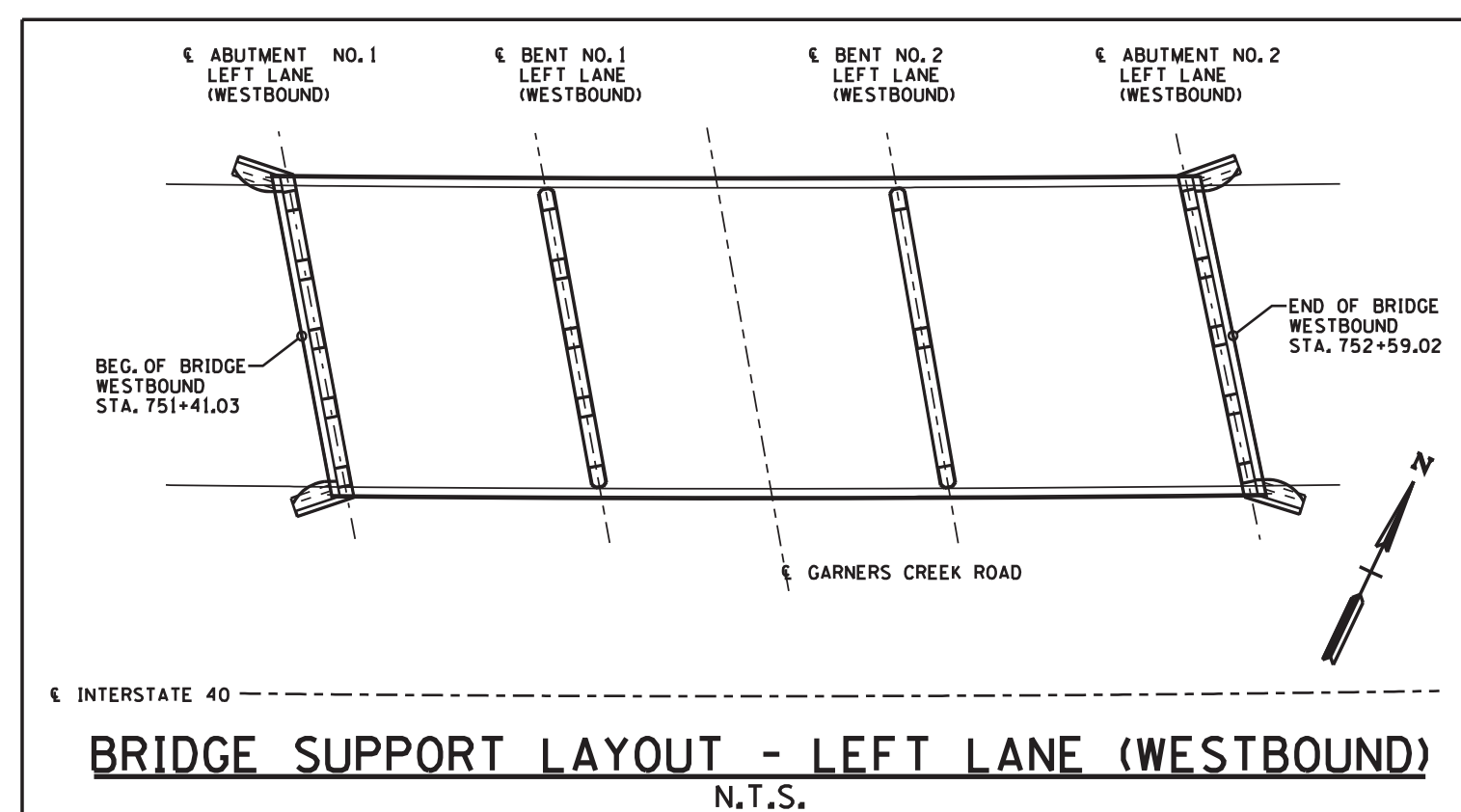


BENT NO. 1 LT

LEGEND	
SPALLED SURFACES	
DELAMINATED SURFACES	
GROUND LEVEL	

NOTE: INFORMATION AND DIMENSIONS FOR CONCRETE REPAIRS ARE PROVIDED FOR ESTIMATING ONLY. TDOT ENGINEER SHALL DELINEATE ACTUAL AREAS TO BE REPAIRED.

- NOTES:**
- (1) REMOVE ALL DEBRIS AROUND BENTS AND ABUTMENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
 - (2) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETERIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.



BRIDGE SUPPORT LAYOUT - LEFT LANE (WESTBOUND)
N.T.S.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

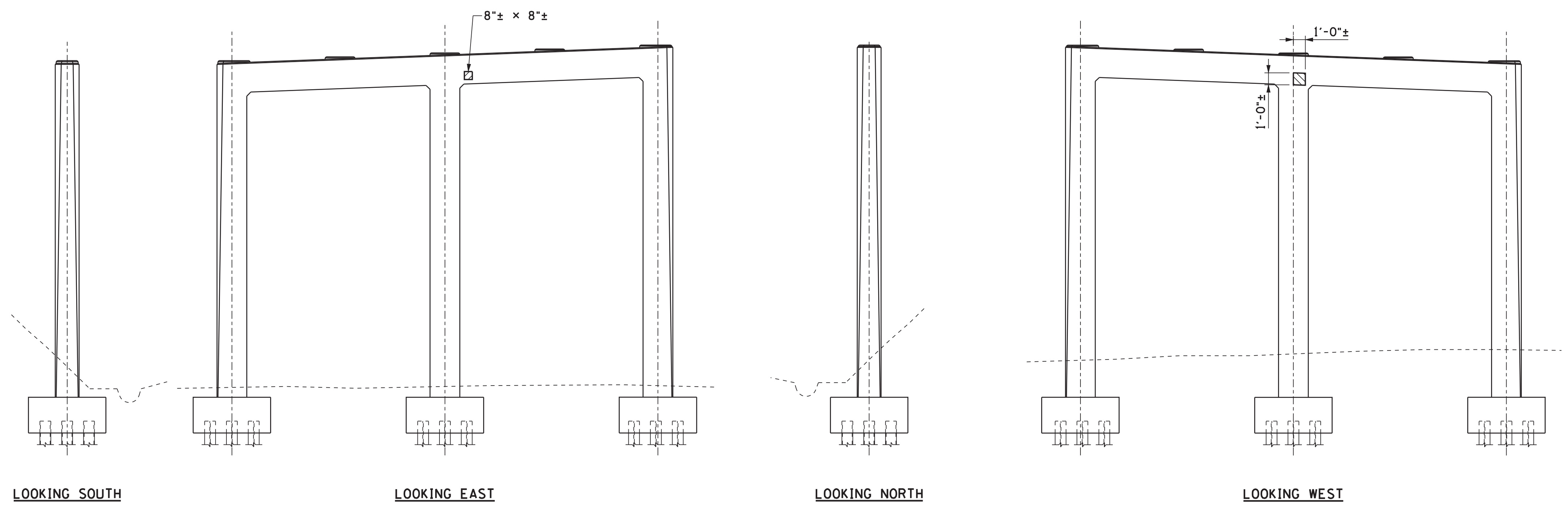
CONCRETE REPAIRS
BENT NO. 1 LT

BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
INTERSTATE 40 OVER GARNERS CREEK ROAD
HICKMAN COUNTY
2012

DESIGNED BY: NICHOLAS POWELL
DRAWN BY: ANGELA MOORE
SUPERVISED BY: LANDEL BILBREY
CHECKED BY: JAMIE GILLESPIE



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
41002-4113-04	2012		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

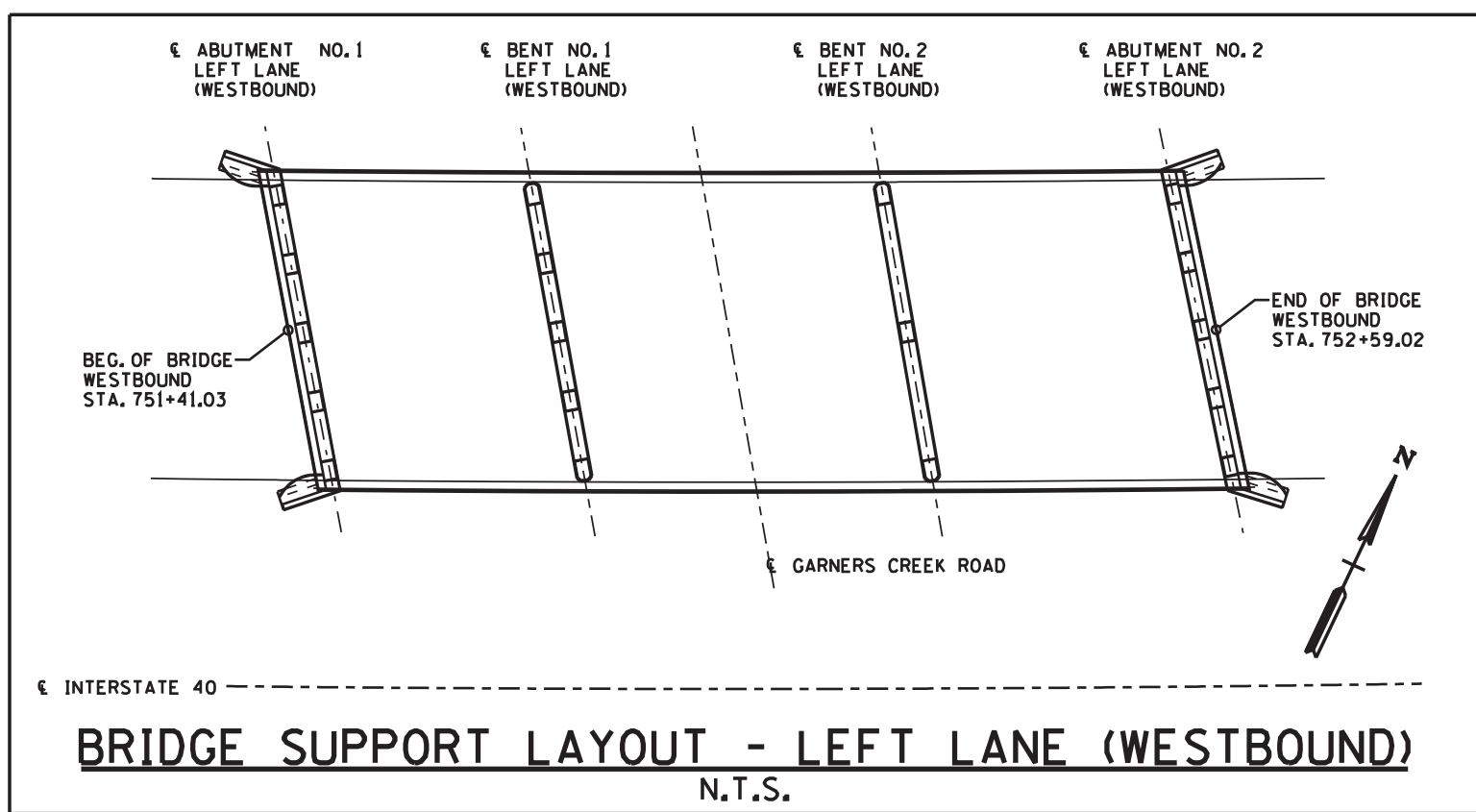


BENT NO. 2 LT

LEGEND	
SPALLED SURFACES	
DELAMINATED SURFACES	
GROUND LEVEL	

NOTE: INFORMATION AND DIMENSIONS FOR CONCRETE REPAIRS ARE PROVIDED FOR ESTIMATING ONLY. TDDT ENGINEER SHALL DELINEATE ACTUAL AREAS TO BE REPAIRED.

- NOTES:**
- (1) REMOVE ALL DEBRIS AROUND BENTS AND ABUTMENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
 - (2) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETERIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.



BRIDGE SUPPORT LAYOUT - LEFT LANE (WESTBOUND)
N.T.S.

DESIGNED BY: NICHOLAS POWELL
 DRAWN BY: ANGELA MOORE
 SUPERVISED BY: LANDEL BILBREY
 CHECKED BY: JAMIE GILLESPIE

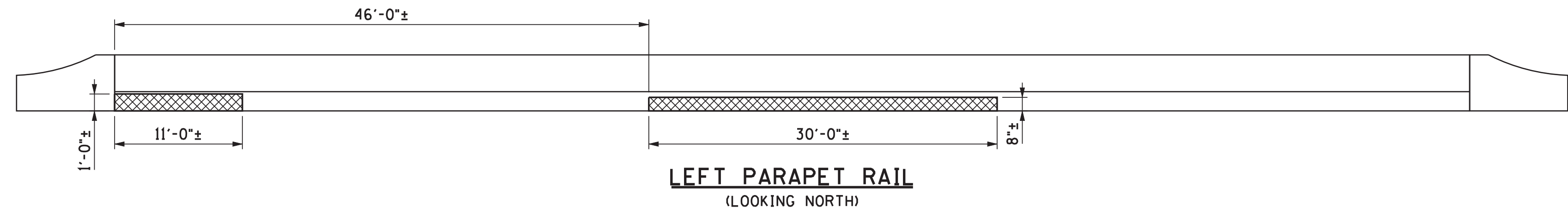
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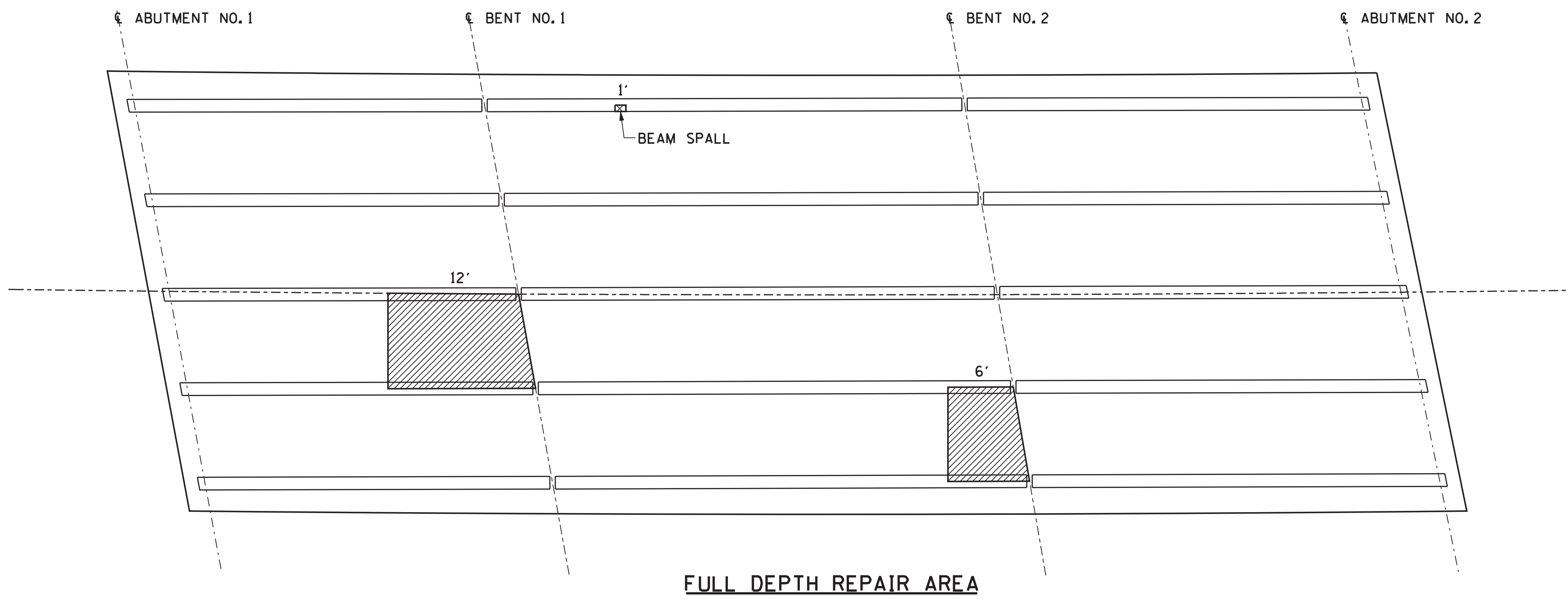
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIRS
 BENT NO. 2 LT

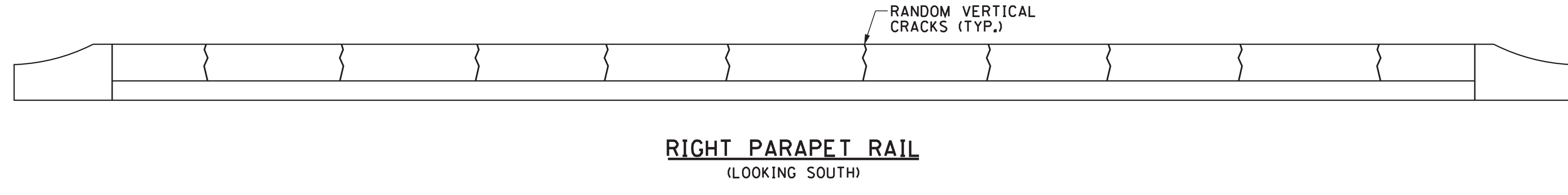
BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
 INTERSTATE 40 OVER GARNERS CREEK ROAD
 HICKMAN COUNTY
 2012



LEFT PARAPET RAIL
(LOOKING NORTH)

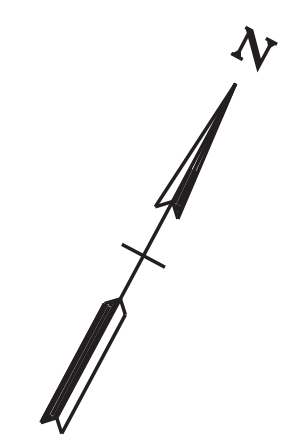


FULL DEPTH REPAIR AREA



RIGHT PARAPET RAIL
(LOOKING SOUTH)

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
41002-4113-04	2012		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



- NOTES:**
- (1) REMOVE ALL DEBRIS AROUND BENTS AND ABUTMENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
 - (2) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETEIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.
 - (3) PARAPET RAIL DISTRESSES REFLECT A VISUAL INSPECTION ONLY. ANY ADDITIONAL DISTRESSES DISCOVERED BY THE TDOT INSPECTOR SHALL BE REPAIRED AT THE UNIT PRICE BID.

- DENOTES: ESTIMATED FULL DEPTH REPAIR AREAS
- DENOTES: SPALLED SURFACES
- DENOTES: DELAMINATED SURFACES

ESTIMATED QUANTITIES	
ITEM NO. 604-10.30 FULL DEPTH S.Y.	ITEM NO. 604-10.50 PARTIAL DEPTH S.Y.
20	75

NOTE: INFORMATION AND DIMENSIONS FOR CONCRETE REPAIRS ARE PROVIDED FOR ESTIMATING ONLY. TDOT ENGINEER SHALL DELINEATE ACTUAL AREAS TO BE REPAIRED.

NOTE: SEE CONCRETE REPAIR DETAILS SHEET FOR REPAIR QUANTITIES FOR BEAMS AND DIAPHRAGMS. ALL DECK REPAIR QUANTITIES SHALL BE DESIGNATED BY THE BRIDGE REPAIR OFFICE AS STATED IN NOTE NO. 1 ON THE DECK REPAIR DETAILS SHEET AND SHALL BE INCLUDED IN ITEM NO. 604-10.30 OR 604-10.50.



04/06/2012
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

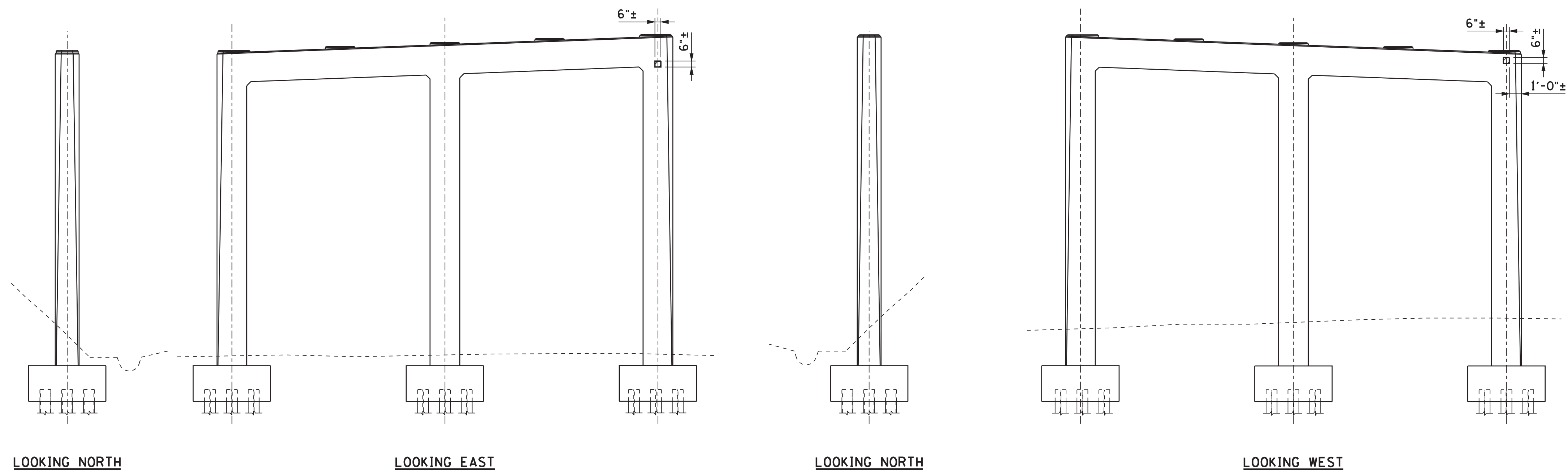
CONCRETE REPAIRS
SLAB & RAIL (LT LANE BRIDGE)

BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
INTERSTATE 40 OVER GARNERS CREEK ROAD
HICKMAN COUNTY
2012

DESIGNED BY: NICHOLAS POWELL
DRAWN BY: ANGELA MOORE
SUPERVISED BY: LANDEL BILBREY
CHECKED BY: JAMIE GILLESPIE

DATE: _____
DATE: _____
DATE: _____
DATE: _____

CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
41002-4113-04	2012		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



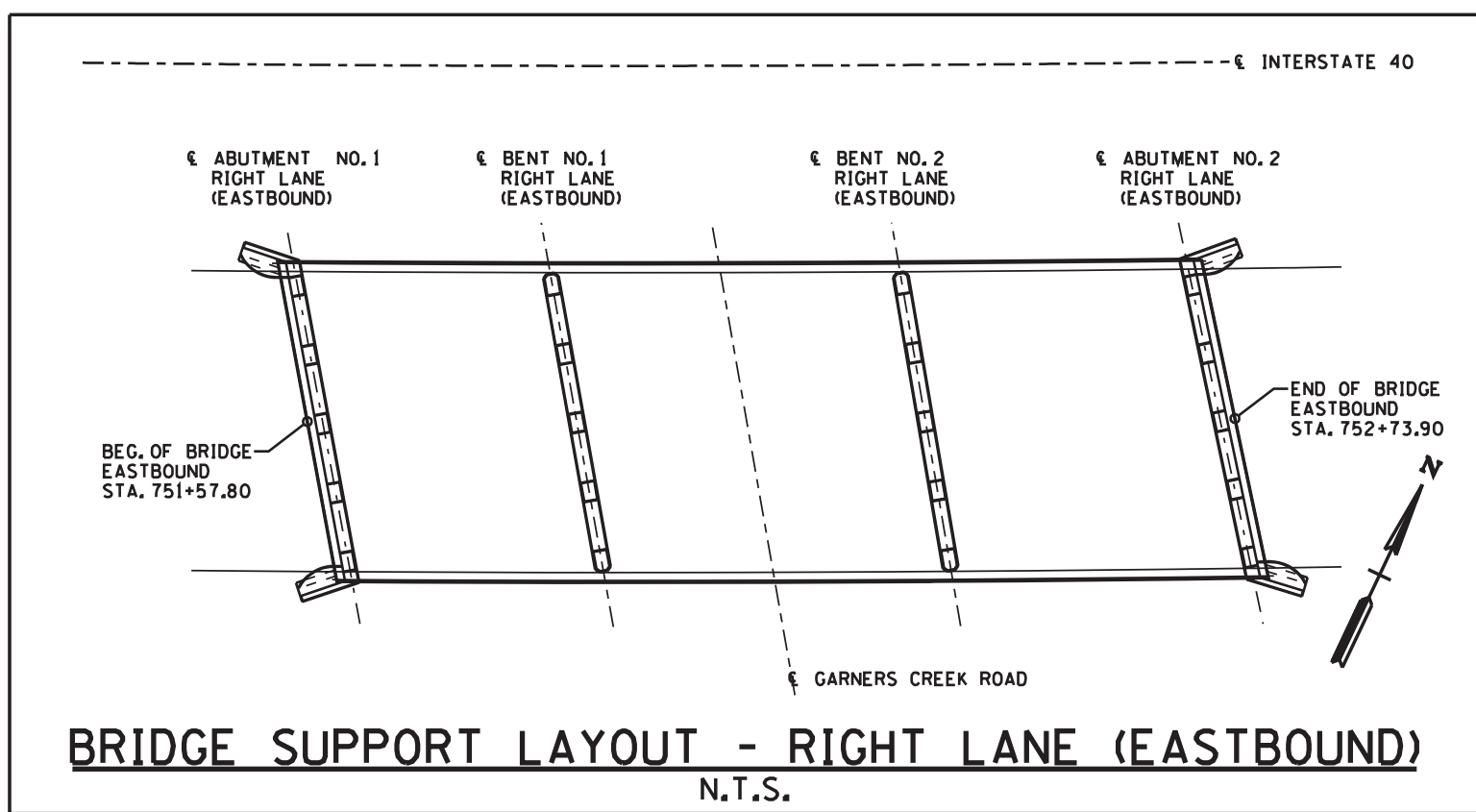
BENT NO. 2 RT

NOTE: BENT NO. 1 RT NOT SHOWN DUE TO THE ABSENCE OF ANY REPAIR AREAS.

LEGEND	
SPALLED SURFACES	
DELAMINATED SURFACES	
GROUND LEVEL	

- NOTES:**
- (1) REMOVE ALL DEBRIS AROUND BENTS AND ABUTMENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
 - (2) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETERIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.

NOTE: INFORMATION AND DIMENSIONS FOR CONCRETE REPAIRS ARE PROVIDED FOR ESTIMATING ONLY. TDOT ENGINEER SHALL DELINEATE ACTUAL AREAS TO BE REPAIRED.



04/06/2012
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

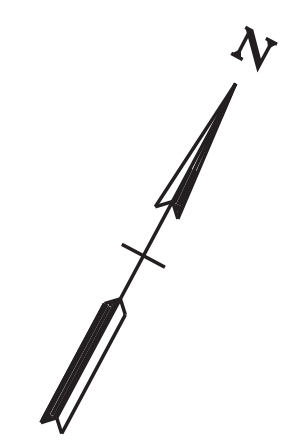
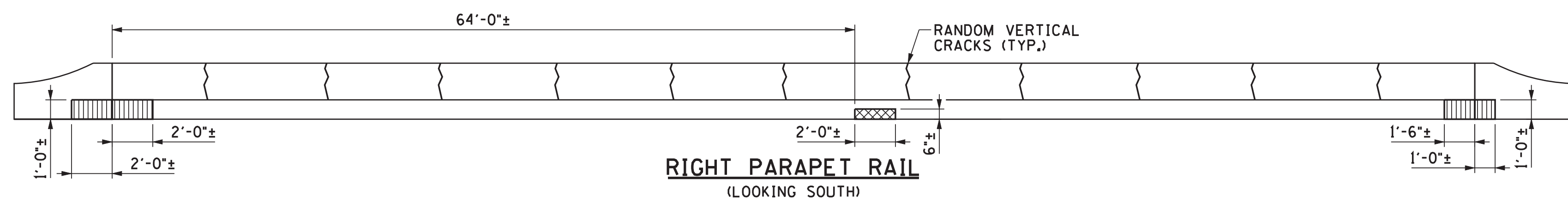
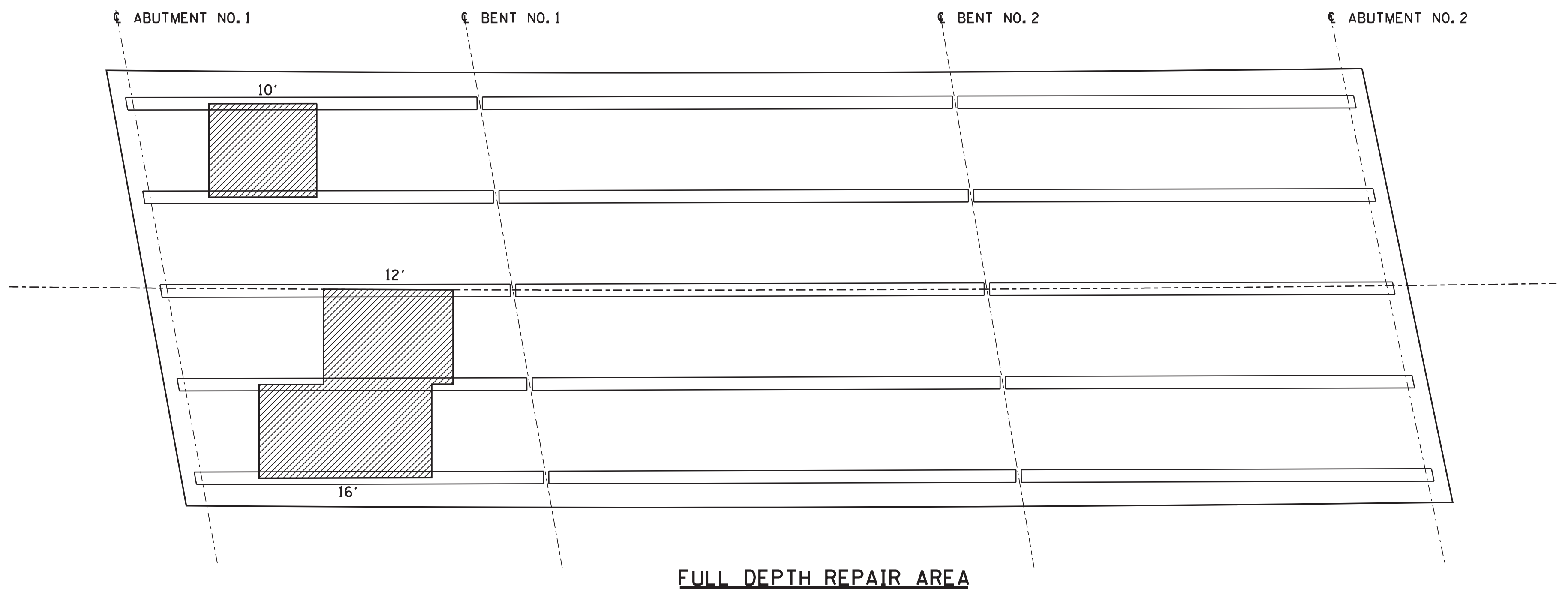
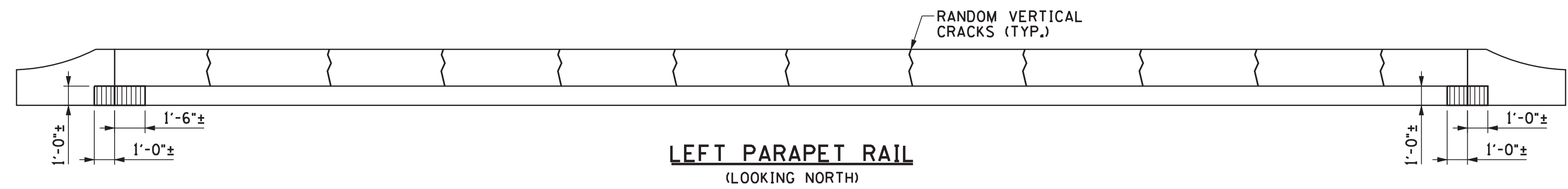
**CONCRETE REPAIRS
BENT NO. 2 RT**

BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
INTERSTATE 40 OVER GARNERS CREEK ROAD
HICKMAN COUNTY
2012

DESIGNED BY NICHOLAS POWELL DATE _____
 DRAWN BY ANGELA MOORE DATE _____
 SUPERVISED BY LANDEL BILBREY DATE _____
 CHECKED BY JAMIE GILLESPIE DATE _____



CONST. NO.			
PROJECT NO.	YEAR	SHEET NO.	
41002-4113-04	2012		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



- NOTES:**
- (1) REMOVE ALL DEBRIS AROUND BENTS AND ABUTMENTS. COST IS TO BE INCLUDED IN OTHER ITEMS.
 - (2) ALL AREAS THAT CONTAIN CALCIUM DEPOSITS SHALL BE REMOVED. ANY CONCRETE THAT HAS DETEIORATED BEHIND THE DEPOSITS SHALL BE REPAIRED.
 - (3) PARAPET RAIL DISTRESSES REFLECT A VISUAL INSPECTION ONLY. ANY ADDITIONAL DISTRESSES DISCOVERED BY THE TDOT INSPECTOR SHALL BE REPAIRED AT THE UNIT PRICE BID.

- DENOTES: ESTIMATED FULL DEPTH REPAIR AREAS
- DENOTES: SPALLED SURFACES
- DENOTES: DELAMINATED SURFACES

NOTE: INFORMATION AND DIMENSIONS FOR CONCRETE REPAIRS ARE PROVIDED FOR ESTIMATING ONLY. TDOT ENGINEER SHALL DELINEATE ACTUAL AREAS TO BE REPAIRED.

ESTIMATED QUANTITIES	
ITEM NO. 604-10.30 FULL DEPTH S.Y.	ITEM NO. 604-10.50 PARTIAL DEPTH S.Y.
40	100

NOTE: SEE CONCRETE REPAIR DETAILS SHEET FOR REPAIR QUANTITIES FOR BEAMS AND DIAPHRAGMS. ALL DECK REPAIR QUANTITIES SHALL BE DESIGNATED BY THE BRIDGE REPAIR OFFICE AS STATED IN NOTE NO.1 ON THE DECK REPAIR DETAILS SHEET AND SHALL BE INCLUDED IN ITEM NO. 604-10.30 OR 604-10.50.

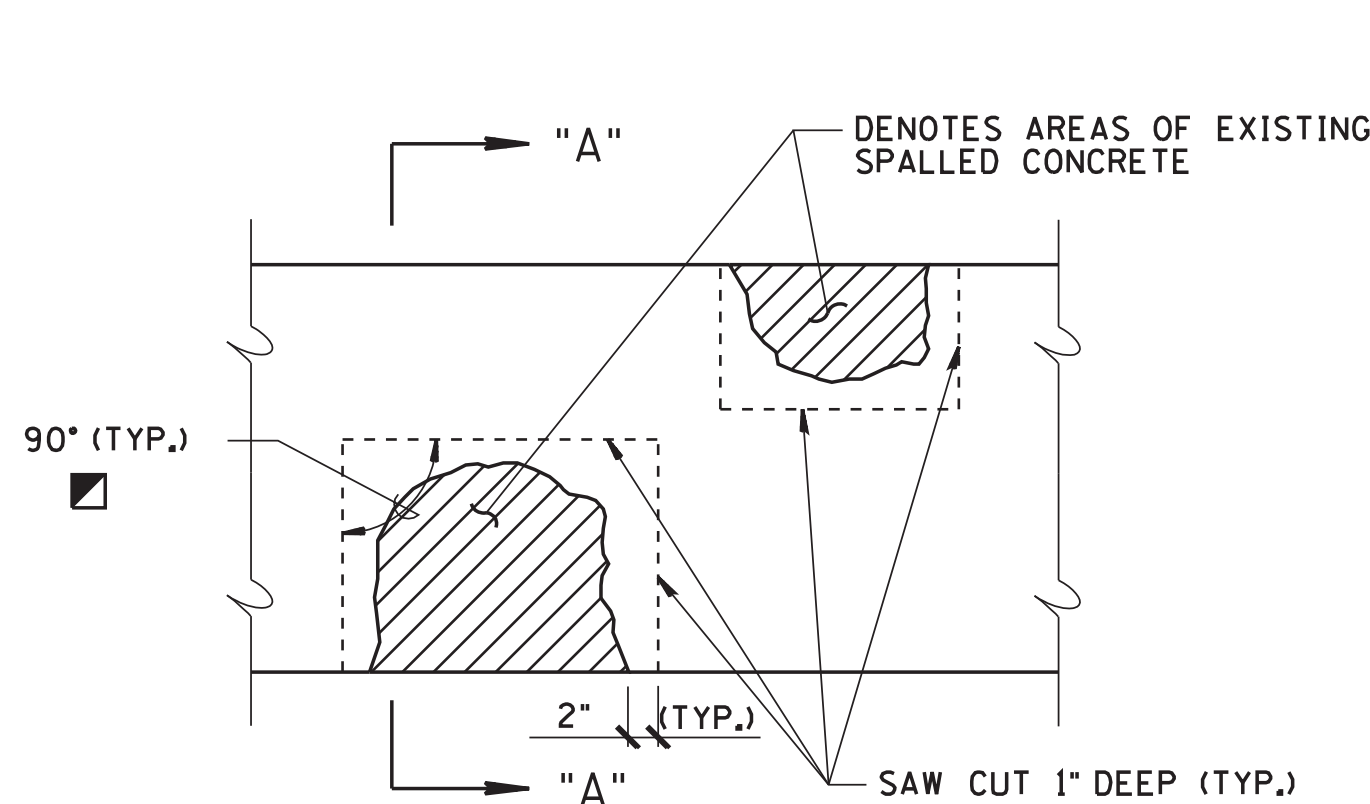
DESIGNED BY: NICHOLAS POWELL
 DRAWN BY: ANGELA MOORE
 SUPERVISED BY: LANDEL BILBREY
 CHECKED BY: JAMIE GILLESPIE

DATE: _____
 DATE: _____
 DATE: _____
 DATE: _____



04/06/2012
 STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIRS
 SLAB & RAIL (RT LANE BRIDGE)
 BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
 INTERSTATE 40 OVER GARNERS CREEK ROAD
 HICKMAN COUNTY
 2012

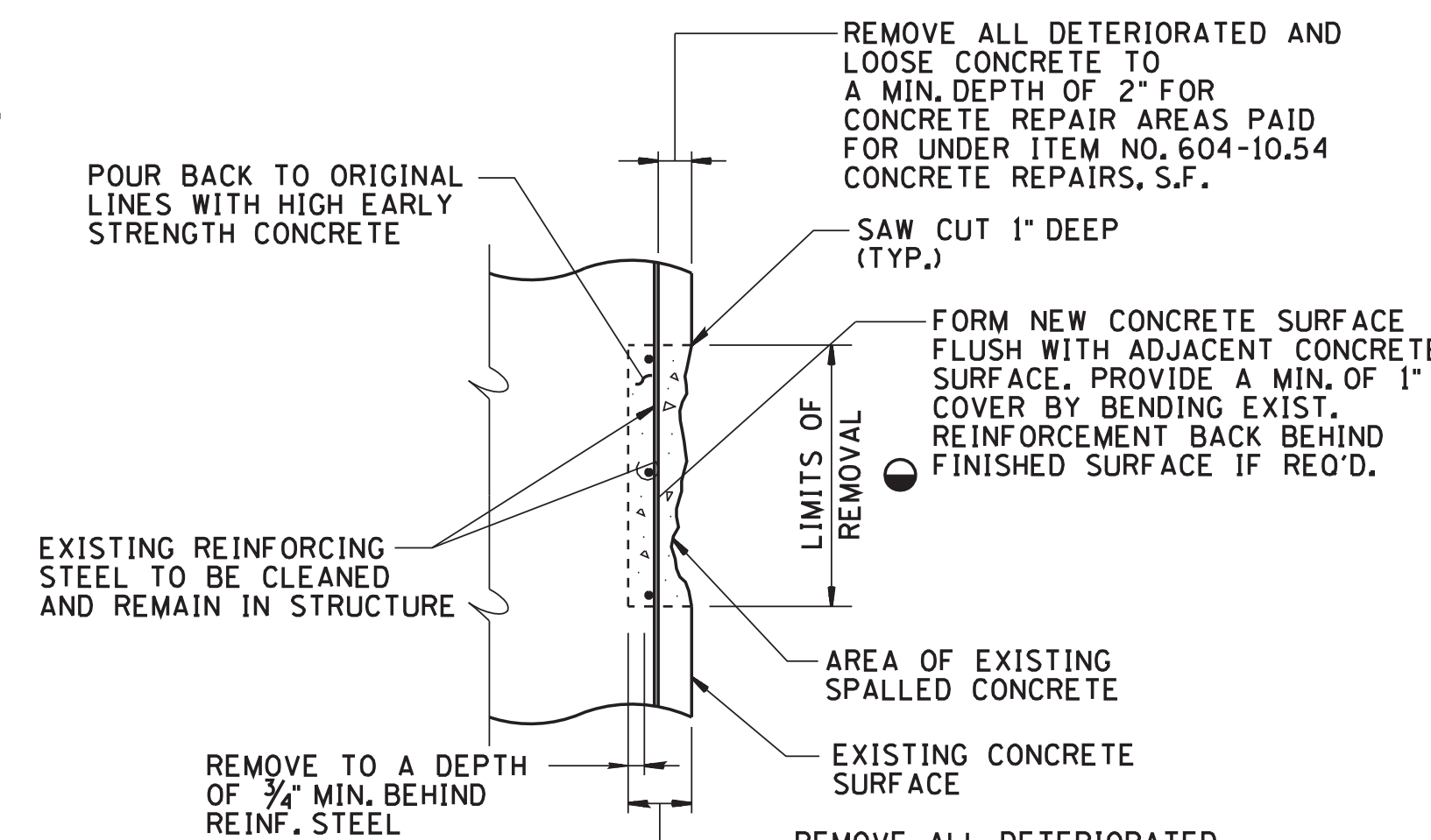


DETAIL SHOWING AREAS OF EXISTING SPALLED CONCRETE SURFACES TO BE REMOVED AND REPAIRED

- DENOTES: LIMITS AND LOCATION OF REPAIRS DESIGNATED ON THE CONCRETE REPAIR SHEETS.
- ◻ DENOTES: SAW CUT EXISTING CONCRETE SURFACES SO AS TO OBTAIN SQUARED CORNERS.

NOTE: THE COST OF REMOVING PORTIONS OF THE EXISTING CONCRETE WITHIN THE LIMITS SHOWN, SAW CUTTING, COMPLETELY CLEANING EXISTING REINFORCING STEEL, HIGH EARLY STRENGTH CONCRETE, FORMING, LABOR AND ALL MISCELLANEOUS MATERIAL NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 604-10.54, CONCRETE REPAIRS, S.F.

NOTE: ITEM NO. 604-10.54, CONCRETE REPAIRS, S.F. SHALL BE BID WITH THE CONTINGENCY THAT IT MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.



SECTION "A-A"

BRIDGE NO. 41-140-1.53LT	
ESTIMATED QUANTITIES	
ABUTMENT NO. 1	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
8	11
BENT NO. 1	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	0
BENT NO. 2	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
1	3
ABUTMENT NO. 2	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
2	17
PARAPET RAILS	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
3	60
TOTAL FOR ITEM NO. 604-10.05 CONCRETE S.F.	TOTAL FOR ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
14	91

BRIDGE NO. 41-140-1.53RT	
ESTIMATED QUANTITIES	
ABUTMENT NO. 1	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
1	3
BENT NO. 1	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
5	5
BENT NO. 2	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
1	7
ABUTMENT NO. 2	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
5	30
PARAPET RAILS	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
8	4
TOTAL FOR ITEM NO. 604-10.05 CONCRETE S.F.	TOTAL FOR ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
20	49

CONST. NO. _____		
PROJECT NO. 41002-4113-04	YEAR 2012	SHEET NO.
REVISIONS		
NO.	DATE	BY
		BRIEF DESCRIPTION

1. ITEM NO. 604-10.05, CONCRETE, S.F. INCLUDES: THE COST OF FORMING, LABOR, CONCRETE, AND ALL MISCELLANEOUS MATERIAL (EXCEPT REINFORCING STEEL) NECESSARY TO REPAIR THE DEFICIENT AREAS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING SLAB REPLACEMENT. DESIGN CALCULATION AND DETAILS OF TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY DEMOLITION IS BEGUN. COST OF STABILIZING THE STRUCTURE SHALL BE INCLUDED IN ITEM NO. 602-10.05, BRACING REPAIRS, L.S..
3. EXTREME CARE SHALL BE TAKEN WHEN REMOVING DETERIORATED CONCRETE FROM THE PIER CAP SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED REINFORCING STEEL SHALL BE COMPLETELY CLEANED TO THE SATISFACTION OF THE ENGINEER BEFORE REPOURING.
4. COST OF REINFORCING STEEL REQUIRED TO REPLACE DETERIORATED OR DEFICIENT SECTIONS SHALL BE INCLUDED IN ITEM NO. 604-03.02, STEEL BAR REINFORCEMENT (BRIDGES). LOCATIONS OR SECTIONS OF STEEL TO BE REPLACED ARE TO BE DETERMINED BY THE ENGINEER DURING DEMOLITION OF THE ABUTMENTS AND BENT CAPS.
5. THE ENGINEER SHALL HAVE THE OPTION OF DESIGNATING A SPALL AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 OR ITEM NO. 604-10.54. PATCHING MATERIAL FOR ITEM NO. 604-10.05 SHALL BE HIGH EARLY STRENGTH CONCRETE. PATCHING MATERIAL FOR ITEM NO. 604-10.54 SHALL BE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL. SEE QUALIFIED PRODUCTS LIST 13, SECTION B.6.
6. EXTREME CARE SHALL BE TAKEN WHEN REMOVING THE EXISTING SPALLED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED REINFORCING STEEL SHALL RECEIVE A COMPLETE CLEANING TO REMOVE ALL RUST. ALL EXISTING REINFORCEMENT SHALL REMAIN IN PLACE. ALL WORK MUST MEET WITH THE FULL APPROVAL OF THE ENGINEER.
7. THE ENGINEER SHALL DESIGNATE ALL SPALLED CONCRETE REPAIR AREAS IN THE FIELD. QUANTITIES GIVEN ARE APPROXIMATE. ITEM NO. 604-10.05 AND ITEM NO. 604-10.54 SHALL BE BID WITH THE CONTINGENCY THAT IT MAY BE INCREASED, DECREASED, OR ELIMINATED BY THE ENGINEER.
8. POWER DRIVEN HAND TOOLS USED FOR REMOVAL OF UNSOUND CONCRETE ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:
 - A. PNEUMATIC HAMMERS HEAVIER THAN 35 LB. CLASS SHALL NOT BE USED.
 - B. CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BEHIND REINFORCING STEEL AND BEAM END REPAIRS.
9. COST OF SAW CUTTING, REMOVING SPALL CONCRETE, CLEANING, PATCHING MATERIAL, LABOR AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN TO BE INCLUDED IN ITEM NO. 604-10.05, CONCRETE, S.F. OR ITEM NO. 604-10.54, CONCRETE REPAIRS, S.F.
10. LIMITS AND LOCATION OF REPAIRS TO BE DESIGNATED BY THE ENGINEER. TEMPORARY BRACING MAY BE REQUIRED AND INCLUDE IN ITEM NO. 602-10.05.
11. SEE BRIDGE GENERAL NOTES FOR INFORMATION ON HIGH EARLY STRENGTH CONCRETE.

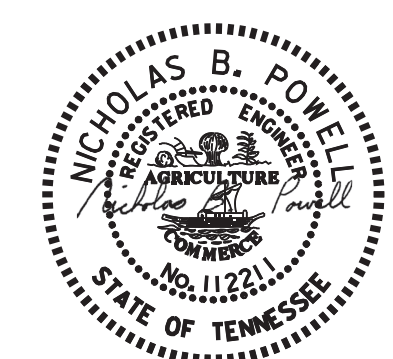
BRIDGE NO. 41-140-1.63LT	
ESTIMATED QUANTITIES	
ABUTMENT NO. 1	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
5	31
BENT NO. 1	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
4	1
BENT NO. 2	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	2
ABUTMENT NO. 2	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
3	14
PARAPET RAILS	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
33	2
TOTAL FOR ITEM NO. 604-10.05 CONCRETE S.F.	TOTAL FOR ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
45	50

BRIDGE NO. 41-140-1.63RT	
ESTIMATED QUANTITIES	
ABUTMENT NO. 1	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
6	18
BENT NO. 1	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	0
BENT NO. 2	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
0	2
ABUTMENT NO. 2	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
2	52
PARAPET RAILS	
ITEM NO. 604-10.05 CONCRETE S.F.	ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
2	13
TOTAL FOR ITEM NO. 604-10.05 CONCRETE S.F.	TOTAL FOR ITEM NO. 604-10.54 CONCRETE REPAIRS S.F.
10	85

DESIGNED BY: NICHOLAS POWELL
 DRAWN BY: ANGELA MOORE
 SUPERVISED BY: LANDEL BILBREY
 CHECKED BY: JAMIE GILLESPIE

DATE _____
 DATE _____
 DATE _____
 DATE _____

JA
 JAMIE GILLESPIE, INC.



04/08/2012
 STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 CONCRETE REPAIR DETAILS

BRIDGE NOS. 41-140-1.53LT AND 41-140-1.53RT
 INTERSTATE 40 OVER GARNERS CREEK
 AND
 BRIDGE NOS. 41-140-1.63LT AND 41-140-1.63RT
 INTERSTATE 40 OVER GARNERS CREEK ROAD
 HICKMAN COUNTY
 2012

BRIDGE NOS. 41-I40-1.53LT & RT

ABUTMENT NOS. 1 & 2 LT - EPOXY

BAR	LOCATION	SIZE	NO. REQ'D	BENDING DIMENSIONS				LENGTH
				A	B	C	D	
A500E	FOUNDATION REPAIR	5	4	45'-10"				45'-10"
A501E	FOUNDATION REPAIR	5	4	45'-6"				45'-6"
R400E	FOUNDATION REPAIR	4	100	1'-1"	2'-7"			3'-8"
V500E	FOUNDATION REPAIR	5	4	2'-6"	6'-4"	135		9'-0"

ABUTMENT NOS. 1 & 2 RT - EPOXY

BAR	LOCATION	SIZE	NO. REQ'D	BENDING DIMENSIONS				LENGTH
				A	B	C	D	
A500E	FOUNDATION REPAIR	5	4	45'-10"				45'-10"
A501E	FOUNDATION REPAIR	5	4	45'-6"				45'-6"
R400E	FOUNDATION REPAIR	4	94	1'-1"	2'-7"			3'-8"

SUPERSTRUCTURE LT - EPOXY

BAR	LOCATION	SIZE	NO. REQ'D	BENDING DIMENSIONS				LENGTH
				A	B	C	D	
V610E	RDWY BRACKET & P.A.B.E.	6	76	1'-4"	2'-0"	120		3'-4"

SUPERSTRUCTURE RT - EPOXY

BAR	LOCATION	SIZE	NO. REQ'D	BENDING DIMENSIONS				LENGTH
				A	B	C	D	
V610E	RDWY BRACKET & P.A.B.E.	6	76	1'-4"	2'-0"	120		3'-4"

BRIDGE NOS. 41-I40-1.63LT & RT

SUPERSTRUCTURE LT - EPOXY

BAR	LOCATION	SIZE	NO. REQ'D	BENDING DIMENSIONS				LENGTH
				A	B	C	D	
A610E	DECK AND P.A.B.E.	6	37	7'-4"				7'-4"
B471E	DECK & PARAPET RAIL	4	16	2'-2"				2'-8"
V610E	RDWY BRACKET & P.A.B.E.	6	74	1'-4"	2'-0"	120		3'-4"
R610E	DECK AND DIAPHRAGM	6	37	2'-10"	1'-6"			4'-4"

SUPERSTRUCTURE RT - EPOXY

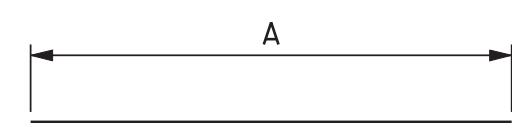
BAR	LOCATION	SIZE	NO. REQ'D	BENDING DIMENSIONS				LENGTH
				A	B	C	D	
A610E	DECK AND P.A.B.E.	6	37	7'-4"				7'-4"
B471E	DECK & PARAPET RAIL	4	16	2'-2"				2'-8"
V610E	RDWY BRACKET & P.A.B.E.	6	74	1'-4"	2'-0"	120		3'-4"
R610E	DECK AND DIAPHRAGM	6	37	2'-10"	1'-6"			4'-4"

CONST. NO.

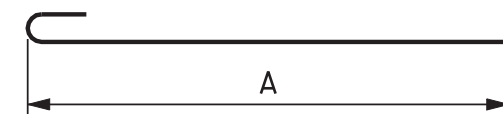
PROJECT NO.	YEAR	SHEET NO.
41002-4113-04	2012	

REVISIONS

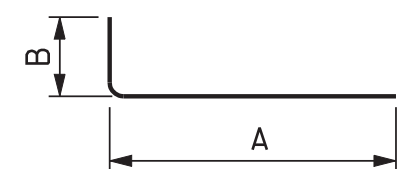
NO.	DATE	BY	BRIEF DESCRIPTION



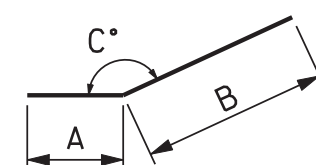
BARS A



BARS B

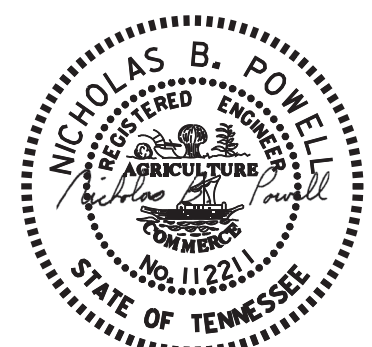


BARS R



BARS V

DESIGNED BY NICHOLAS POWELL DATE _____
 DRAWN BY ANGELA MOORE DATE _____
 SUPERVISED BY LANDEL BILBREY DATE _____
 CHECKED BY JAMIE GILLESPIE DATE _____



04/06/2012

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

BILL OF STEEL

BRIDGE NOS. 41-I40-1.53LT AND 41-I40-1.53RT
 INTERSTATE 40 OVER GARNERS CREEK
 AND
 BRIDGE NOS. 41-I40-1.63LT AND 41-I40-1.63RT
 INTERSTATE 40 OVER GARNERS CREEK ROAD
 HICKMAN COUNTY
 2012

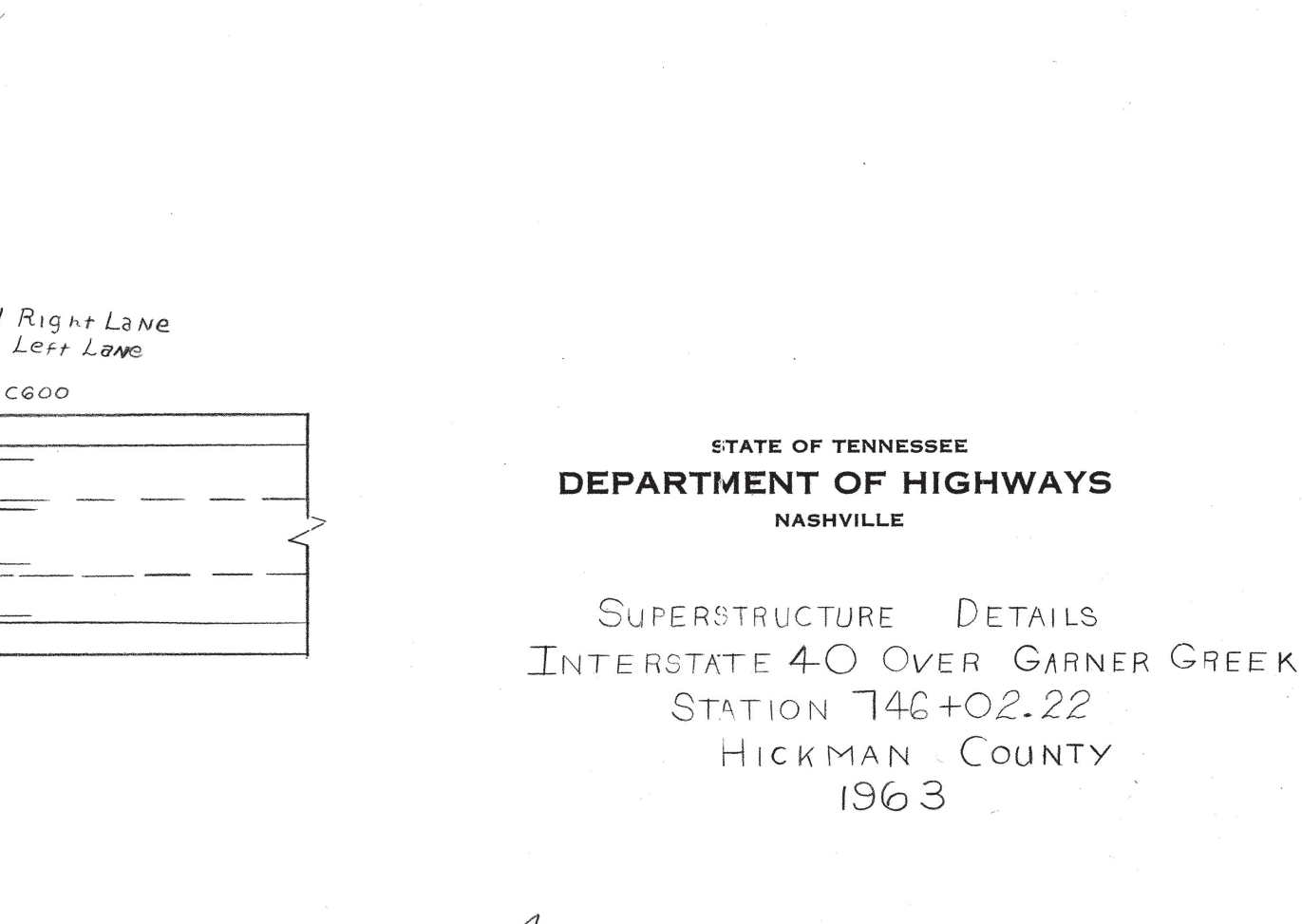
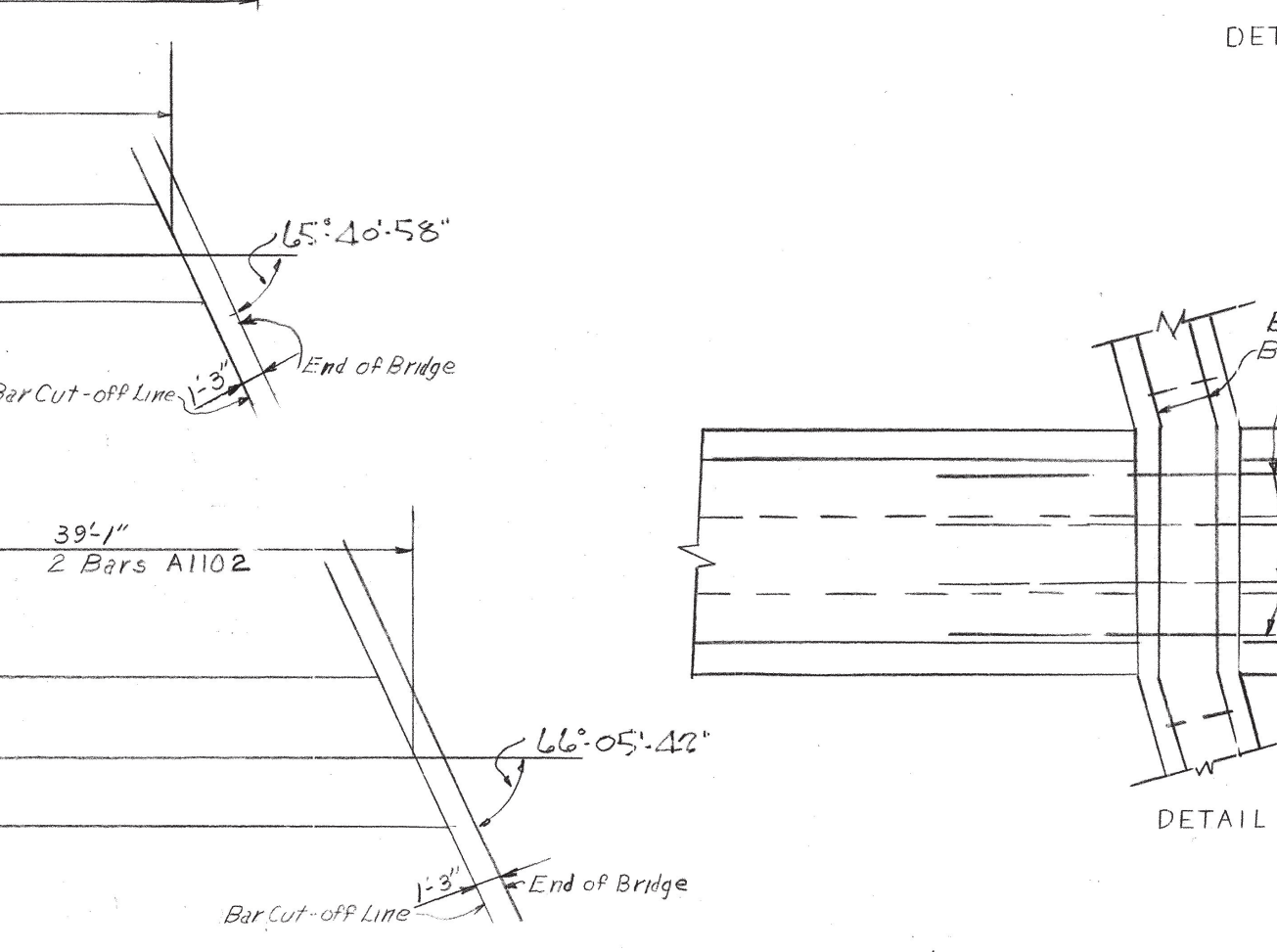
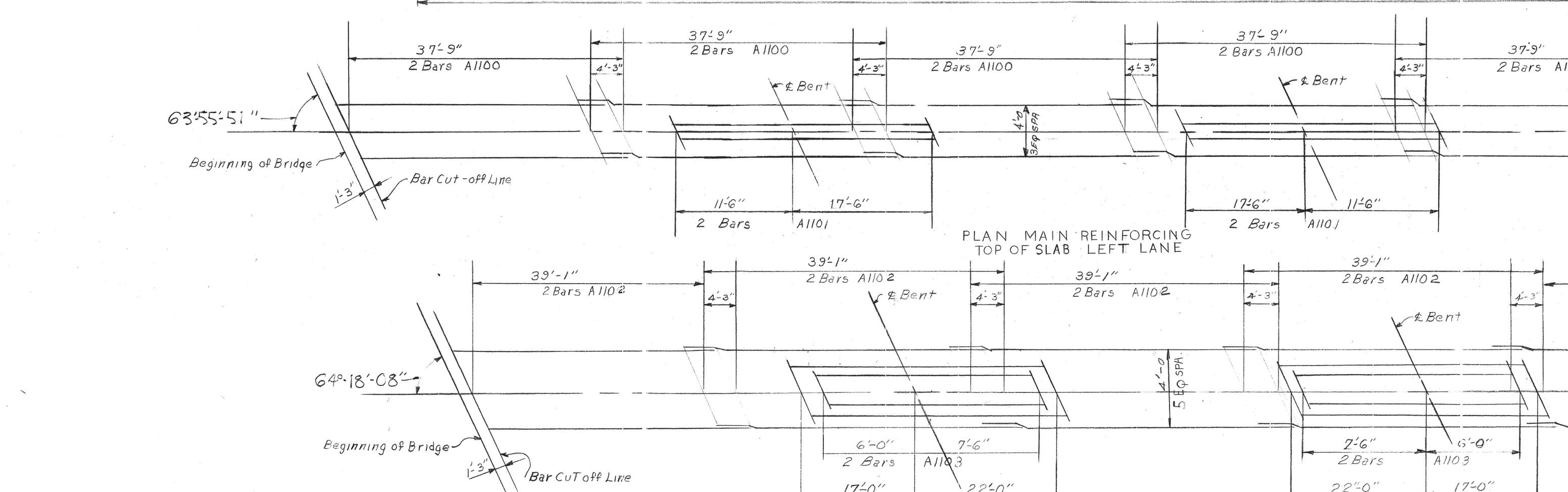
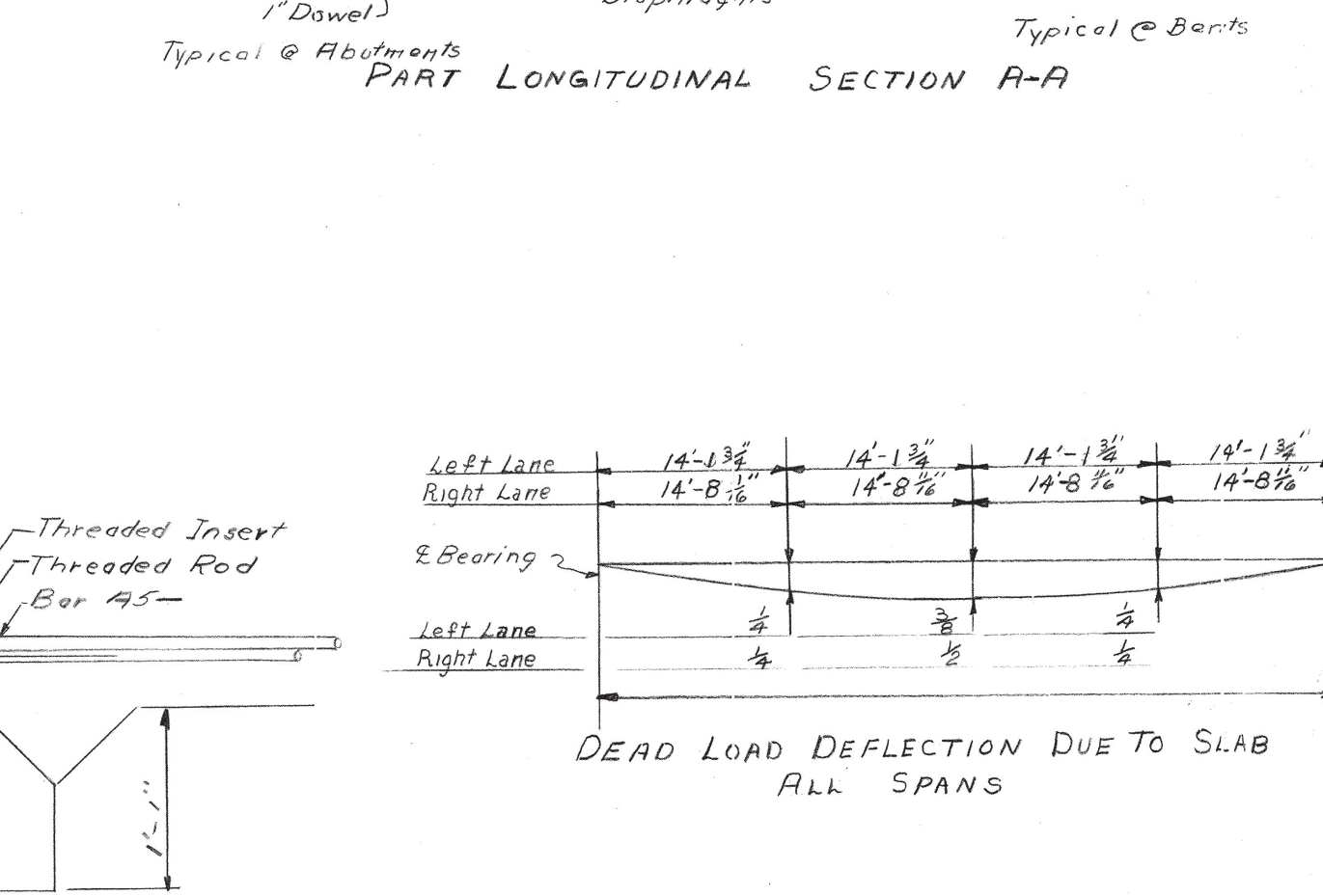
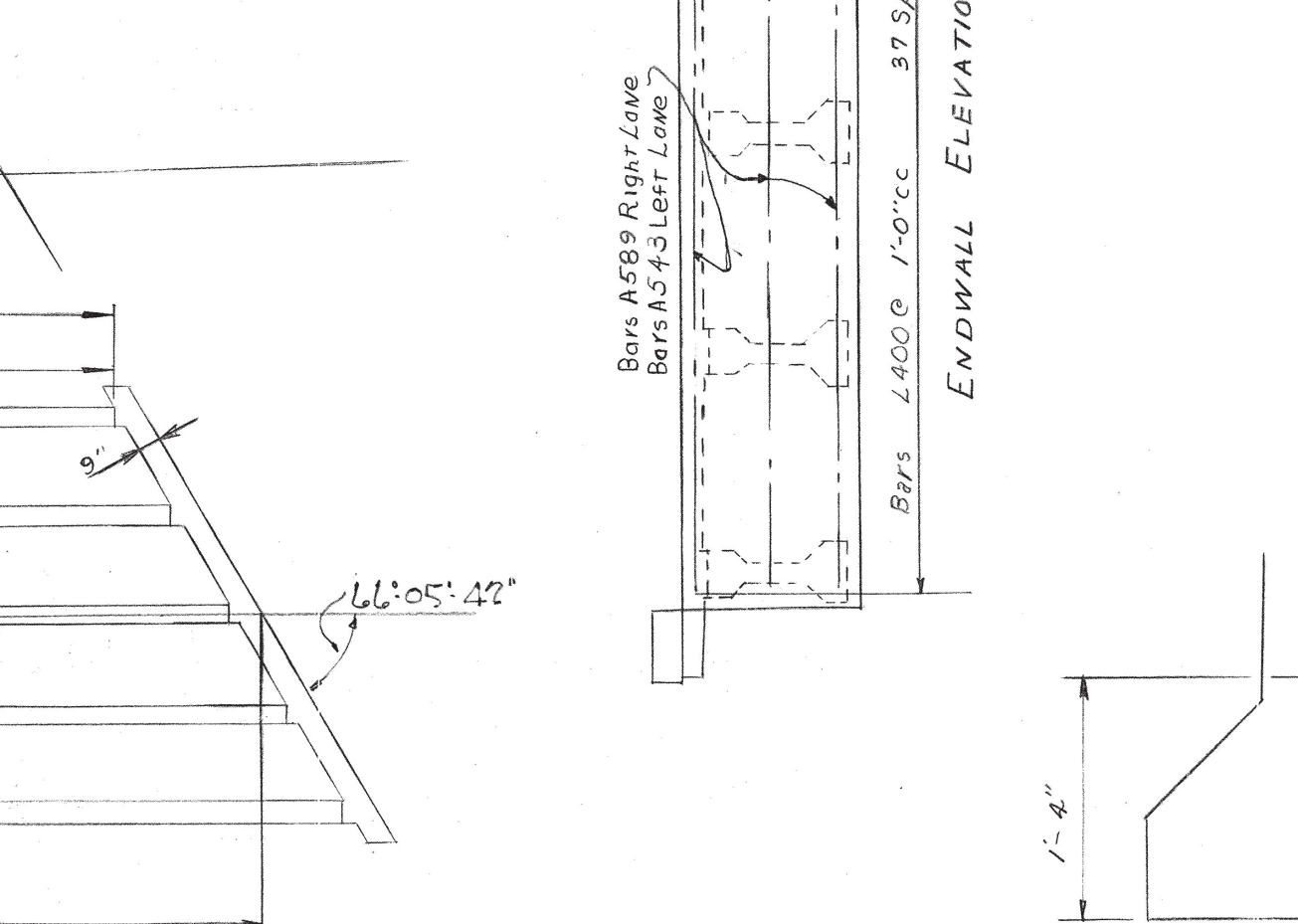
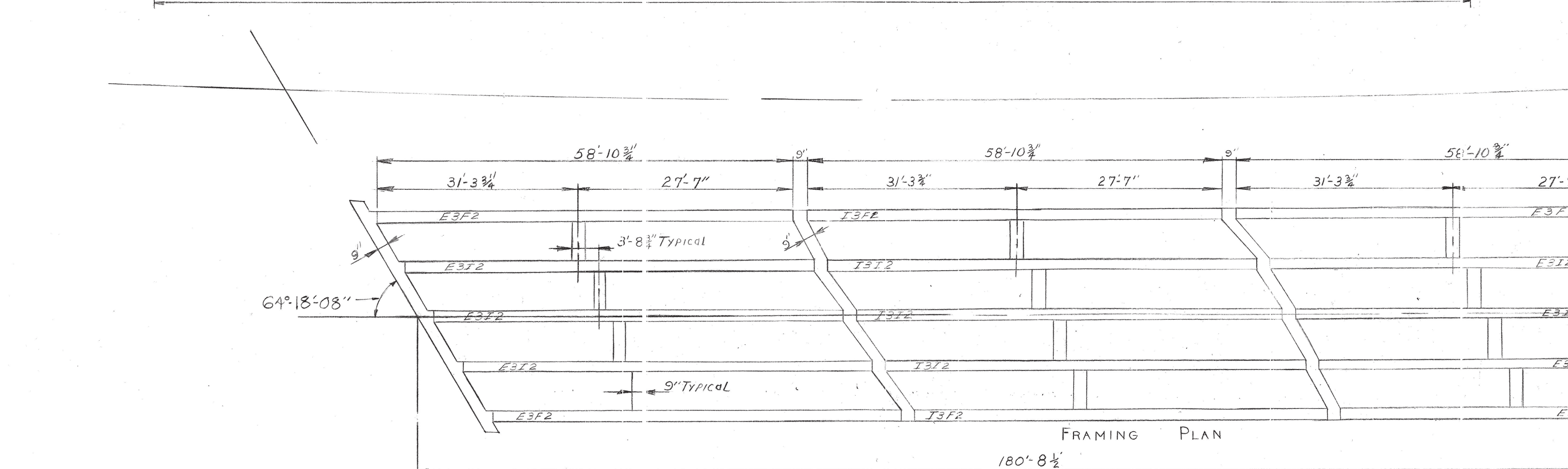
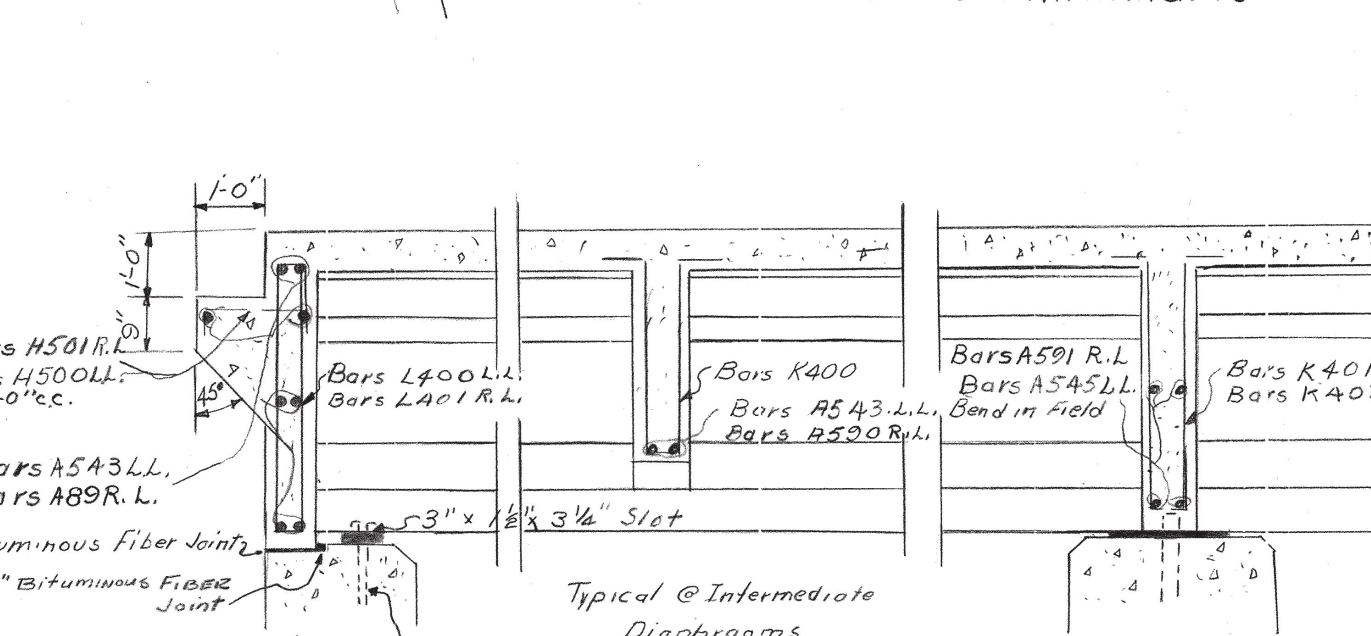
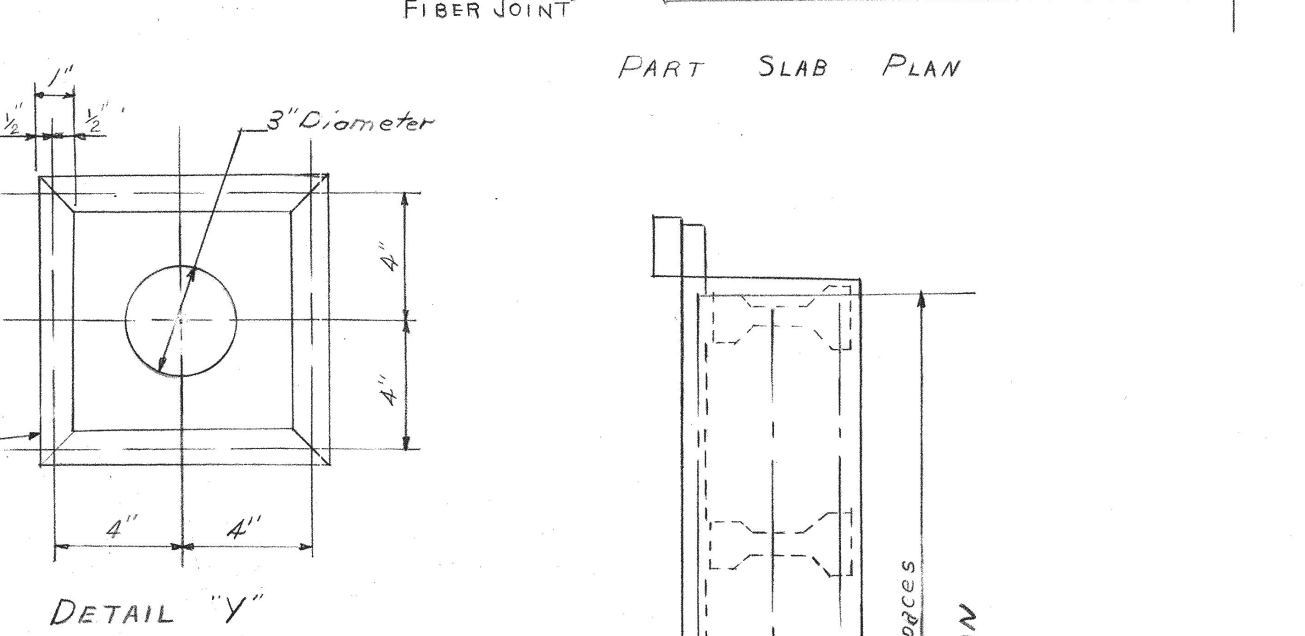
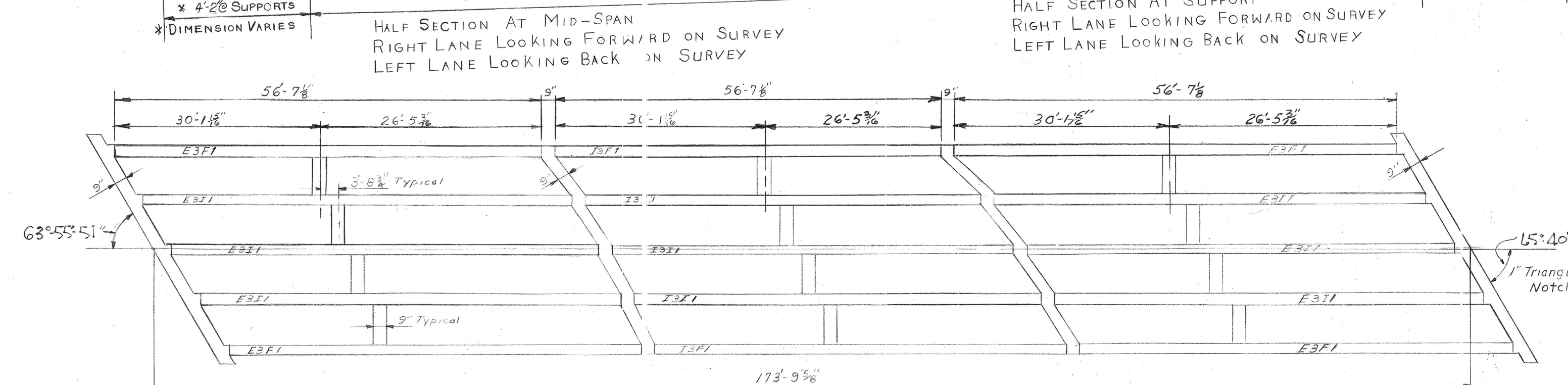
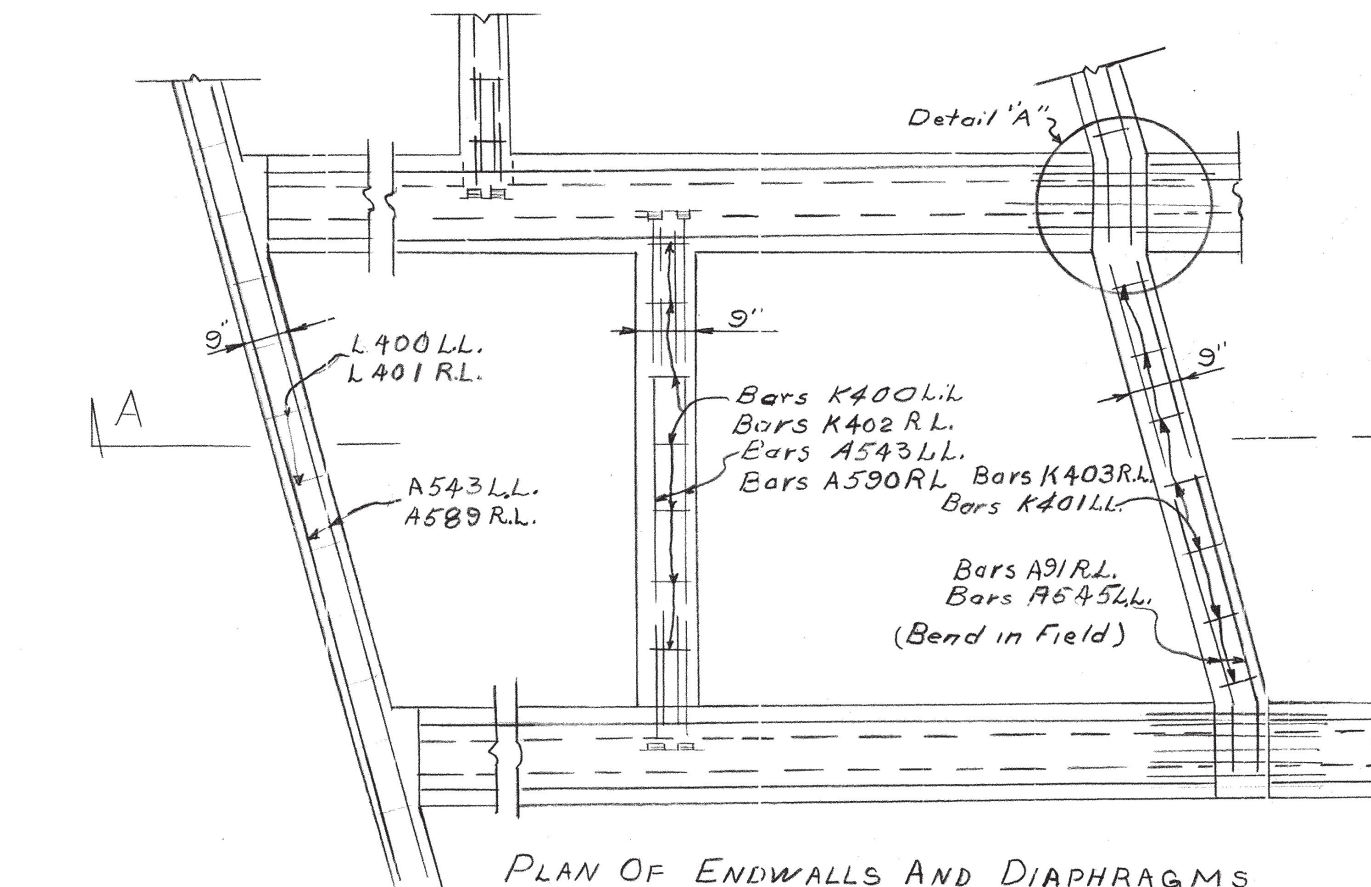
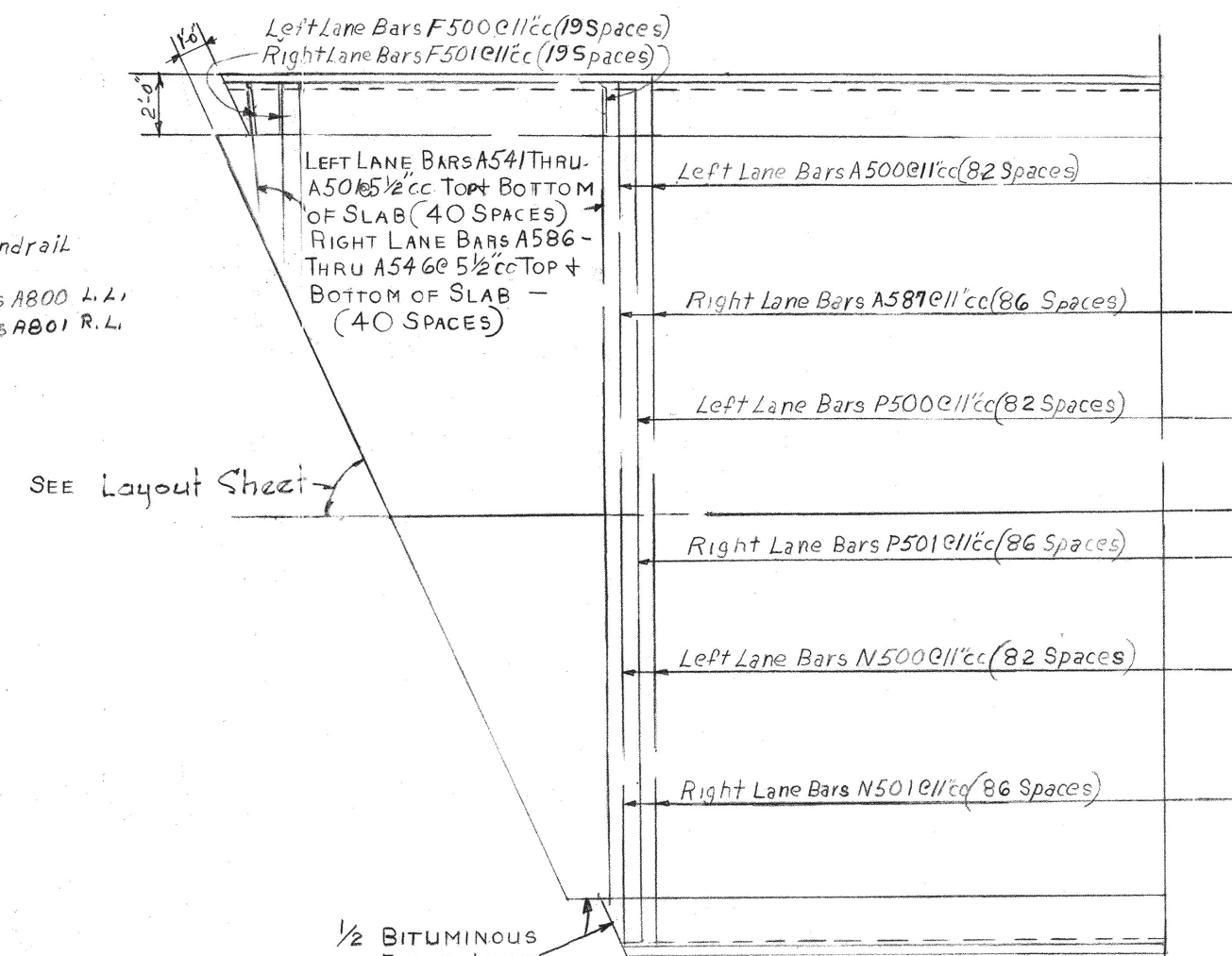
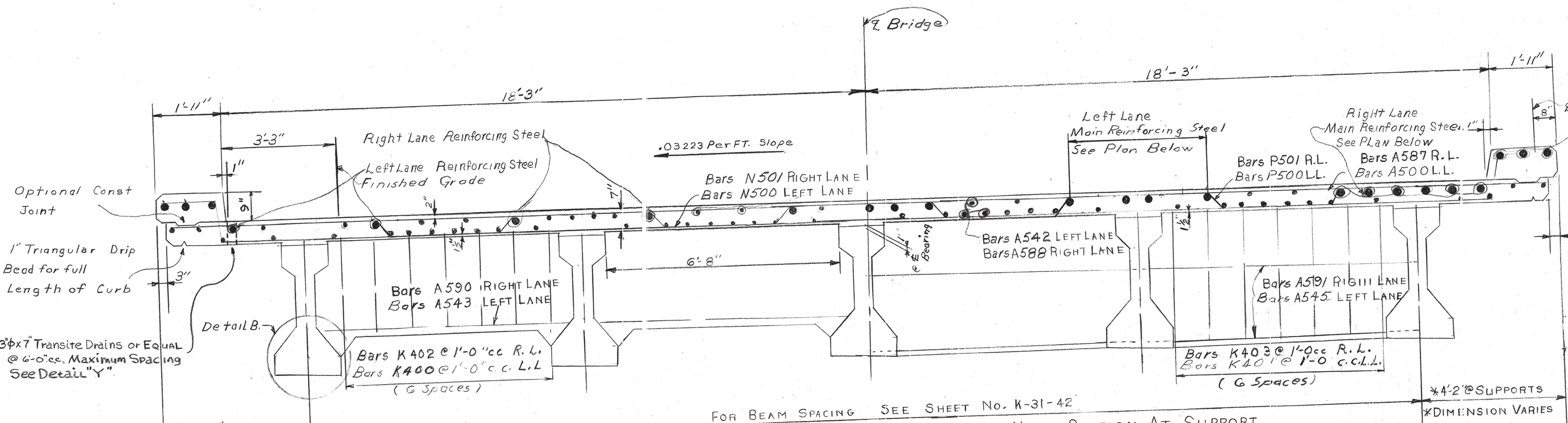
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
3	TENN.		19	46	256

I-40-3(46)154

Note: When pouring curb, provisions shall be made for setting handrail steel. See Dwg. H-5-10

Note: All slab steel to be placed radially

Reinforcement Symmetrical about Center of Bridge



DESIGNED BY: Couch
 DRAWN BY: J. G. BROWN
 CHECKED BY: _____
 DATE: 6-28-63
 DATE: 7-16-63
 DATE: _____

PLAN MAIN REINFORCING TOP OF SLAB RIGHT LANE

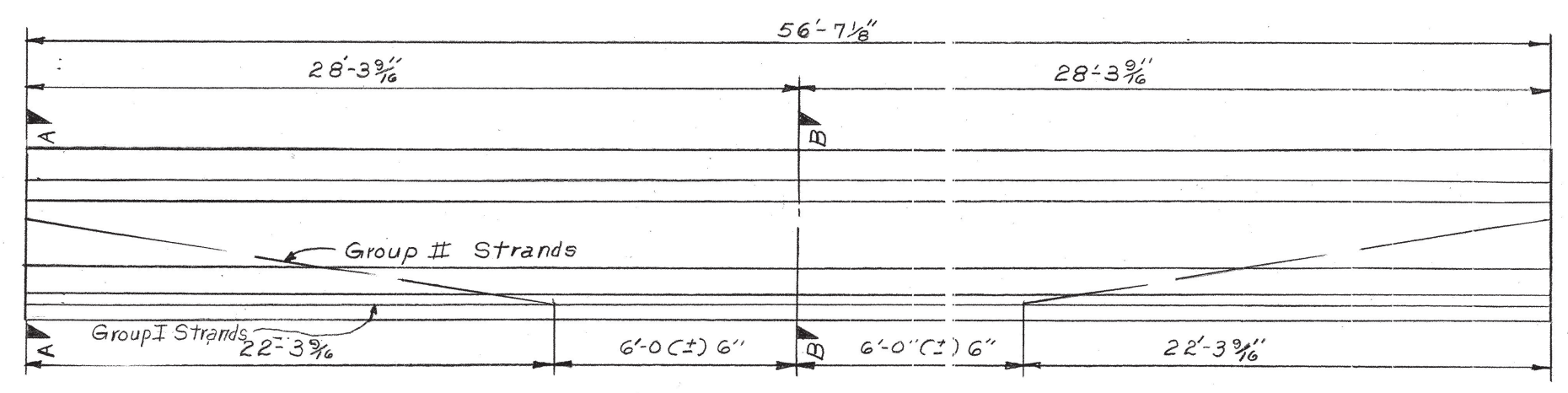
STATE OF TENNESSEE
 DEPARTMENT OF HIGHWAYS
 NASHVILLE
 SUPERSTRUCTURE DETAILS
 INTERSTATE 40 OVER GARNER GREEK
 STATION 746+02.22
 HICKMAN COUNTY
 1963

CORRECT: Fred Green
 APPROVED: R. Overlap
 BRIDGE ENGINEER
 STATE HIGHWAY ENGINEER

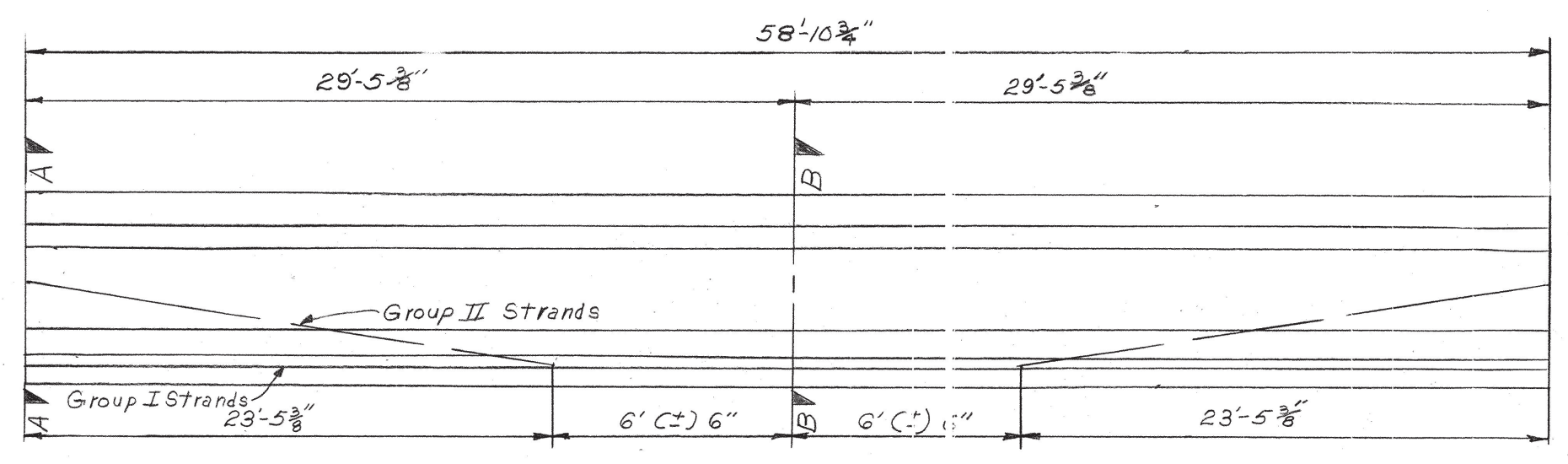
K-31-43

REVISIONS: 10-28-65 Angles Added
 11-10-65
 12-30-65

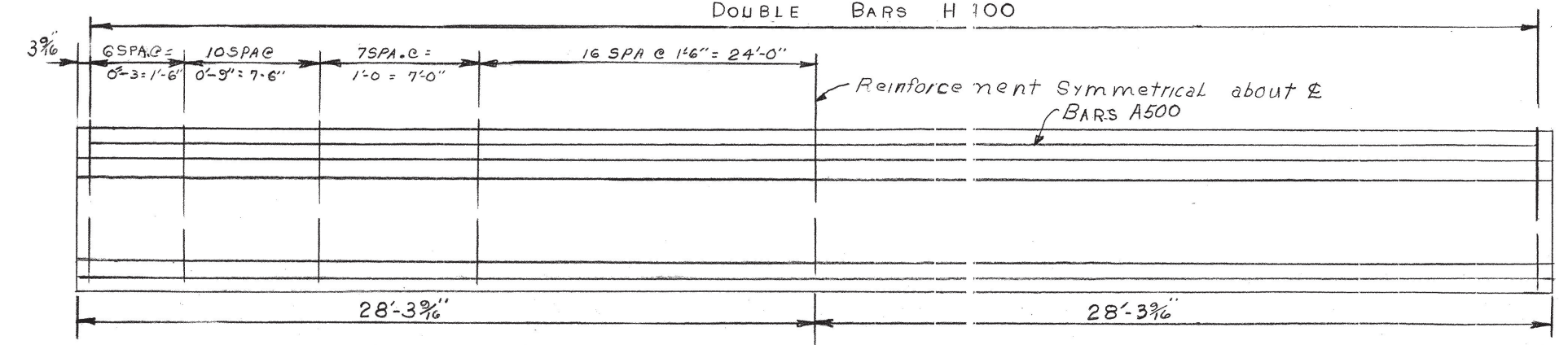
I-40-3(46)154



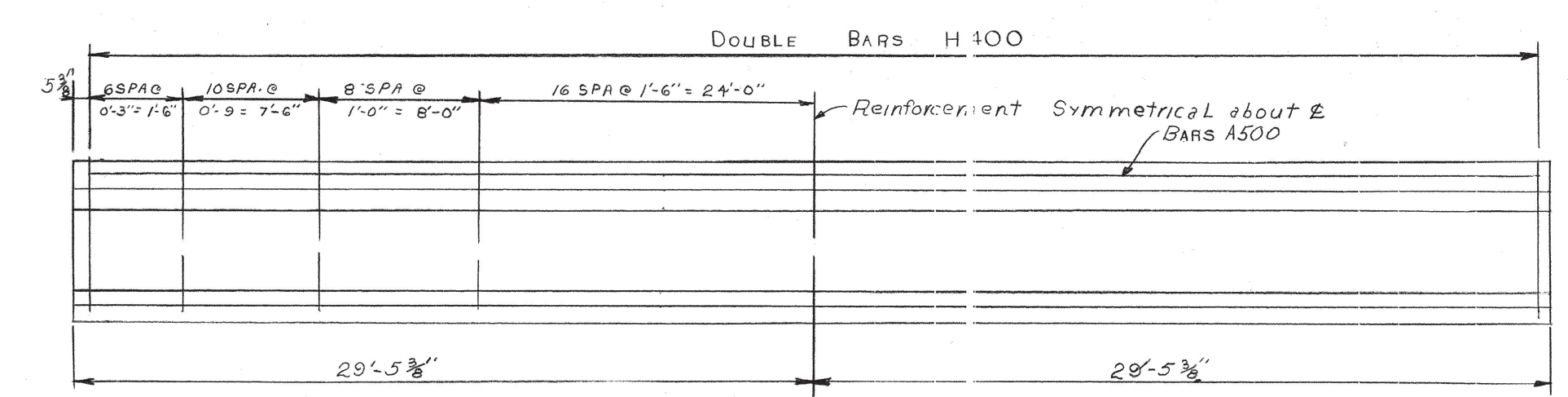
LEFT LANE ELEVATION SHOWING STRAND DEFLECTION PATTERN



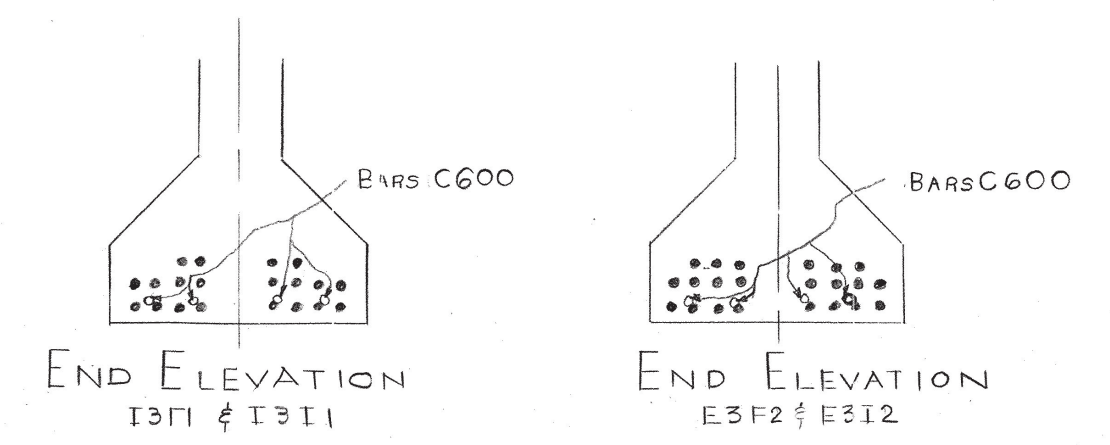
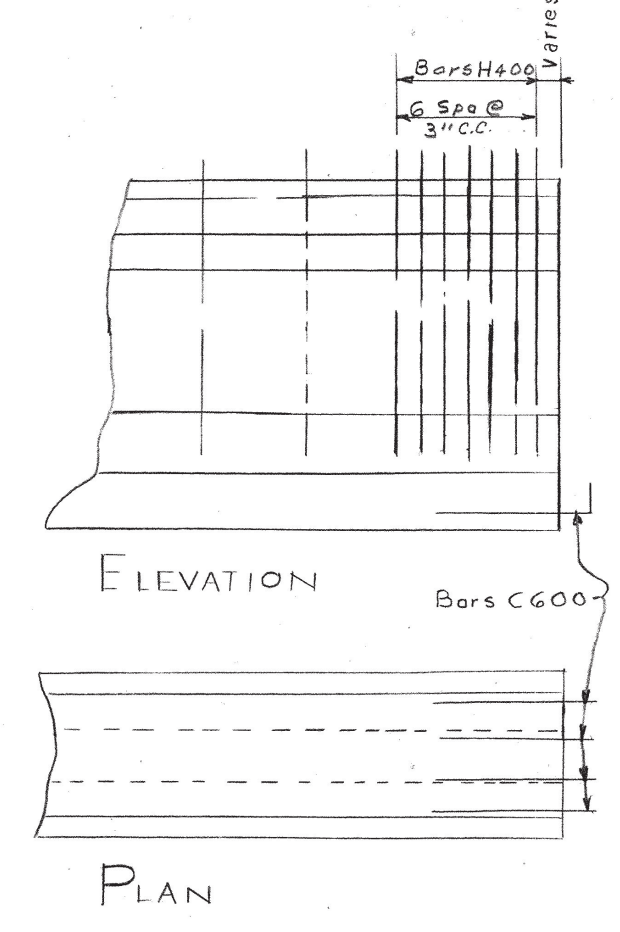
RIGHT LANE ELEVATION SHOWING STRAND DEFLECTION PATTERN



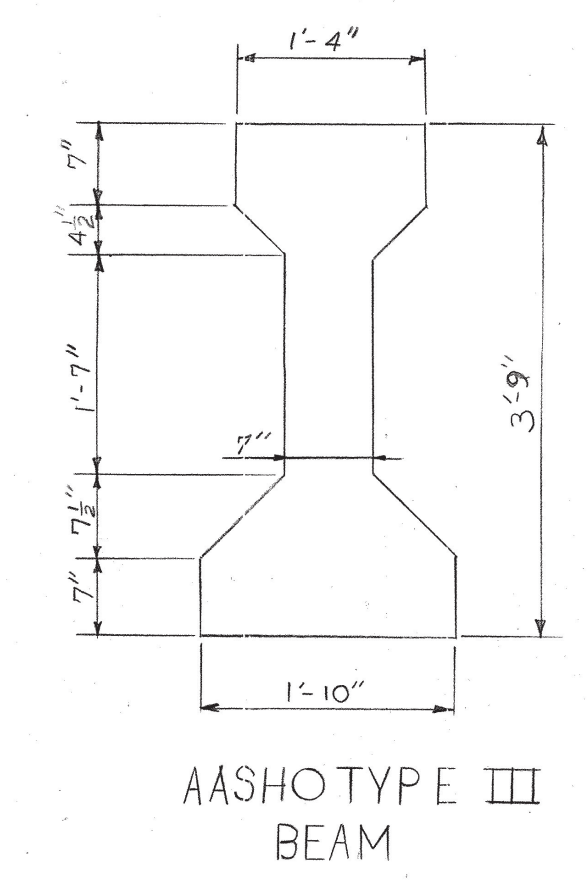
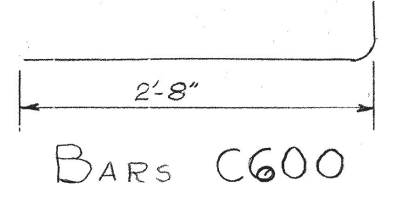
LEFT LANE ELEVATION SHOWING BEAM REINFORCEMENT



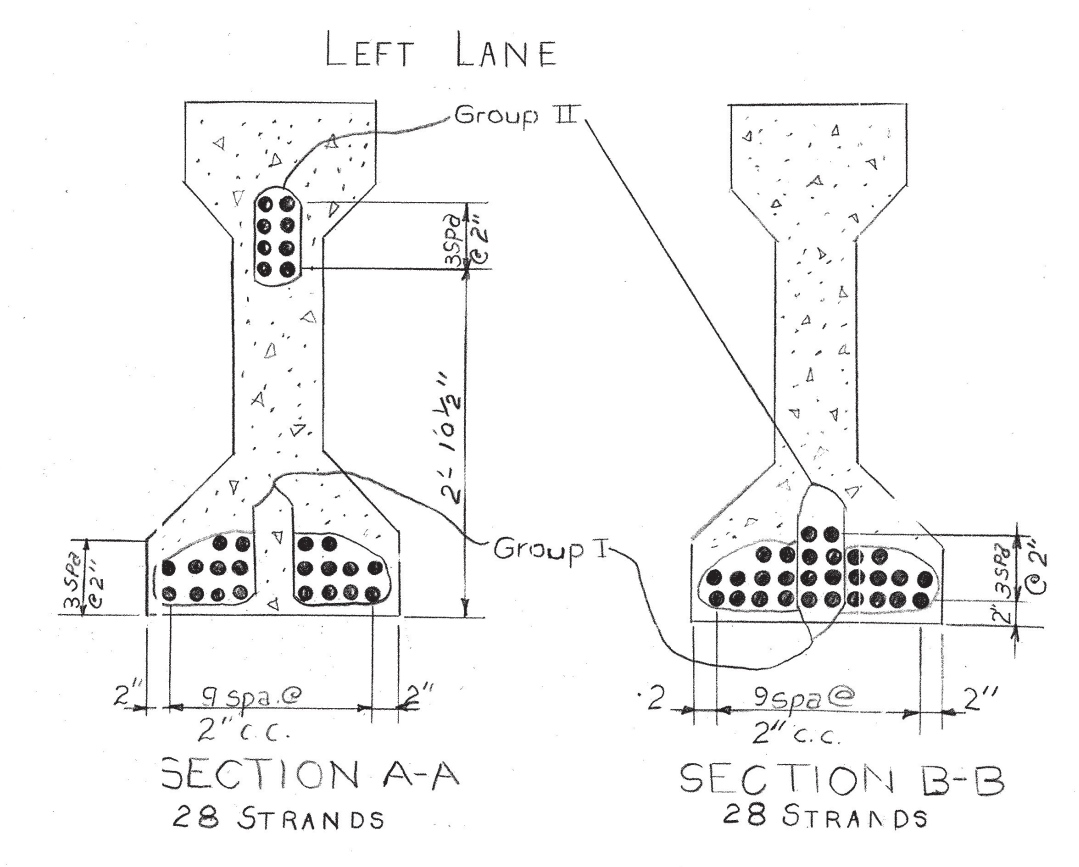
RIGHT LANE ELEVATION SHOWING BEAM REINFORCEMENT



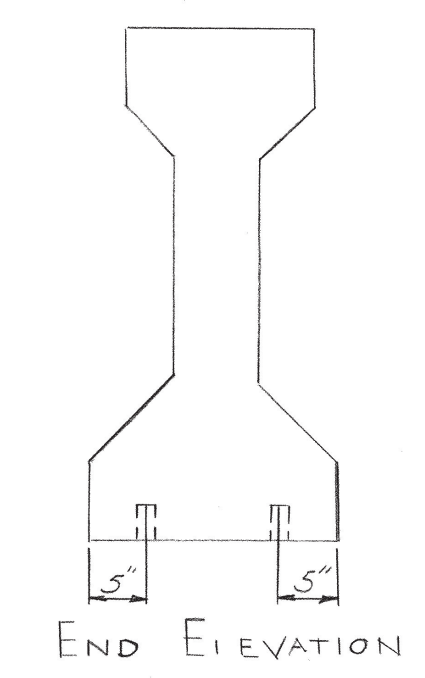
DETAILS SHOWING BEAM CONNECTION Typical at Bents



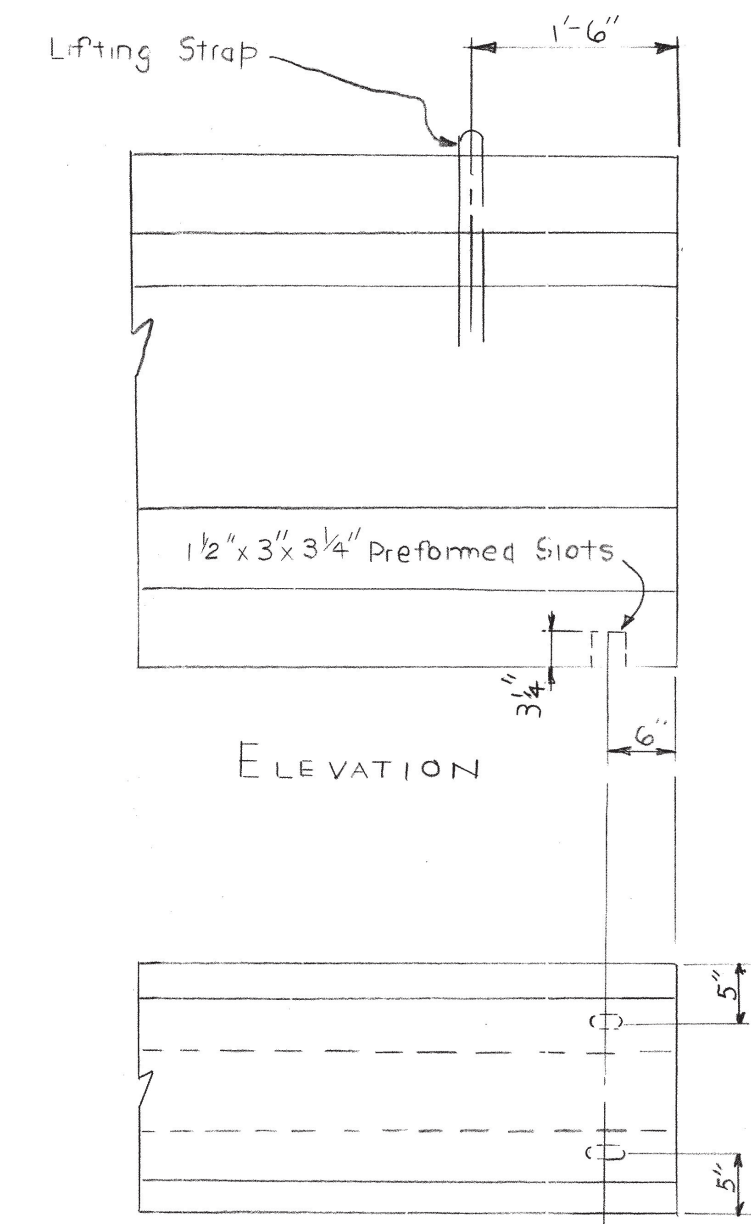
AASHTO TYPE III BEAM



SECTION A-A 28 STRANDS SECTION B-B 28 STRANDS



END ELEVATION



DETAILS AT ABUTMENT END OF BEAM

LEFT LANE BILL OF STEEL - EACH BEAM I3F1 & I3I1

Bar	Size	No.	Length
A500	5	2	56'-0"
C600	6	8	3'-0"
H400	4	158	5'-2"

LEFT LANE BILL OF STEEL - EACH BEAM E3F1 & E3I1

Bar	Size	No.	Length
A500	5	2	56'-0"
C600	6	4	3'-0"
H400	4	158	5'-2"
H500	5	5	3'-4"

- General Notes:
- All prestressing strands to be "M" 47 wire coated stress-relieved prestressed strands.
 - An initial force of 18,000 lbs shall be applied to each strand in all beams.
 - Top of beam to be rough floated. At approximately the time of initial set, the top of the beam shall be scrubbed transversely with a coarse wire brush to remove all laitance and to produce a rough surface.
 - All beams are AASHTO-PCI standard Type III.
 - 96 Threaded Bars To Provide A 2'-0" Lap with Bars A500 or A550, 48 Threaded Inserts, Richmond Type DT-1, OR Equal, To Be Furnished By Fabricator.

RIGHT LANE BILL OF STEEL - EACH BEAM I3F2 & I3I2

Bar	Size	No.	Length
A500	5	2	58'-0"
C600	6	8	3'-0"
H400	4	162	5'-2"

RIGHT LANE BILL OF STEEL - EACH BEAM E3F2 & E3I2

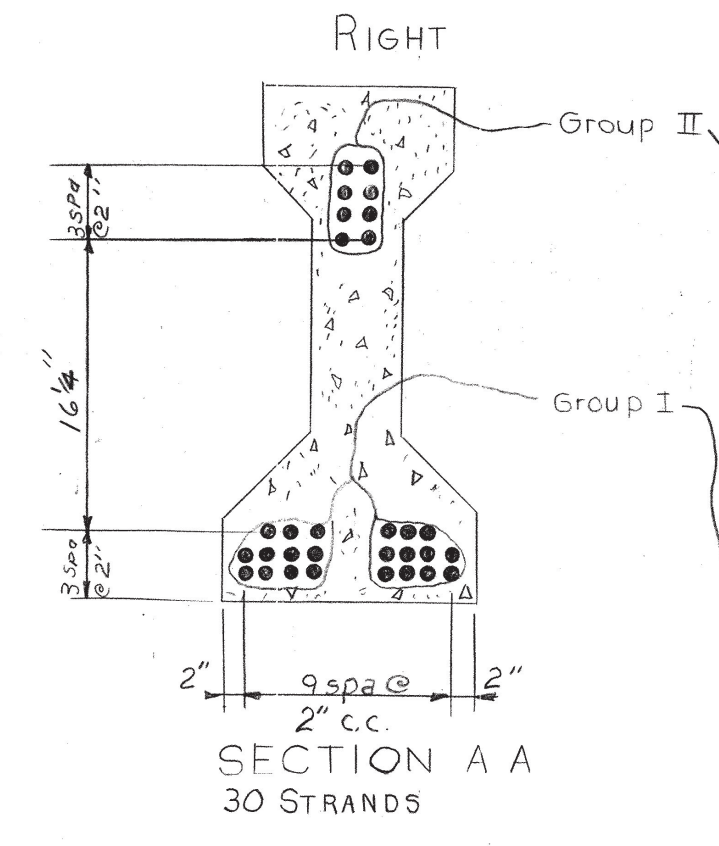
Bar	Size	No.	Length
A500	5	2	58'-0"
C600	6	4	3'-0"
H400	4	162	5'-2"
H500	5	5	3'-4"

LEFT LANE ESTIMATED QUANTITIES EACH BEAM

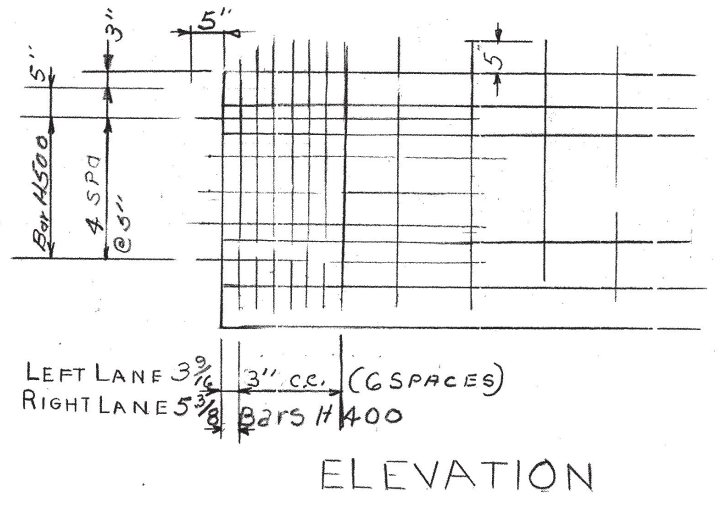
BEAM	Concrete Class A Cu Yds.	Reinforcing Steel lbs.	Prestressing Steel lbs.
E3F1 & E3I1	8.2	698	591
I3F1 & I3I1	8.2	698	591

RIGHT LANE ESTIMATED QUANTITIES EACH BEAM

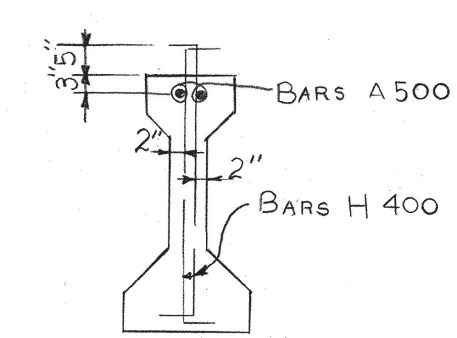
BEAM	Concrete Class A Cu Yds.	Reinforcing Steel lbs.	Prestressing Steel lbs.
E3F2 & E3I2	8.5	712	659
I3F2 & I3I2	8.5	712	659



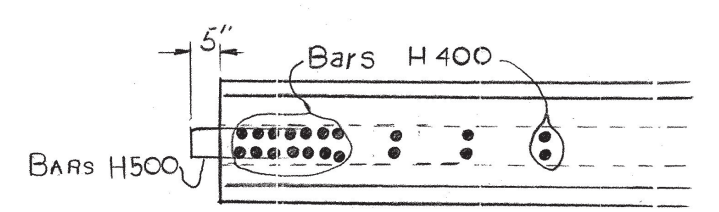
SECTION A A 30 STRANDS SECTION B B 30 STRANDS



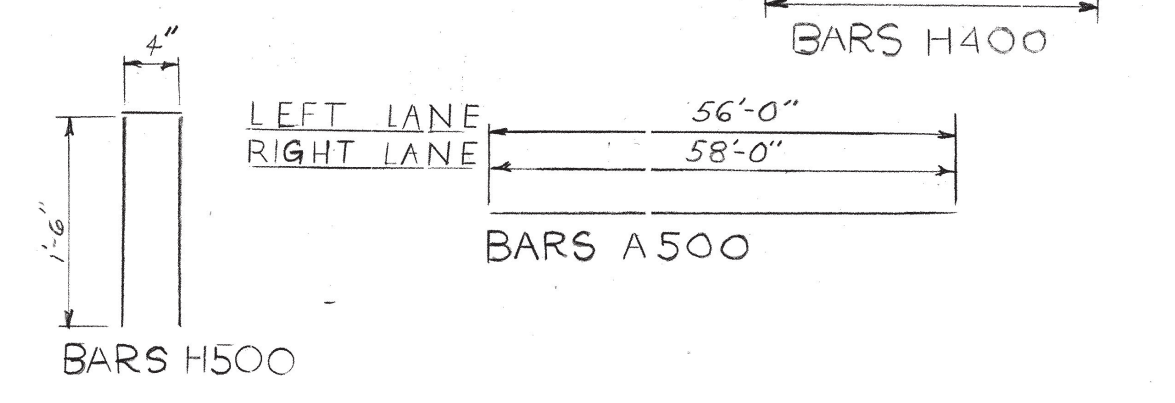
ELEVATION



WEB REINFORCEMENT



PLAN END REINFORCEMENT Typical at Abutments



LEFT LANE RIGHT LANE BARS H 400 BARS A 500 BARS H 500

STATE OF TENNESSEE DEPARTMENT OF HIGHWAYS NASHVILLE

PRESTRESSED BEAM DETAILS INTERSTATE 40 OVER GARNER CREEK STATION 746+02.22 HICKMAN COUNTY 1963

DESIGNED BY: Cochran DATE: 6-28-63
 DRAWN BY: T.E. Peters DATE: _____
 TRACED BY: _____ DATE: _____
 CHECKED BY: _____ DATE: _____

CORRECT: Fred Greer BRIDGE ENGINEER
 APPROVED: M. Dunlap STATE HIGHWAY ENGINEER

K-31-44

NOTE: When pouring Abutment Beam provisions shall be made for setting Dowel Bars for Prestressed Beams. Dowel Bar projection 3/4".

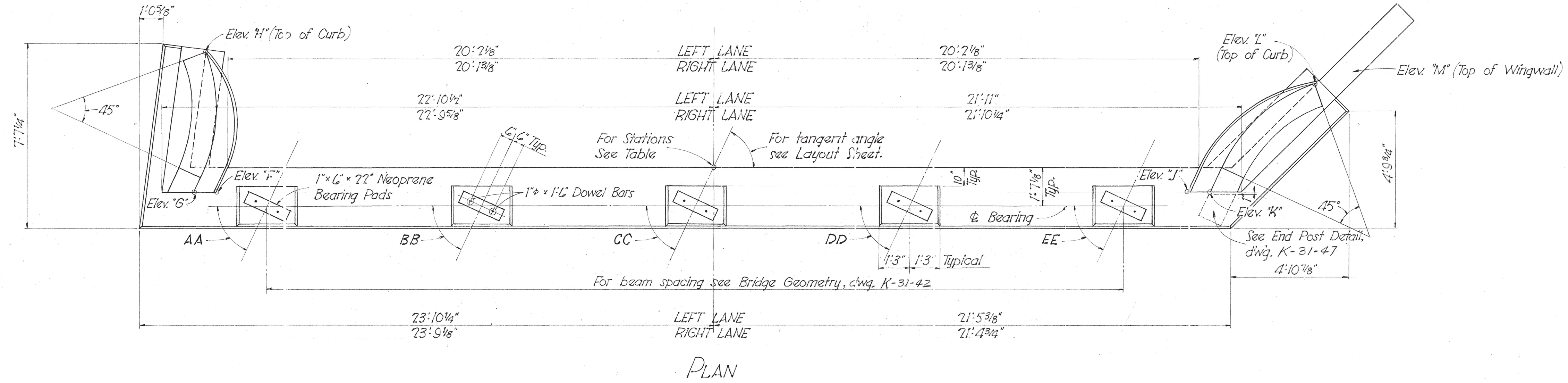
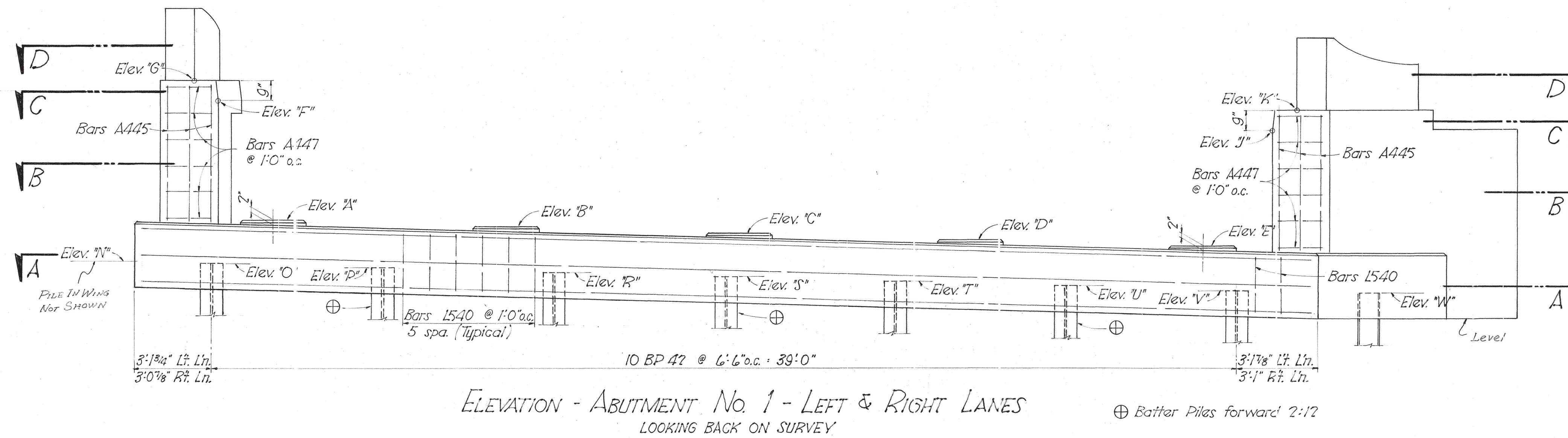


TABLE OF ANGLES

Item	AA	BB	CC	DD	EE
Abutment No. 1 - Lt. Ln.	64°18'01"	64°15'41"	64°13'27"	64°11'01"	64°08'46"
Abutment No. 1 - Rt. Ln.	64°40'38"	64°38'23"	64°36'08"	64°33'52"	64°31'36"



NOTE: For Sections A-A, B-B, C-C & D-D see dwg. K-31-47

ESTIMATED QUANTITIES

Concrete Class A	38.2	Cu. Yds.
Reinforcing Steel	4178	Lbs.

TABLE OF STATIONS & ELEVATIONS

Item	Stations	Elevations																				
		A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	R	S	T	U	V
Abutment No. 1 - Lt. Ln.	745+83.66	594.41	594.15	593.88	593.62	593.36	598.98	599.73	599.72	597.77	596.53	598.53	597.78	592.90	592.61	592.61	592.42	592.23	592.04	591.85	591.66	591.57
Abutment No. 1 - Rt. Ln.	746+70.77	598.08	597.82	597.58	597.32	597.07	602.66	603.40	603.41	601.51	602.26	602.28	601.53	596.56	596.47	596.29	596.11	595.92	595.74	595.55	595.37	595.28

STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS
NASHVILLE
ABUTMENTS No. 1
INTERSTATE 40 OVER GARNER CREEK
STATION 746+02.22
HICKMAN COUNTY
1963

CORRECT: Fred Gene
APPROVED: M. Dunlap
STATE HIGHWAY ENGINEER

Revised: 10-28-63 Tangent Angle Note Added
Elevated: 11-30-63

DESIGNED BY: CFC DATE: 6-28-63
DRAWN BY: JMS DATE:
TRACED BY: DATE:
CHECKED BY: DATE:

I-40-3(46)154

NOTE: When pouring Abutment Beam provisions shall be made for setting Dowel Bars for Prestressed Beams. Dowel Bar projection 3 3/4".

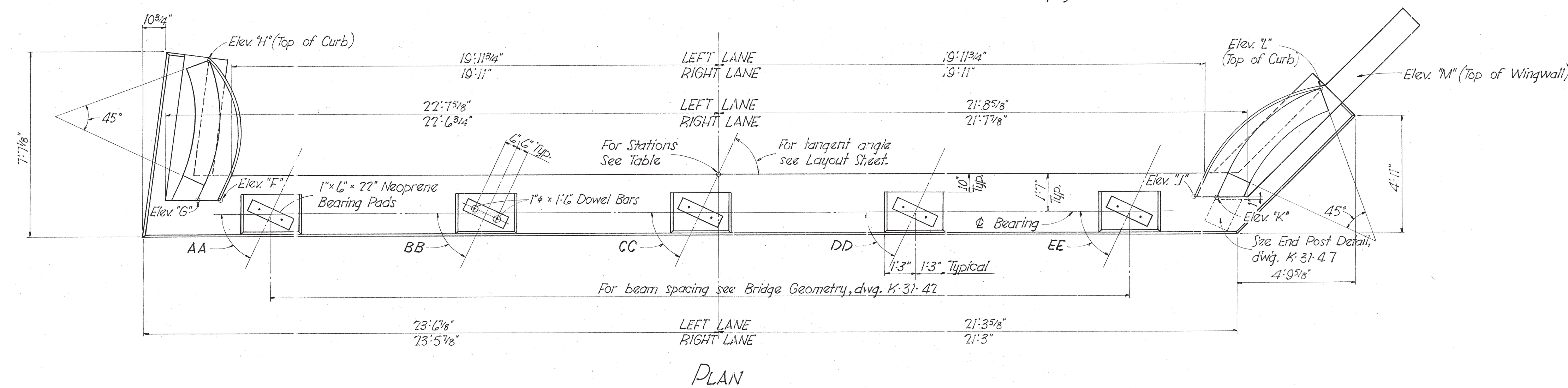
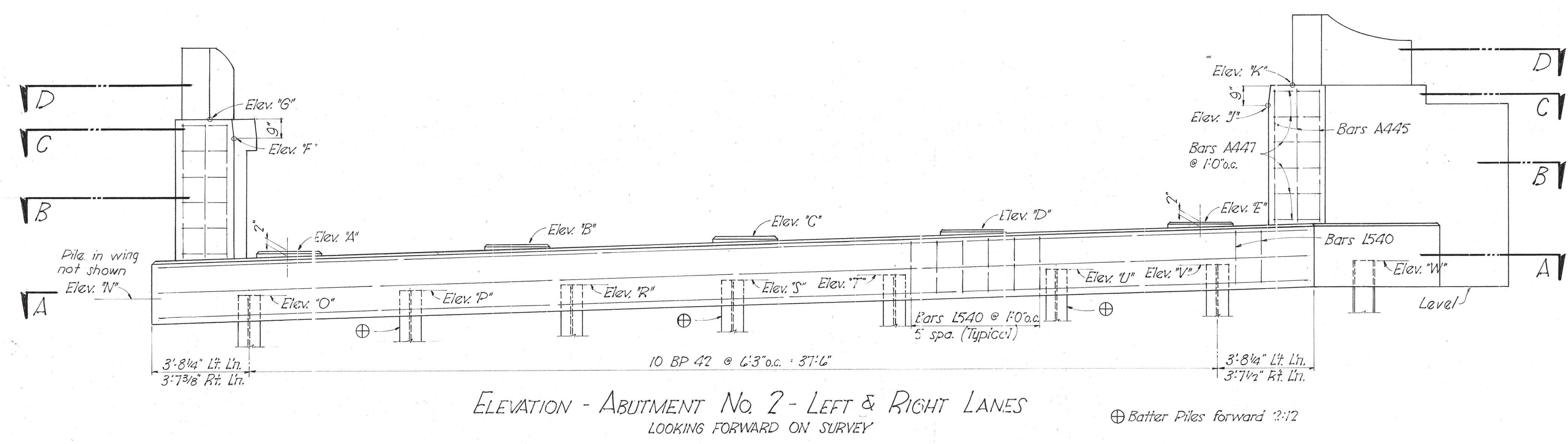


TABLE OF ANGLES

Item	AA	BB	CC	DD	EE
Abutment No. 2-Lt. Ln.	65°18'54"	65°21'07"	65°23'21"	65°25'33"	65°27'46"
Abutment No. 2-Rt. Ln.	65°43'22"	65°45'31"	65°47'40"	65°49'48"	65°51'56"



NOTE: For Sections A-A, B-B, C-C & D-D see dwg. K-31-47

ESTIMATED QUANTITIES

Concrete Class "A"	38.2	Cu. Yds.
Reinforcing Steel	4.178	Lbs.

TABLE OF STATIONS & ELEVATIONS

Item	Stations	Elevations																				
		"A"	"B"	"C"	"D"	"E"	"F"	"G"	"H"	"J"	"K"	"L"	"M"	"N"	"O"	"P"	"R"	"S"	"T"	"U"	"V"	"W"
Abutment No. 2-Lt. Ln.	747+58.83	594.61	594.91	595.22	595.53	595.84	599.01	599.76	599.83	600.42	601.17	601.25	600.50	592.75	592.88	593.10	593.32	592.54	593.76	593.98	594.20	594.33
Abutment No. 2-Rt. Ln.	748+00.05	597.77	598.07	598.35	598.65	598.95	602.19	602.94	602.99	603.54	604.30	604.36	603.61	595.97	596.04	596.25	596.47	596.68	596.89	597.10	597.31	597.43

Revised 10-28-63 Tangent Angle Note Added
REVISED 11-20-63

DESIGNED BY: CFC DATE: 6-28-63
 DRAWN BY: JWS DATE:
 TRACED BY: DATE:
 CHECKED BY: DATE:

CORRECT: Fred Gene
 APPROVED: [Signature] STATE HIGHWAY ENGINEER

K-31-46

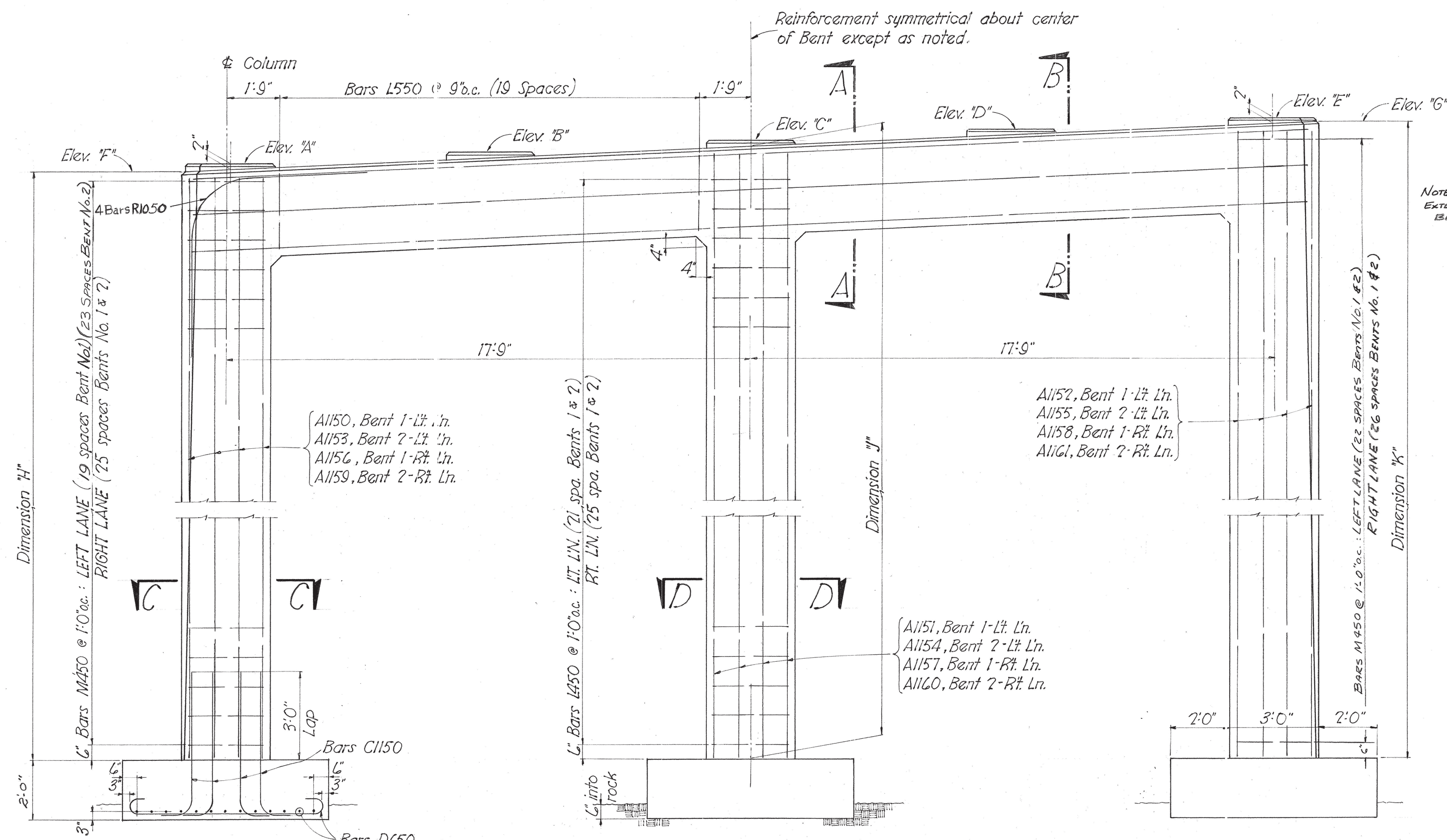
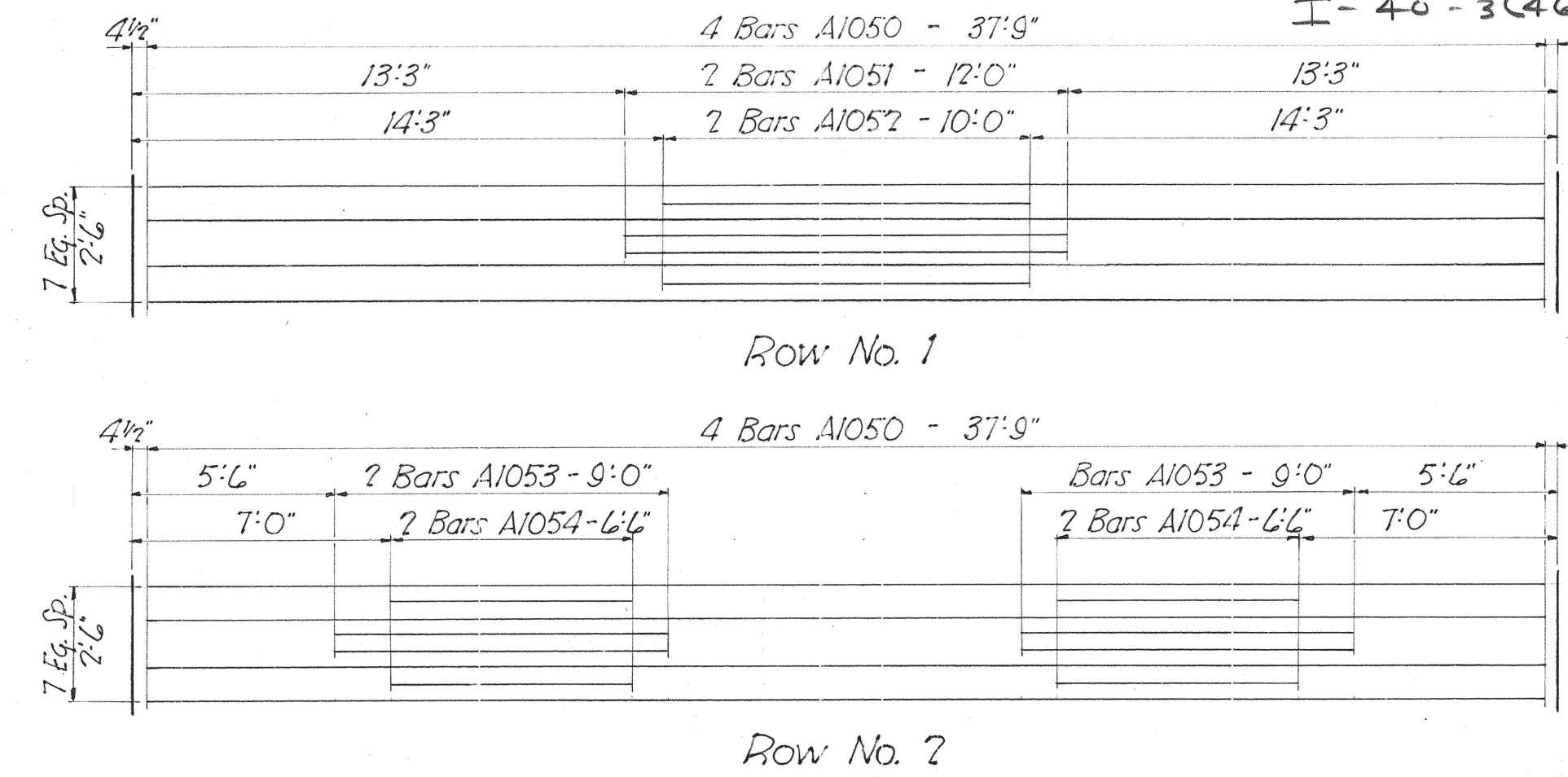
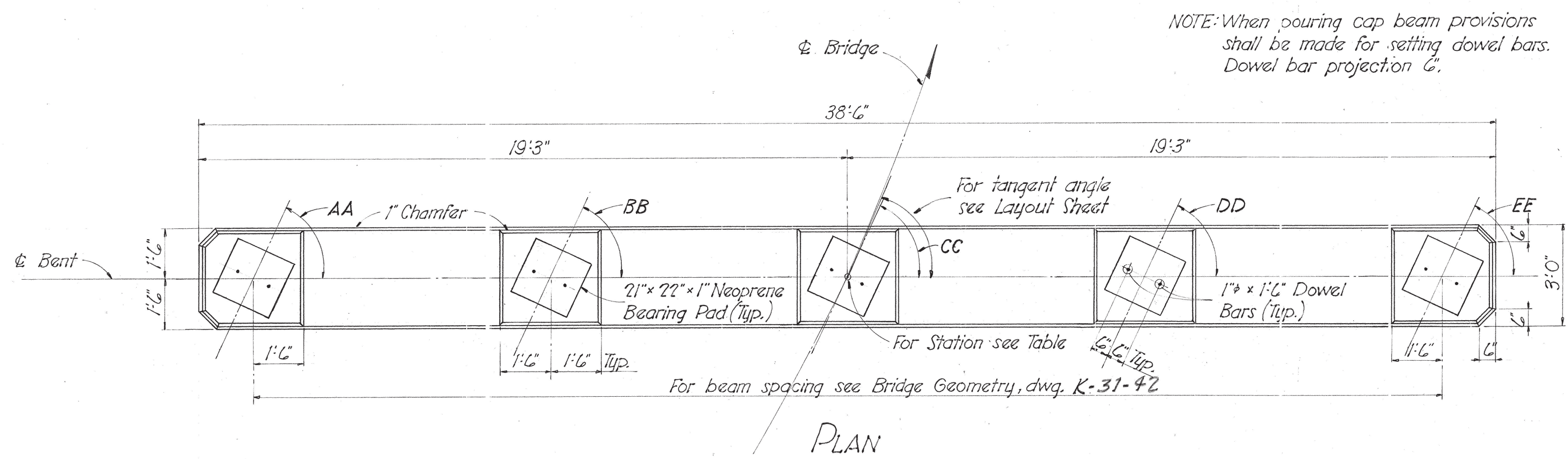
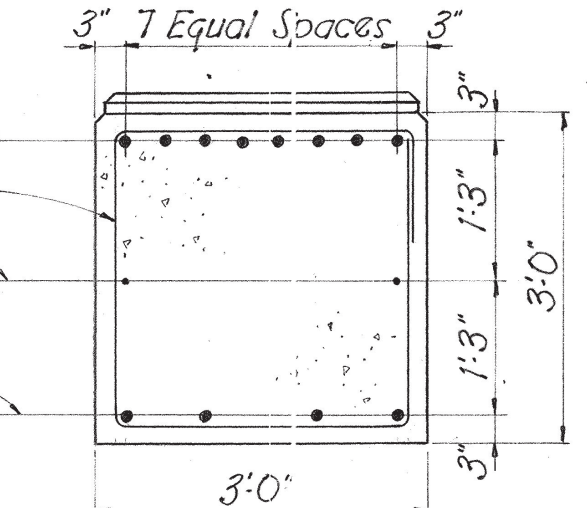
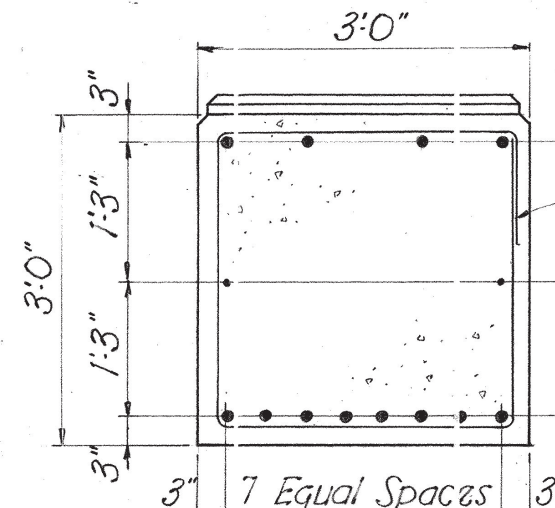
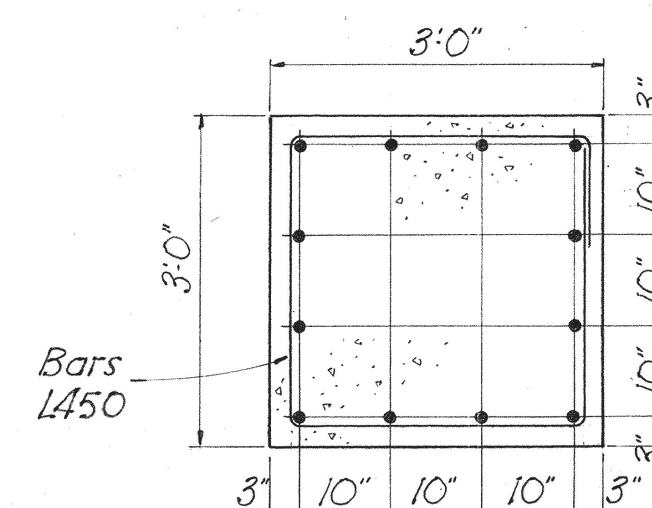
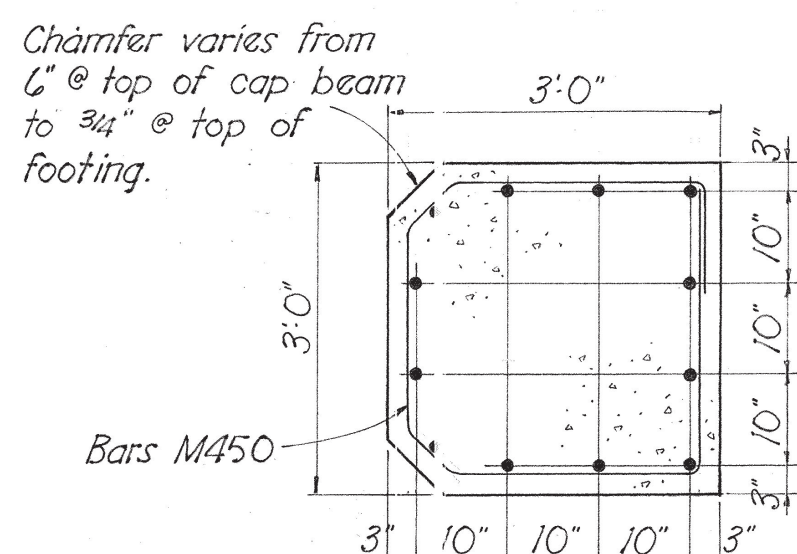
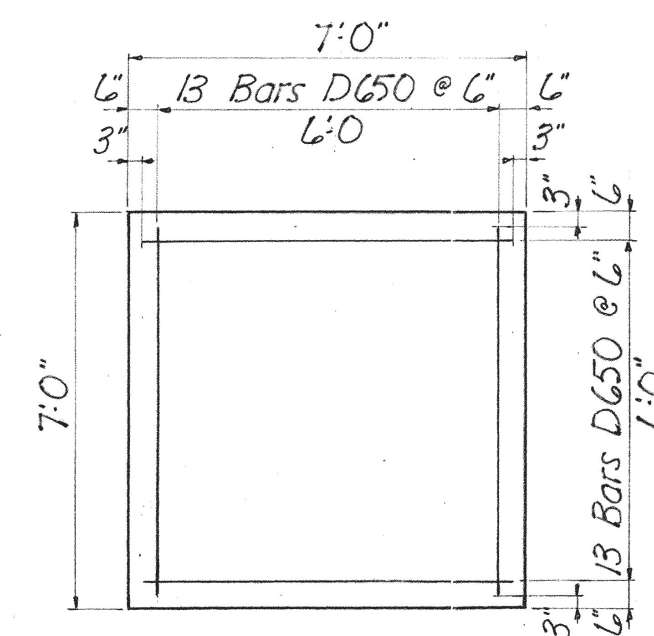


TABLE OF STATIONS, ANGLES, ELEVATIONS & DIMENSIONS

Item	Left Lane		Right Lane		
	Bent No. 1	Bent No. 2	Bent No. 1	Bent No. 2	
Stations	746+42.34	747+00.15	746+80.82	747+40.00	
Angles	AA	64°43'50"	65°18'54"	65°07'29"	65°43'22"
	BB	64°46'07"	65°21'07"	65°09'47"	65°45'31"
	CC	64°48'24"	65°23'21"	65°11'54"	65°47'40"
	DD	64°50'40"	65°25'33"	65°14'06"	65°49'48"
	EE	64°52'56"	65°27'46"	65°16'17"	65°51'56"
Elevations	A	593.53	593.95	597.07	597.30
	B	593.81	594.25	597.34	597.59
	C	594.10	594.54	597.60	597.86
	D	594.38	594.84	597.87	598.14
	E	594.65	595.12	598.14	598.43
	F	593.32	593.74	596.85	597.08
G	594.53	595.00	598.02	598.31	
Dim. H	20'-0"	23'-9"	26'-0"	25'-9"	
Dim. J	21'-9"	22'-9"	26'-6"	26'-3"	
Dim. K	23'-3"	23'-6"	27'-0"	27'-3"	

ESTIMATED QUANTITIES

Item	Concrete Class "A" Reinforcing Steel	
	Cu. Yds.	Lbs.
Bent No. 1	42.7	9238
Bent No. 2	44.3	9580
Bent No. 1	47.5	10,264
Bent No. 2	47.4	10,248



DESIGNED BY: C.F.C. DATE: 6-29-63
 DRAWN BY: MWS DATE: 7-22-63
 TRACED BY: DATE: _____
 CHECKED BY: DATE: _____

CORRECT: Fred Greer
 APPROVED: R. Dunlap
 BRIDGE ENGINEER
 STATE HIGHWAY ENGINEER

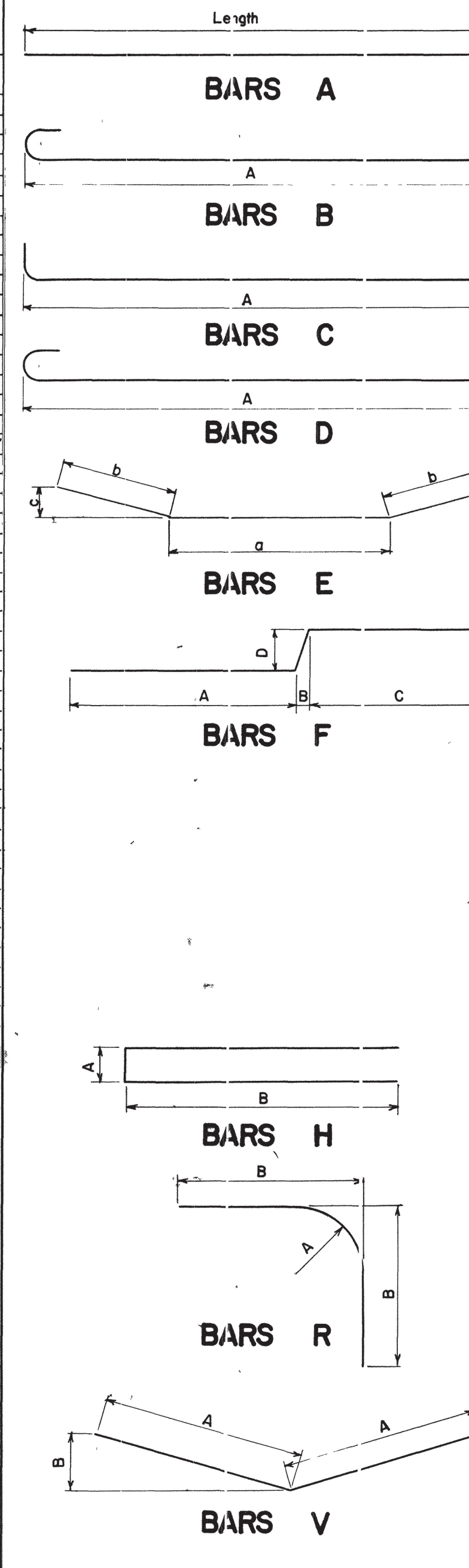
LEFT LANE

BILL OF STEEL

RIGHT LANE

FED ROAD DIST NC	STATE	FED AID PROJ NO	FISCAL YEAR	SHEET NO	TOTAL SHEETS
3	TENN.		19	52	256

SUPERSTRUCTURE					ABUTMENTS NO. 1 & 2					BENTS NO. 1 & 2								
Bar	Location	Size	No. Reqd.	Bending Dimensions	Length	Bar	Location	Size	No. Reqd.	Bending Dimensions	Length	Bar	Location	Size	No. Reqd.	Bending Dimensions	Length	
					A	B	C	D						A	B	C	D	
A501	SLAB	5	166		39'-2"	A140	Beam	#4	70		73'-6"	A1050	Cap. Beam	#10	16		37'-9"	
A502	"	5	4		39'-2"	A141	Beam	#4	70		73'-6"	A1051	"	#10	4		12'-0"	
A503	"	5	4		38'-2"	A142	Beam	#4	70		73'-6"	A1052	"	#10	4		10'-0"	
A504	"	5	4		37'-9"	A143	Beam	#4	70		73'-6"	A1053	"	#10	8		9'-0"	
A505	"	5	4		37'-9"	A144	Beam	#4	70		73'-6"	A1054	"	#10	8		6'-6"	
A506	"	5	4		37'-9"	A145	Beam	#4	70		73'-6"	A1150	Columns (Bent 1)	#11	12		19'-6"	
A507	"	5	4		36'-10"	A146	Beam	#4	70		73'-6"	A1151	" (Bent 1)	#11	12		21'-3"	
A508	"	5	4		35'-10"	A147	Wingwall	#4	70		73'-6"	A1152	" (Bent 2)	#11	12		22'-9"	
A509	"	5	4		34'-10"	A148	Wingwall	#4	70		73'-6"	A1153	" (Bent 2)	#11	12		23'-3"	
A510	"	5	4		34'-10"	A149	Curb	#4	70		73'-6"	A1154	" (Bent 2)	#11	12		23'-3"	
A511	"	5	4		33'-10"	A150	Curb	#4	70		73'-6"	A1155	" (Bent 2)	#11	12		23'-0"	
A512	"	5	4		32'-2"	A151	Wing Post	#4	70		73'-6"	A1156	Footings & Columns	#11	72	4'-9"	8'-6"	
A513	"	5	4		31'-3"	A152	"	#4	70		73'-6"	A1560	Footings	#6	156	6'-6"	7'-10"	
A514	"	5	4		30'-3"	A153	"	#4	70		73'-6"	A550	Cap. Beam	#5	4		37'-9"	
A515	"	5	4		29'-4"	A154	"	#4	70		73'-6"	A550	Middle Columns	#4	44	2'-8" 1'-0" 2'-8"	11'-8"	
A516	"	5	4		28'-6"	A155	"	#4	70		73'-6"	A550	Chamfered "	#4	88	2'-8" 2'-8" 5" 1'-0"	11'-8"	
A517	"	5	4		27'-5"	A156	Wing Post	#4	70		73'-6"	R1050	Columns	#10	16	2'-0" 2'-0"	11'-2"	
A518	"	5	4		26'-5"	A157	Beam	#7	30		73'-6"	L550	Cap. Beam	#5	80	2'-8" 1'-0" 2'-8"	11'-8"	
A519	"	5	4		25'-5"	A158	Beam	#7	30		73'-6"							
A520	"	5	4		24'-6"	A159	Beam & Curb	#4	52	3'-2" 2'-0"	4'-8"							
A521	"	5	4		23'-7"	A160	Curb	#4	52	3'-2" 2'-0"	4'-8"							
A522	"	5	4		22'-8"	A161	"	#4	52	3'-2" 2'-0"	4'-8"							
A523	"	5	4		21'-9"	A162	"	#4	52	3'-2" 2'-0"	4'-8"							
A524	"	5	4		20'-9"	A163	"	#4	52	3'-2" 2'-0"	4'-8"							
A525	"	5	4		19'-9"	A164	"	#4	52	3'-2" 2'-0"	4'-8"							
A526	"	5	4		18'-9"	A165	"	#4	52	3'-2" 2'-0"	4'-8"							
A527	"	5	4		17'-9"	A166	Curb	#4	52	3'-2" 2'-0"	4'-8"							
A528	"	5	4		16'-10"	A167	Beam	#5	76	2'-2" 1'-0" 2'-2"	9'-8"							
A529	"	5	4		15'-10"													
A530	"	5	4		14'-10"													
A531	"	5	4		13'-11"													
A532	"	5	4		12'-11"													
A533	"	5	4		11'-2"													
A534	"	5	4		10'-3"													
A535	"	5	4		9'-4"													
A536	"	5	4		8'-5"													
A537	"	5	4		7'-6"													
A538	"	5	4		6'-7"													
A539	"	5	4		5'-8"													
A540	"	5	4		5'-9"													
A541	"	5	4		5'-10"													
A542	SLAB	5	240		36'-3"													
A543	END WALL	5	16		39'-9"													
A544	INT DIAPHRAGM	5	24		7'-5"													
A545	SUPPORT DIAPH	5	24		35'-6"													
A800	CURBS	8	30		36'-3"													
A1100	TOP OF SLAB	11	50		37'-9"													
A1101	TOP OF SLAB	11	30		29'-0"													
K400	INT DIAPH.	4	84	6" 5" 2'-8"	6'-9"													
K401	SUPPORT DIAPH	4	56	6" 5" 3'-9"	8'-11"													
L400	END WALL	4	76	5" 6" 3'-11"	9'-2"													
H500	RD. WY. BRACKET	5	76	1'-5" 6" 2'-5"	2'-5"													
F500	Seal Course	5	40	2'-6" 1'-6" 1'-7"	5'-11"													
N500	SLAB	5	164	3'-0" 1'-6" 3'-0" 1'-0"	41'-7"													
P500	SLAB	5	163	3'-0" 5'-0" 4'-0" 3'-5"	40'-4"													



SUPERSTRUCTURE					ABUTMENTS NO. 1 & 2					BENTS NO. 1 & 2								
Bar	Location	Size	No. Reqd.	Bending Dimensions	Length	Bar	Location	Size	No. Reqd.	Bending Dimensions	Length	Bar	Location	Size	No. Reqd.	Bending Dimensions	Length	
					A	B	C	D						A	B	C	D	
A546	SLAB	5	174		39'-2"	A1050	Cap. Beam	#10	16		37'-9"	A1050	Cap. Beam	#10	16		37'-9"	
A547	"	5	4		38'-2"	A1051	"	#10	4		12'-0"	A1051	"	#10	4		12'-0"	
A548	"	5	4		37'-9"	A1052	"	#10	4		10'-0"	A1052	"	#10	4		10'-0"	
A549	"	5	4		37'-9"	A1053	"	#10	8		9'-0"	A1053	"	#10	8		9'-0"	
A550	"	5	4		37'-9"	A1054	"	#10	8		6'-6"	A1054	"	#10	8		6'-6"	
A551	"	5	4		37'-9"	A1150	Columns (Bent 1)	#11	12		19'-6"	A1150	Columns (Bent 1)	#11	12		19'-6"	
A552	"	5	4		36'-10"	A1151	" (Bent 1)	#11	12		21'-3"	A1151	" (Bent 1)	#11	12		21'-3"	
A553	"	5	4		35'-10"	A1152	" (Bent 2)	#11	12		22'-9"	A1152	" (Bent 2)	#11	12		22'-9"	
A554	"	5	4		34'-10"	A1153	" (Bent 2)	#11	12		23'-3"	A1153	" (Bent 2)	#11	12		23'-3"	
A555	"	5	4		34'-10"	A1154	" (Bent 2)	#11	12		23'-3"	A1154	" (Bent 2)	#11	12		23'-3"	
A556	"	5	4		33'-10"	A1155	" (Bent 2)	#11	12		23'-0"	A1155	" (Bent 2)	#11	12		23'-0"	
A557	"	5	4		32'-2"	A1156	Footings & Columns	#11	72	4'-9"	8'-6"	A1156	Footings & Columns	#11	72	4'-9"	8'-6"	
A558	"	5	4		31'-3"	A1560	Footings	#6	156	6'-6"	7'-10"	A1560	Footings	#6	156	6'-6"	7'-10"	
A559	"	5	4		30'-3"	A550	Cap. Beam	#5	4		37'-9"	A550	Cap. Beam	#5	4		37'-9"	
A560	"	5	4		29'-4"	A550	Middle Columns	#4	44	2'-8" 1'-0" 2'-8"	11'-8"	A550	Middle Columns	#4	52		11'-8"	
A561	"	5	4		28'-6"	A550	Chamfered "	#4	88	2'-8" 2'-8" 5" 1'-0"	11'-8"	A550	Chamfered "	#4	102	2'-8" 2'-8" 5" 1'-0"	11'-8"	
A562	"	5	4		27'-5"	R1050	Columns	#10	16	2'-0" 2'-0"	11'-2"	R1050	Columns	#10	16	2'-0" 2'-0"	11'-2"	
A563	"	5	4		26'-5"	L550	Cap. Beam	#5	80	2'-8" 1'-0" 2'-8"	11'-8"	L550	Cap. Beam	#5	80	2'-8" 1'-0" 2'-8"	11'-8"	
A564	"	5	4		25'-5"													
A565	"	5	4		24'-6"													
A566	"	5	4		23'-7"													
A567	"	5	4		22'-8"													
A568	"	5	4		21'-9"													
A569	"	5	4		20'-9"													
A570	"	5	4		19'-9"													
A571	"	5	4		18'-9"													
A572	"	5	4		17'-9"													
A573	"	5	4		16'-10"													
A574	"	5	4		15'-10"													
A575	"	5	4		14'-10"													
A576	"	5	4		13'-11"													
A577	"	5	4		12'-11"													
A578	"	5	4		11'-2"													
A579	"	5	4		10'-3"													
A580	"	5	4		9'-4"													
A581	"	5	4		8'-5"													
A582	"	5	4		7'-6"													
A583	"	5	4		6'-7"													
A584	"	5	4		5'-8"													
A585	"	5	4		5'-9"													
A586	"	5	4		5'-10"													
A586	SLAB	5	241		37'-9"													
A587	END WALL	5	16		39'-9"													
A588	INT DIAPHRAGM	5	24		7'-5"													
A589	SUPPORT DIAPH	5	24		35'-6"													
A801	CURBS	8	30		36'-3"													
A1102	TOP OF SLAB	11	50		37'-9"													